



AMERICAN
STRUCTUREPOINT
INC.

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION/ENVIRONMENTAL
ASSESSMENT FORM**

Road/County

Yeager Road/ Tippecanoe County

Project Description:

Intersection and road improvements

Prepared by:

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Indianapolis, Indiana 46256

Des. No. 0600696

Project No. IN20061278

March 15, 2010



Indiana Department of Transportation

County Tippecanoe Route Yeager Rd Des. No. 0600696 Project No. _____

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	Yeager Road, Tippecanoe County
Designation Number:	0600696
Project Description/Termini:	Reconstruction of Yeager Road between US 231 and US 52 and addition of curb, gutter, sidewalk and multi-use path.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
X	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, OES.
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, OES, FHWA.
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: OES, FHWA.

Approval *MLC* 4-12-10 *[Signature]* 4-5-10
 ESM Signature Date OES Signature Date

 FHWA Signature Date

Release for Public Involvement MLE
 ESM Initials Date

BSJ 1-8-10
 OES Initials Date

Certification of Public Involvement *[Signature]* 3-8-10
 Manager, Public Hearings Signature Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

Name and organization of CE/EA Preparer: Brianna W. Chapman, American Structurepoint, Inc.

This is page 1 of 23. Project name: Yeager Road Reconstruction Project Date: 12/29/2009

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Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Discuss what public involvement activities (legal notices, letters to affected property owners and residents, meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

The Section 106 Area of Potential Effect (APE) determination (36 CFR 800.4(a)(1)) and the no adverse effect determination (36 CFR 800.11(e)) was approved by INDOT on August 7, 2009 and distributed to SHPO and other consulting parties on August 18, 2009. A public notice was posted in the *Journal and Courier* on August 18, 2009 to advertise the finding and request any comments, questions, or to submit requests for a public meeting. The public had thirty days to submit comments with a comment period ending on September 18, 2009. The Section 106 Documentation was made available to the public during this time at American Structurepoint, Inc. The Section 106 Documentation is included in the appendix of this report. Two letters supporting the finding were received during the comment period however no requests for public meeting were received. The proof of publication and letters received during the comment period are included in the Section 106 Documentation in Appendix D.

A public hearing was scheduled and conducted in compliance with public involvement procedures developed by INDOT and approved by the FHWA. A Legal Notice of Public Hearing was published in the *Journal and Courier* on January 26, 2010 and February 3, 2010. A copy of the Legal Notice was also sent to property owners within the project corridor. The advertised date for the hearing was Wednesday February 10, 2010 with an open house at 6 p.m. and formal presentation at 7 p.m. in the City Council Chambers of the West Lafayette City Hall at 609 West Navajo Street, West Lafayette, Indiana. The hearing was held in compliance with Title 23, Code of Federal Regulations, Section 771.11 (h) entitled *Early Coordination, Public Involvement and Project Development* and the INDOT *Public Involvement Procedures* approved by the Federal Highway Administration, U.S. Department of Transportation on April, 1, 2009.

The Level 3 Categorical Exclusion Document, approved for public involvement, was available for public review and comment prior to the hearing at the following locations:

- City of West Lafayette Public Library,
208 West Columbia Street
West Lafayette, Indiana 47906
- City of West Lafayette City Hall,
609 West Navajo Street
West Lafayette, Indiana 47906

One verbal public comment was recorded at the Public Hearing and seven (7) additional written comments were received after the close of the hearing.

Four of the seven written comments received questioned the need for a roundabout at the intersection of Yeager Road and Northwestern Avenue (US 52) because they are potentially dangerous for motorists who do not normally or have never driven through a roundabout. Other concerns included loss of parking for the ASIA Market, cost of a roundabout compared to a signalized intersection, and expediting the residential relocation.

Copies of all Public Hearing documents can be found in Appendix G of this document. These documents include: the Legal Notice of Public Hearing, PowerPoint presentation slides, Public Hearing Information Packet, the Public Hearing Transcript, and Public Comments and Responses.

On March 8, 2010, the Indiana Department of Transportation issued a *Certification of Public Hearing Requirements and Socio-Economic Ecological and Environmental Evaluations* by signing title page of the environmental document.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks:

No opposition or concerns regarding the proposed road project along Yeager Road in West Lafayette, Indiana have been indicated.

Opportunity to hold Public Hearing not Required	
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Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: City of West Lafayette INDOT District: Crawfordsville
 Local Name of the Facility: Yeager Road

Funding Source: Federal State Local Private

PURPOSE AND NEED:

Describe the problem that the project will address.

The primary purpose of the proposed project is to provide system linkage, sufficient capacity, and safe travel for motorist, bicycles and pedestrians along Yeager Road. A secondary purpose of the project is to address the deteriorating pavement and upgrade the storm drainage system.

It is necessary to improve the capacity, deteriorating pavement and upgrade the storm drainage system because Yeager Road is a main north-south corridor in West Lafayette. Yeager Road/Northwestern Avenue is one of only three major north-south corridors in West Lafayette, with the other two being Salisbury Street and Soldiers Home Road/Happy Hollow Road/River Road. Yeager Road connects a federal highway, US 231 (Northwestern Avenue), with another federal highway, US 52 (Sagamore Parkway West) and provides two of the US 52 and US 231 interchange movements (NB to EB and WB to SB). Further improvements to Yeager Road north of Sagamore Parkway West are shown in the Tippecanoe County Area Plan Commission's (TCAPC) 2025 Plan.

Yeager Road is a main thoroughfare for Purdue University football and basketball traffic. Added capacity along Yeager Road is needed to facilitate traffic during home football and basketball games.

It is also necessary to address safety concerns in the project area. Crash data collected from 2006, 2007, and half of 2008, identified 33 crashes within 500 feet of the intersection of Yeager Road and Northwestern Avenue. Upon studying the crash data and considering the factors involved in 82% of the crashes, the need to reduce vehicle speeds and improve intersection geometry were identified.

In addition, multi-residential properties and student housing are located within the project area. Because of the large student population residing in the area, there is a need for safe pedestrian and bicycle multi-use paths to provide additional access to and from campus and retail centers, as well as the recreational benefit that a multi-use path would provide. The multi-use path would also provide bicyclists and pedestrians with vital links to other planned Trails/Greenway projects identified in the 2009 TIP for the Tippecanoe County Area Plan Commission.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Tippecanoe
 Municipality: West Lafayette

Limits of Proposed Work: Yeager Road between the intersections with US 231 and US 52.

Total Work Length: 1,300 ft.

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

Yes¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

If yes, when did the FHWA grant a conditional approval for this project?

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

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In the Remarks box below, describe in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

The City of West Lafayette is planning a project which involves the widening and reconstruction of Yeager Road. Yeager Road is located on the west side of West Lafayette, Tippecanoe County Indiana. The project limits are from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52), a distance of approximately 1,300 feet. The project will include Transportation System Management (TSM) improvements, which include providing and improving drainage, maintaining the existing center two way left turn lane (TWLTL) for adjacent property access, providing one additional through lane in each direction, along with a multi-use path/trail or sidewalk for pedestrians.

Yeager Road is currently a three-lane road with two through lanes and one center two way left turn lane. Yeager Road and Sagamore Parkway (US 52) intersect at a four-way stop light controlled intersection. Yeager Road and Northwestern Avenue (US 231) intersect at a three-way stop light controlled intersection. The existing road cross section does not include curb and gutters or sidewalks/multi-use paths.

This road reconstruction (non-freeway) project will include specific improvements as follows. The existing three lane configuration will be reconstructed adding two additional through lanes, one in each direction. The center two way left turn lane will be maintained, as there are numerous private driveways along the project corridor. The road cross-section will thus include 4 thru lanes 12 feet wide and one two way left turn lane, 16 feet wide.

A sidewalk will be constructed on the east side of Yeager Road and a multi-use path/trail will be constructed on the west side of the road way. Curbs and gutters will be added to Yeager Road to improve drainage along the roadway.

A modern roundabout will be constructed at the existing intersection of Northwestern Avenue (US 231) and Yeager Road. The existing signalized intersection is skewed causing delay at the intersection and impaired sight distance. The existing Yeager Road intersection with Northwestern Avenue (US 231) will require reconstruction to accommodate the added travel lanes and improve the substandard skew (22.6°) and curvature (245 ft radius) to meet design standards.

See Appendix B for mapping of the project area. Project Plans are included in Appendix D.

OTHER ALTERNATIVES CONSIDERED:

Describe alternatives considered, including the Do-Nothing Alternative and an explanation of why each non-preferred alternative was not selected.

Alternative A: (The Do Nothing Alternative) would leave the existing road as it currently is. This alternative would not correct the safety hazards present due to large volumes of traffic. This roadway is heavily used during college events and is an important route for visitors and therefore this option was not chosen. This alternative was not chosen because it does not meet the purpose and need for the proposed project.

Alternative B:

This alternative includes reconstructing the roadway with full-depth pavement. With reconstruction, the roadway cross section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. No travel lanes will be added as a part of this alternative. For non-motorized travel, sidewalks with trail connections will be constructed. No reconfiguration of either intersection is proposed as a part of this alternative. This alternative was not chosen as it did not meet the capacity deficiencies as noted in the purpose and need of the project.

Alternative C:

This alternative involves the addition of two travel lanes in each direction to create a 5-lane cross section. This alternative also includes the improvements to storm drainage facilities and the addition of concrete curb and gutters. Sidewalks with trail connections will also be constructed. This alternative involves no improvements to the intersections of Yeager Road and U.S. 52 or Yeager Road and Northwestern Avenue. This alternative was not chosen as it did not fully meet the capacity deficiencies present as noted in the purpose and need for the project.

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The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems, or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe)

X
X
X
X

ROADWAY CHARACTER:

Functional Classification:	Urban Arterial Intermediate						
Current ADT:	17,290	VPD 20(08)	Design Year ADT:	21,100	VPD 20(28)		
Current Year DHV	not available	Trucks	2%	Design Year DHV	1,899	Trucks	2%
Designed Speed (mph):	35	Legal Speed (mph):	30				

	Existing	Proposed
Number of Lanes:	3	5
Type of Lanes:	Through, TWLTL	Through, TWLTL
Pavement Width:	38 ft.	68 ft.
Shoulder Width:	N/A ft.	2 ft.
Median Width:	TWLTL; 12 ft.	TWLTL; 16 ft.
Sidewalk Width:	N/A ft.	Left 10; Right 5 ft.

Setting:	<input type="checkbox"/> Urban	<input checked="" type="checkbox"/> Suburban	<input type="checkbox"/> Rural
Topography:	<input checked="" type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input type="checkbox"/> Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

DESIGN CRITERIA FOR BRIDGES:

Structure Number(s): NA Sufficiency Rating: NA

	Existing	Proposed
Bridge Type:		
Number of Spans:		
Weight Restrictions:	ton	ton
Height Restrictions:	ft.	ft.
Curb to Curb Width:	ft.	ft.
Outside to Outside Width:	ft.	ft.
Shoulder Width:	ft.	ft.
Length of Channel Work:	ft.	ft.

Describe bridges and structures; provide specific location information for small structures.

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Remarks: No bridges or structures are present in the project area and no bridges or structures are proposed for this project.

	Yes	No
Will the structure be rehabilitated or replaced as part of the project?	<input type="checkbox"/>	<input type="checkbox"/>

If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: Maintenance of traffic will consist of phasing the construction into four phases. Phase I road reconstruction will begin on the east half of Yeager Road while traffic is maintained within two lanes on the west, utilizing the southbound lane and the two way left turn lane (TWLTL). Access to Yeager Road from Northwestern Avenue and Sagamore Parkway will be maintained. In addition, access to residences, business, and local streets will be maintained during road construction. Phase II construction will shift to the southbound lanes; therefore, traffic will be shifted to the east using the northbound lane and the TWLTL. Access to Yeager Road from Sagamore Parkway and Northwestern Avenue will be maintained as well as access to residences and businesses. Phase III will consist of construction on the roundabout at the intersection of Northwestern Avenue and Yeager Road. All lanes on Yeager Road will be open for traffic. Traffic on Northwestern Avenue will use the existing northbound lanes for north and southbound traffic. Traffic through the intersection traveling north will curve around construction following the future path of the roundabout. All access will be maintained during Phase III. Access to Northwestern Avenue from Yeager Road will be maintained and access from Northwestern onto Yeager Road will also be maintained. Phase IV of construction will consist of incidental work within the roundabout and proposed medians along Northwestern Avenue. Final pavement markings will be added. Traffic will utilize the proposed roundabout and all access will be open.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 301,995 (2011) Right-of-Way: \$ 1,072,000 (2011) Construction: \$ 1,268,000 (2011)
 Anticipated Start Date of Construction: Fall 2011

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.15	0.19
Commercial	1.21	0.53
Agricultural	---	---
Forest	---	---
Wetlands	---	---
Other: Golf course	0.18	0.00
Other:	---	---
Other:	---	---
TOTAL	1.54	0.72

Remarks: Approximately 1.54 acres of additional permanent right-of-way will be needed from 15 parcels for this project. The right-of-way will extend 50 feet from the centerline on the east and west side of Yeager Road. Existing right-of-way extends approximately 20 feet from the centerline on both sides of Yeager Road. The right of way to be acquired will be primarily commercial and residential. Approximately 0.72 acres of temporary right-of-way will be required for grading, driveway construction, and path/sidewalk construction. One residential and one commercial relocation are anticipated to be required as a part of the proposed project. The addresses are as follows:

- Allstate Insurance Agency 2306 Northwestern Avenue (commercial relocation)
- 2331 Yeager Road (residential relocation)

All right-of-way will be acquired in accordance with the Uniform Relocation Act and other applicable federal and state procedures.

Right-of-way plans are included in Appendix D.

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Jurisdictional Ditches	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State Wild, Scenic or Recreational River	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: A site visit was conducted on April 29, 2008. No streams, rivers, watercourses or jurisdictional ditches were identified during the field investigation. See Appendix F.

	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
Other Surface Waters	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Reservoirs	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Lakes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: No other surface waters were noted in the project area. For reference see Appendix F.

Wetlands	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Total wetland area: 0 acre(s) Total wetland area impacted: 0 acre(s)
 (If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments

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Wetlands	<u>Documentation</u>		<u>OES Approval Dates</u>
	Yes	No	
Wetland Determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Wetland Delineation Report	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
USACE Isolated Waters Determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Mitigation Plan	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

	<u>Individual Wetland Finding</u>	
	Yes	No
Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):		
Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>	<input type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>	<input type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>	<input type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>	<input type="checkbox"/>
The project not meeting the identified needs.	<input type="checkbox"/>	<input type="checkbox"/>

Measures to avoid, minimize and mitigate wetland impacts need to be discussed in the remarks section

Remarks: Per coordination with the United States Fish and Wildlife Service (USFWS), this project will have no effect on wetlands or other significant habitat types. A site visit was conducted on April 29, 2008, to identify wetlands or other "waters of the US." No wetlands or other "waters of the US" were identified within the project area. See Appendix C for coordination with the USFWS. An Ecological Site Assessment is included in Appendix F.

Terrestrial Habitat	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<i>Use the remarks table to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).</i>				
Remarks:	<p>Per coordination with the United States Fish and Wildlife Service (USFWS), no natural habitat types will be impacted by the proposed project. Land-use in the project area is residential/ urban, where limited terrestrial habitat exists. The habitat in the project area is not unique or high quality. See Appendix C for coordination with the USFWS.</p> <p>The project requires 1.54 acres of additional permanent right of way. Land use is commercial and residential. The project area consists of maintained lawn areas. There are scattered trees throughout the project area, which are mostly ornamental trees for landscaping purposes. Approximately 0.99 acre of maintained lawn area will be impacted by the proposed project.</p>			

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Does the proposed project involve the Karst Region of Indiana?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Use the remarks table to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

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Remarks:

The karst region of Indiana is characterized by the presence of limestone or other soluble bedrock, where drainage has been diverted underground. In these areas, features such as sinkholes, sinking streams, springs, and caves dominate the surface and subsurface landforms. In order to protect these features, and the wildlife uniquely associated with karst features (e.g., blind cave fish), a Memorandum of Understanding was signed in 1993 by INDOT, IDNR, IDEM, and the US Fish and Wildlife Service. Pursuant to the 1993 Karst MOU, transportation projects within karst areas of Indiana require specific evaluation and assessment. Counties exhibiting karst topography in Indiana include: Putnam, Morgan, Owen, Monroe, Greene, Martin, Lawrence, Orange, Dubois, Washington, Floyd, Harrison, and Crawford.

The proposed project is located in Tippecanoe County, which is not located in the Karst Region of Indiana as defined by the 1993 Karst Memorandum of Understanding. No karst features were identified during the field investigation on April 29, 2008. A Karst Features Assessment is not required.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Threatened or Endangered Species				
Within the known range of any federal species?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Any critical habitat identified within project area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Federal species found in project area (based upon informal consultation)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is Section 7 formal consultation required for this action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

In correspondence with the U.S. Fish and Wildlife Service (May 13, 2008), the proposed project area is within the range of the federally endangered Indiana Bat (*Myotis sodalis*) and the bald eagle (*Haliaeetus leucocephalus*), although the project is not likely to adversely affect either species. The bald eagle has been removed from the federal endangered species list; however the species is still protected under the Bald and Golden Eagle Protection Act. The US Fish and Wildlife Service states that there is no critical habitat for the above mentioned species within the project area. Critical habitat is defined as having required habitat for endangered, threatened or rare species as well as a documented modern, post 1970 occurrence. The project area has no documented occurrence related to any endangered species and provides no specific habitat for endangered species identified by the *Endangered Species Act of 1973* known to Indiana.

In correspondence with the Department of Natural Resource (July 23, 2008), the Natural Heritage Program database indicated no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

SECTION B – OTHER RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Drinking Water Resources				
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Public Water System(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wellhead Protection Area	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks:

According to the IDEM Groundwater Section, the proposed project is not located within a Wellhead Protection Area. See Appendix C for coordination with IDEM Groundwater Section.

In Indiana, one Sole Source Aquifer has been designated by the USEPA, namely the St. Joseph Aquifer system located in portions of St. Joseph, Elkhart, Kosciusko, Noble, and LaGrange Counties, in northern Indiana. The proposed project is located in Tippecanoe County; therefore, the project is not located within the area of the St. Joseph Sole Source Aquifer. The FHWA/EPA Sole Source Aquifer MOU is not applicable to this project, and a Groundwater Impact Assessment is not required.

Public water and sewer systems are present within the project area. No impact to either system is anticipated as a part of this project. A sewer force main adjacent to Yeager Road will be relocated by the City of West Lafayette as part of a separate project but will occur simultaneously with construction along Yeager Road.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Flood Plains				
Longitudinal Encroachment	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Transverse Encroachment	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the project located in a FEMA designated floodplain?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project.	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

The project area is not located in nor does it cross any FEMA designated floodplains. For reference to floodplain mapping see Appendix F.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Farmland				
Agricultural Lands	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NRCS-CPA-1006 Form scored ≥ 160 ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Provide the NRCS score and state whether there is a significant loss of farmland as a result of the project in the remarks section.

Remarks:

As required by the Farmland Protection Policy Act, the NRCS has been coordinated with and according to the NRCS this project will not cause a conversion of prime farmland. The project area is largely suburban-urban with no agricultural land. No other alternatives other than those already discussed in this document will be considered without a re-evaluation of the project's potential impacts upon farmland. This project will not have a significant impact to farmland. See Appendix C for coordination with the NRCS

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SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates
Minor Projects PA Clearance			

Eligible and/or Listed
Resource Present

Results of Research

	Yes	No
Archaeology	<input type="checkbox"/>	<input checked="" type="checkbox"/>
History/Architecture	<input checked="" type="checkbox"/>	<input type="checkbox"/>
NRHP Buildings/Site(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NRHP District(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
NRHP Bridge(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Project Effect

	Yes	Not Applicable	SHPO/OES/FHWA Approval Dates
No Historic Properties Affected	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
No Adverse Effect	<input checked="" type="checkbox"/>	<input type="checkbox"/>	OES: 08/07/2009; SHPO: 09/02/2009
Adverse Effect	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

Documentation Prepared

Documentation

	Yes	Not Applicable	SHPO/OES/FHWA Approval Dates
Historic Properties Short Report	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Historic Property Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>	INDOT: 08/06/2008; SHPO: 11/07/2008
Archaeological Records Check/ Review	<input checked="" type="checkbox"/>	<input type="checkbox"/>	INDOT: 5/22/2009 (AI); SHPO: 7/13/2009 (AI)
Archaeological Phase Ia Survey Report	<input checked="" type="checkbox"/>	<input type="checkbox"/>	INDOT: 9/11/2008; SHPO: 10/15/2008
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input checked="" type="checkbox"/>	INDOT: 9/11/2008; SHPO: 10/15/2008
Archaeological Phase II Investigation Report	<input type="checkbox"/>	<input checked="" type="checkbox"/>	INDOT: 5/08/2009 (AI); SHPO 07/13/2009 (AI)
Archaeological Phase III Data Recovery	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
APE, Eligibility and Effect Determination	<input checked="" type="checkbox"/>	<input type="checkbox"/>	INDOT: 08/07/2009; SHPO: 09/02/2009
800.11 Documentation	<input checked="" type="checkbox"/>	<input type="checkbox"/>	INDOT: 08/07/2009; SHPO: 09/02/2009
Memorandum of Agreement	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

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Remarks:

Determination of the Area of Potential Effect (APE): The Area of Potential Effects (APE) has been drawn to encompass properties on all sides of the undertaking with a view of the improvements. All parcels adjacent to the project area are included in the APE. A map of the APE is included in the Section 106 Documentation in Appendix D.

Archaeology: Archeological Consultants of Ossian completed an Archaeological Field Reconnaissance of the proposed Yeager Road Improvement project on August 27, 2008. No archaeological sites were located during the field reconnaissance. The Archaeological Field Reconnaissance Report concluded that no properties on or eligible for listing on the National Register of Historic Places will be affected by the proposed project. The Field Reconnaissance Report was concurred with by INDOT-CRS on 9/11/2008 and by the IDNR-DHPA on 10/15/2008. Additional right-of-way was added to the project area and an additional Archaeological Field Reconnaissance Report was prepared for the additional area. The report was concurred with by INDOT-CRS on 05/08/2009 and by SHPO on 07/13/2009. No additional sites on or eligible for listing on the National Register of Historic Places will be affected by the proposed project.

Historic Properties: A Historic Properties Report (HPR) was prepared by Weintraut & Associates Historians, Inc. in July 2008 for the proposed Yeager Road Improvement Project. Historic properties were identified and evaluated in accordance with current Section 106 federal regulations. One property was recommended as eligible for listing on the National Register of Historic Places, the Masonic Temple, located east of the intersection of Yeager Road and Northwestern Avenue. Five additional properties within the Area of Potential Effect (APE) were evaluated and determined ineligible for listing on the National Register due to lack of significance and loss of integrity. INDOT-CRS concurred with the HPR on August 6, 2008. The HPR was sent to SHPO on October 27, 2008. SHPO concurred with the eligibility of the Masonic Temple on November 7, 2008. A HPR- Additional Information (AI) was prepared in March 2009 because additional right-of-way was added to the project area. An additional five properties were identified as 50 years old or older; however, none were considered eligible for listing in the National Register of Historic Places. The HPR –Additional Information was concurred with by INDOT on May 22, 2009 and IDNR-DHPA on July 13, 2009.

Coordination with Consulting Parties:

An invitation to consulting parties and a request for participation in the Section 106 process was provided to federal, state, and local agencies on October 28, 2008. A copy of the INDOT approved Historic Properties Report was included with the invitation letter. Those agencies were invited to be consulting parties and participate in the development of the project in accordance with provisions of Section 106 of the NHPA. No comments or requests to participate were received during the 30-day comment period. One request to participate was sent in February 2009 from the Historic Landmarks Foundation of Indiana and is included in the list of consulting parties.

The following is a list of organizations and individuals invited to be consulting parties. If no response was received after 30 days, it was assumed the parties involved did not wish to act as consulting parties.

Organization/Name	Response
State Historic Preservation Officer (DNR)	Participant 11/07/2008
Indiana Historical Society	No response
Tippecanoe County Historical Society	No response
Mr. Paul Schueler, Tippecanoe County Historian	No response
Historic Landmarks Foundation of Indiana, Western Regional Office	Requested participation 02/18/2009
Mayor, City of West Lafayette	No response

Public Involvement: A public notice was published in the West Lafayette Journal and Courier on August 19, 2009 advertising the finding and requesting comments. All comments were requested by September 19, 2009. The proof of publication and all comments received are included in Appendix G.

The State Historic Preservation Officer and the one consulting party (Historic Landmarks Foundation of Indiana Western Regional Office) were supplied the HPR and Section 106 Documentation and Finding on August 10, 2009. The Historic Landmarks Foundation of Indiana agreed that the proposed project will not have an adverse effect on the qualifying characteristics of the Masonic Temple.

Documentation, Findings: The Section 106 APE Determination and the Finding of No Adverse Effect were approved by INDOT on August 7, 2009. The Documentation was supplied to SHPO and consulting parties for review on August 10, 2009. On September 2, 2009, the State Historic Preservation Officer (SHPO) concurred with the INDOT finding.

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If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, construction in the immediate area of the find will be stopped, and the INDOT Cultural Resources Section, FHWA, and the IDNR-SHPO will be notified immediately.

The Section 106 process has been completed, and the FHWA responsibilities for Section 106 have been fulfilled. See Appendix D for the Section 106 Documentation and Findings.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>		<u>Use</u>		<u>FHWA / OES Approval/dates</u>
	Yes	No	Yes	No	
Parks & Other Recreational Land					
Publicly owned park	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Publicly owned recreation area	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Programmatic Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Other (school, state/national forest, bikeway, etc.)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>		FHWA: 11/23/2009; OES: 11/18/2009
“De minimis“ Impact	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>		FHWA: 11/23/2009; OES: 11/18/2009

	<u>Presence</u>		<u>Use</u>		<u>FHWA / OES Approval/dates</u>
	Yes	No	Yes	No	
Wildlife & Waterfowl Refuges					
Federal					
National Wildlife Refuge	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
State					
State Fish & Wildlife Area – recreation or refuge areas only	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Programmatic Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
“De minimis“ Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>			

	<u>Presence</u>		<u>Use</u>		<u>FHWA / OES approval/dates</u>
	Yes	No	Yes	No	
Historic Properties					
Sites eligible and/or listed on the NRHP	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Programmatic Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
“De minimis“ Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>			

Discuss Programmatic Section 4 (f) and De minimis Section 4(f) impacts in the remarks section below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, De minimis and

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Individual Section 4(f) documents please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks: The proposed project will convert a strip of land that is currently part of the Kampen Golf Course to transportation use resulting in a Section 4(f) take. Kampen Golf Course is owned by the Purdue Research Foundation (PRF) and is part of the Birck Boilermaker Complex. The anticipated impacts will occur in the area between the existing right-of-way of Northwestern Avenue and the landscaping mounds running parallel to Northwestern Avenue, an area typically not used by golfers. A project description and project plans were supplied to PRF on October 12, 2009 for their review and comments. PRF stated that they have no objection to the encroachment that is identified as being onto the outer limits of Kampen Golf Course along Northwestern Avenue.

The proposed project will have a 'de minimis' impact to the Kampen Golf Course, a recreational resource protected under Section 4(f). The proposed project will not disrupt the function and use of the golf course and would therefore fall under a 'de minimis' impact to the resource. The Section 4(f) Documentation was submitted to INDOT on October 21, 2009 and approved by INDOT OES on 11/18/2009. The 'de minimis' impact was approved by FHWA on 11/23/2009. A copy of the report is included in Appendix I.

Section 6(f) Involvement Yes No Yes No

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks: No Section 6(f) resources were identified by DNR's Division of Outdoor Recreation in a letter dated June 3, 2008 or by site inspection. The project will not involve any properties acquired by or improved with the Land and Water Conservation Fund. Coordination with DNR Division of Outdoor Recreation is included in Appendix C.

SECTION E – AIR QUALITY

<u>Air Quality</u>	Yes	No
Conformity Status of the Project		
Is the project in an air quality non-attainment or maintenance area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input type="checkbox"/>	<input type="checkbox"/>
If NO then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>
Is an MSAT analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: This project is located in Tippecanoe County. This county is currently not listed as a non-attainment or maintenance area for any pollutants, and therefore does not require an air quality analysis. The project is included in the Tippecanoe Area Plan Commission FY 2009 Transportation Improvement Plan (TIP). An excerpt from the TIP is included in Appendix H of this report.

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SECTION F – NOISE

Noise	Yes	No
Is a noise analysis required in accordance with FHWA regulations and INDOT's noise policy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	No	Yes/ Date
OES Approval of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: This project is not a Type 1 project. In accordance with 23 CFR 772 and the INDOT Traffic Noise Policy (approved on February 2007), this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

This project is included in the FY 2009 Transportation Improvement Program of the Tippecanoe County Area Plan Commission (TCAPC). Yeager Road from US 231 (Northwestern Ave) to US 52 (Sagamore Parkway West) was recognized as heavily congested and functions as part of the interchange between US 52 and US 231.

The multi-use path will provide bicyclists and pedestrians with vital links to other planned Trails/Greenway projects identified in the 2009 TIP for the Tippecanoe County Area Plan Commission.

No work shall occur during the Purdue University home football games as this is a time of increased vehicular and pedestrian traffic. Intermediate construction dates will be set in order to keep all travel lanes open during the Purdue football season. The City of West Lafayette requested that all travel lanes be open and unobstructed during home football game weekends, Friday through Sunday (See Section J).

Indirect and Cumulative Impacts	Yes	No
Will the proposed action result in substantial indirect or cumulative impacts?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The proposed project is not likely to result in indirect or cumulative impacts to the community. The project will add capacity to an already developed area.

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Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public utilities, fire, police, emergency services, religious institutions, public transportation or pedestrian and bicycle facilities?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

No road closures are anticipated during construction of the proposed project. At a minimum, two lanes shall remain open to through traffic. Traffic will be maintained to provide access to all approach streets, businesses, and residences during construction. At a minimum, local access will always be provided throughout the duration of the project. During the Purdue football season, all travel lanes shall be open for the increased traffic volumes.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?
 Are any EJ populations located within the project area?
 Will the project result in adversely high or disproportionate impacts to the EJ population?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

According to the 2000 US Census Bureau information, the proposed project is within one block group, Block Group 5, Census Tract 52 (Affected Community, AC). The extreme ends of the project are within separate block groups. An Environmental Justice population exists if the low-income population or minority population is 25 percent higher than the population in the community of comparison. See Appendix J for the Environmental Justice Analysis for the proposed project.

The Affected Community (Block Group 5) has a higher than average percentage of racial minorities, Hispanic and Latino populations in comparison to the City of West Lafayette (Community of Comparison, COC). One hundred and twenty-five percent of the population of minorities in the COC is 23.1 percent and the percent of the population of minorities in the AC is 32 percent. Therefore the percent population of minority populations in the AC exceeds the minimum of 25 percent higher than the COC threshold.

The AC does not contain a higher than average low income population. One hundred and twenty-five percent of the low income population of the COC is 47.9 and the percent population of low income in the AC is 35.1 percent. The low income population in the AC does not exceed the threshold of 25 percent greater than the COC's population of low income.

The proposed project will have minimal ecological, cultural, community and human health impacts. The project area is dominated by maintained grass and small landscape trees and shrubs which do not provide significant habitat for wildlife. In addition no structures that are listed or eligible for listing on the National Register for Historic Places will be impacted by the proposed project. No additional sources of pollution will arise from the proposed project and a decrease in air toxins may result due to the decrease in traffic congestion. The proposed project will not impact community cohesion or connectivity.

The proposed right of way acquisition and the two anticipated relocations (See 'Relocation of People, Businesses or Farms') associated with this project will not result in significant and disproportionate impacts to the affected community as defined by in EO 12898. The proposed project will benefit the community by providing a safe corridor for pedestrians and bicyclists as well as provide an improved roadway corridor, improved storm water management, and an improved intersection at Northwestern Avenue and Yeager Road.

Relocation of People, Businesses or Farms:

Will the proposed action result in the relocation people, businesses or farms?
 Is a business needs survey required?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Number of relocations: Residences: 1 Businesses: 1 Farms: Other:

If a business information survey or Conceptual Stage Report is required, discuss the results in the Remarks section.

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Remarks: One business and one residential relocation are anticipated as a result of the proposed road project. The business, Allstate Insurance Agency is located north of the intersection of Yeager Road and Northwestern Avenue. The business is located within a converted residential one-story building. The residential property is located at 2331 Yeager Road, just north of the Allstate Insurance Company. The house is a two-story structure with two detached outbuildings. Neither property was identified as listed or eligible for listing on the National Register of Historic Places.

The existing stop light controlled intersection of Yeager Road and US 231 known locally as Northwestern Avenue will be replaced with a modern roundabout. A structure eligible for listing on the National Register of Historic Places is located in the northeast corner of the intersection of US 231 and Yeager Road, the Masonic Lodge. To avoid any impacts to the Masonic Lodge, relocations north of the intersection of Yeager Road and Northwestern are necessary for the proposed roundabout.

All right-of way and properties will be acquired in accordance with appropriate federal and state procedures. Those procedures include specific requirements for appraisals, review appraisals, and negotiations. Compliance with these procedures will assure fair and equitable treatment of affected owners. All acquisition activities will be accomplished under provisions of Uniform Relocation Assistance and Real Property Acquisition Act (49 CFR 24).

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

	Documentation	
	Yes	No
Red Flag Investigation	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Hazardous Materials Site Assessment Form	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Phase I Initial Site Assessment (ISA)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Phase II Preliminary Site Investigation(PSI)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Design/Specifications for Remediation required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	No	Yes/ Date
OES Review of Investigations	<input type="checkbox"/>	Not Applicable- LPA

Include a summary of findings for each investigation.

Remarks: A Red Flag Investigation was prepared for the proposed project by American Structurepoint, Inc on September 24, 2009. Based on the findings, additional investigations for hazardous materials (i.e. Phase I Initial Site Assessment) is warranted.

A search of nationwide and local database sources was provided for this report by IndianaMap. The following sites were found within a 0.5 mile search radius:

- Three Industrial Waste Sites
- Three Underground Storage Tank Sites
- Six Leaking Underground Storage Tank Sites

The Red Flag Investigation concluded that no further investigations are needed based on the geology, hydrology, or infrastructure surveys within the project area. However, A Phase 1a Initial Site Assessment is recommended for specific sites within the project area due to the potential for contaminated soils or other hazardous materials. Based on these findings, a Phase 1a Initial Site Assessment is warranted for the following sites.

- 1) CTS Corp
- 2) Grace Baptist Church
- 3) State Farm
- 4) Walgreens/Jiffy Lube
- 5) George Davis Manor
- 6) Amoco SS 10109 (BP Gas Station)
- 7) Shell Station

A copy of the Red Flag Investigation is included in Appendix E.

A Phase I ISA was performed for the project area to determine potential sources of hazardous materials that may affect the proposed project within the proposed right-of-way (ROW). Evaluation of the project study area included, but was not

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limited to, the following sources available: aerial photography, regulatory records, and field reconnaissance. The results of this investigation revealed one site as a recognized environmental condition (REC), namely, the BP Amoco station located at the southwest corner of Yeager Road and Sagamore Parkway. Further investigation of the soils and groundwater adjacent to this site is recommended prior to the proposed construction of the Yeager Road improvements. A copy of the text of the report can be found in Appendix E.

A copy of the Phase I ISA was provided to The City of West Lafayette on March 15, 2010.

SECTION I – PERMITS CHECKLIST

	<u>Required</u>	<u>Not Required</u>
Army Corps of Engineers (404/Section10 Permit)		
Individual Permit (IP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDEM		
Section 401 WQC	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Rule 5	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDNR		
Construction in a Floodway	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Navigable Waterway Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mitigation Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
US Coast Guard Section 9 Bridge Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Others (Please discuss in the Remarks section below)	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: A Rule 5 Permit will be needed for the proposed project. No other permits are required for the proposed project.

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SECTION J- ENVIRONMENTAL COMMITMENTS

Information below must be included on Commitments Summary Form. List all commitments, indicating which are firm and which are optional.

Remarks:

Firm Commitments

1. If permanent of temporary right of way amounts change, the INDOT Office of Environmental Services must be contacted immediately. [INDOT OES]
2. Any work in a wetland area within INDOT's right of way or in borrow/waste areas is prohibited unless specifically allowed in the US Army Corps of Engineers of IDEM permit. [INDOT OES]
3. If any archaeological artifacts or human remains are uncovered during construction, federal law and regulations (16 USC 470, et seq.; 36 CFR 800.11, et al.) and State Law (IC 14-21-1) require that work must stop immediately and that the discovery must be reported to the Division of Historic Preservation and Arhcaeology in the Indiana Department of Natural Resources within 2 business days. [INDOT OES and IDNR]
4. If any potential hazardous materials are discovered during construction the IDEM Spill Line should be notified with details of the discovery within 24 hours. INDOT Office of Environmental Services, Hazardous Materials Unit should then be contacted to organize the proper handling of the material to be in accordance with IDEM guidelines. [INDOT OES]
5. Projects involving construction activity, which includes clearing, grading, excavation, and other land disturbing activities, that results in the disturbance of one or more acres of total land area, require a Rule 5 Storm Water Runoff Permit. [IDEM]
6. Vegetative wastes must be taken to a registered yard waste composting facility or the waste must be chipped or shredded with composting on-site. You must register with IDEM if more than 2,000 pounds is to be composted. [IDEM]
7. Precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. [IDEM]
8. All facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposed) must be inspected by an Indiana –licensed asbestos inspector prior to the commencement of any renovation or demolition activities. [IDEM]
9. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent oil distillate, is prohibited during the months of April through October. [IDEM]
10. In all cases where a demolition will occur, the owner or operator must notify IDEM 10 working days prior to the demolition using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf> [IDEM]
11. If the site is found to contain any areas used to dispose of solid or hazardous waste, the IDEM Office of Land Quality must be contacted at 317-308-3103. [IDEM]
12. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. [IDEM]
13. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. [IDEM]
14. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion. [DNR]

Items for Further Consideration

- Plant five trees (at least two inches in diameter at breast height) for each tree removed that is ten inches or greater in diameter at breast height.
- During Purdue University football season, all travel lanes on Yeager Road, Northwestern Avenue, and Sagamore Parkway must remain open and unobstructed during home football game weekends, Friday through Sunday [City of West Lafayette].

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SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Remarks:

Agency	Date of Response and Summary of Contents
US Fish and Wildlife Service	<p><u>May 13, 2008</u></p> <ul style="list-style-type: none"> • The project, as proposed, will have no effect on wetlands or other significant habitat types. • This precludes the need for further consultation as required under Section 7 of the Endangered Species Act of 1973.
Department of Natural Resources Division of Water	<p><u>May 8, 2009</u></p> <ul style="list-style-type: none"> • No formal approval by the DNR under the regulatory programs administered by the Division of Water is required for this project. • No plant or animal species listed as state or federally threatened, endangered or rare have been reported to occur in the project vicinity. • Fish, wildlife, and botanical resources losses as a result of this project should be minimal.
US Coast Guard	<p><u>May 22, 2008</u></p> <ul style="list-style-type: none"> • No jurisdiction is held by the Coast Guard on the project area for bridge administration purposes. • A Coast Guard permit is not required.
Natural Resource Conservation Service	<p><u>May 13, 2008</u></p> <ul style="list-style-type: none"> • The project will not cause a conversion of prime farmland.
Tippecanoe County Sheriff's Office	<p><u>May 22, 2008</u></p> <ul style="list-style-type: none"> • The project timeline should accommodate the Purdue University football season for safety reasons regarding pedestrians, drivers, and workers.
US Environmental Protection Agency-Sole Source Aquifer Program	<p><u>May 28, 2008</u></p> <ul style="list-style-type: none"> • West Lafayette, Indiana is not located within a Sole Source Aquifer. • Safeguards should be put in place throughout construction to ensure that groundwater is not endangered. • Potential safeguards include: cautious disposal of hazardous materials, precautions for fueling large equipment, and utilizing "green infrastructure."
INDOT Office of Aviation	<p><u>May 8, 2008</u></p> <ul style="list-style-type: none"> • There is one airport near the proposed project area • The Purdue Airport is a public-use airport and is located approximately 12, 700 feet south of the proposed project site • The project should have no impact on the airspace or air navigation.
DNR Division of Recreation	<p><u>June 3, 2008</u></p> <ul style="list-style-type: none"> • There will no negative effect on any site currently envumbered under 6(f)3 through the federally sponsored Land and Water Conservation Fund (LWCF). • There are no LWCF property in the immediate vicinity; therefore there will be no taking of LWCG property out of outdoor recreational use.
Indiana Geological Survey	<p><u>June 13, 2008</u></p> <ul style="list-style-type: none"> • No unusual and/or problem geographic, geological, geophysical or topographic features exist within the project limits. • No mineral resources have been identified within the area. • No active or abandoned mineral resources extraction sites are located nearby.

This is page 22 of 24. Project name: Yeager Road Reconstruction Date: 03/15/2010

Indiana Department of Transportation

County Tippecanoe Route Yeager Rd. Des. No. 0600696 Project No. _____

<p>Indiana Department of Environmental Management Automatic Response</p>	<p><u>May 6, 2008</u></p> <ul style="list-style-type: none"> • Section 404 of the Clean Water Act requires you obtain a permit from the US Army Corps of Engineers before discharging dredged or fill materials into any wetlands other waters, such as rivers or streams. • In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. • If the USACE determines that a wetland or other water body is isolated is regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's office of Water Quality is required for any activity that results in the discharge of dredged or fill materials. • If the project involves over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to waterbodies, seek input from the Office of Water Quality. • Work within the 100-Year floodway of a given water body is regulated by the department of Natural Resources, Division of Water. • The physical disturbance of a stream and riparian vegetation, especially large trees overhanging any affected water body should be limited to only that which is absolutely necessary to complete the project. • For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality- Watershed Planning Branch regarding the need for a Rule 5 Storm Water Runoff Permit. • Be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. If the project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM. • Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase and after completion of the project, to minimize the impacts associated with storm water runoff. • For project involving impacts to fish and botanical resources, contact the Department of Natural Resources- Division of Fish and Wildlife. • For projects involving water main reconstruction, water main extensions, and new public water supplies, contact the Office of Water Quality- Drinking Water Branch. • For Projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality-Permits Branch for the need for a National Pollutant Discharge Elimination System Permit. • For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality- Permits Branch. • The proposed project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. • For open burning and disposing of organic debris seek guidance from IDEM for open burning permits. • Preferably IDEM recommends taking vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site. • Minimize fugitive dust emissions. • Precautionary measure should be taken to avoid an outbreak of histoplasmosis, a disease which stems from bird or bat droppings. Be
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This is page 23 of 24. Project name: Yeager Road Reconstruction Date: 03/15/2010

Indiana Department of Transportation

County Tippecanoe Route Yeager Rd. Des. No. 0600696 Project No. _____

	<p>aware of this in areas where blackbirds, pigeons or bats have roosted for 3-5 years.</p> <ul style="list-style-type: none">• The US EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L.• All facilities slated for renovation or demolition may require inspections by an Indiana-licensed asbestos inspector prior to commencement of any renovation or demolition activities. Exceptions to this are available through IDEM's Section of Lead/Asbestos• In all cases where demolition activities will occur, the owner/operator must notify IDEM 10 working days prior to the demolition. A fee may be incurred.• IDEM encourages all efforts to minimize human exposure to lead-base paint chips and dust. IDEM's lead-based paint work practice standards, licensing and notification requirements may be required.• Ensure asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than 7% oil distillate, is prohibited during the months of April through October.• IF the project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality.• Contact the IDEM office of Land Quality if a site is found to contain any areas used to dispose of solid or hazardous waste.• All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility.• If any contaminated soils are discovered during the project, they maybe subject to disposal as hazardous waste.• If PCBs or asbestos are found at this site contact the Industrial Waste Section of the Office of Land Quality (IDEM).• If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact IDEM Underground Storage Tank program.
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Appendix A: INDOT Supporting Documentation
a. Threshold Chart

Categorical Exclusion Level Thresholds

	Level 1	Level 2	Level 3	Level 4
Relocations	None	≤ 2	> 2	> 10
Right of way¹	< 0.5 acres	< 10 acres	≥ 10 acres	≥ 10 acres
Length of added through lane	None	< 1 miles	≥ 1 mile	≥ 1 mile
Traffic pattern alteration	None	None	Yes	Yes
New alignment	None	None	< 1 mile	≥ 1 mile ²
Wetlands*	< 0.1 acres	< 1 acre	< 1 acre	≥ 1 acre
Section 4(f)	None	None	Programmatic/ <i>de minimis</i> Findings ³	Individual 4(f)
Section 6(f)	None	None	Any impacts	Any impacts
Section 106*	“No Historic Properties Affected” or falls within guidelines of Minor Projects PA	"No Adverse Effect"	“Adverse Effect”	If ACHP involved
Noise Analysis Required*	No	No	Yes ⁴	Yes ⁴
Threatened/Endangered Species*	"No Effect", or Falls within Guidelines of USFWS 9/8/93 Programmatic Response	“Not likely to Adversely Effect”	“Not likely to Adversely Effect”	“Likely to Adversely Effect” ⁵
Sole Source Aquifer Groundwater Assessment	Detailed Assessment Not Required	Detailed Assessment Not Required	Detailed Assessment Not Required	Detailed Assessment Required
Approval Level*				
• ESM ⁶	Yes	Yes	Yes	Yes
• OES			Yes	Yes
• FHWA			Yes	Yes

*These thresholds have changed from the March 2006 Manual.

¹Permanent and/or temporary right of way.

²If the length of the new alignment is equal to or greater than one mile, contact the FHWA’s Air Quality/Environmental Specialist.

³The FHWA must review and approve Programmatic and *de minimis* Section 4(f) prior to CE approval.

⁴In accordance with INDOT’s Noise Policy.

⁵If the project is considered Likely to Adversely Affect Threatened and/or Endangered Species, INDOT and the FHWA should be consulted to determine whether a higher class of document is warranted.

⁶Environmental Scoping Manager

In accordance with the <i>Categorical Exclusion Programmatic Agreement</i> between INDOT and FHWA, the following type of environmental documentation is needed:	
	No Additional Documentation: State-Funded Project. For projects that are 100% state-funded, and meets IDEM's approved list of Categorical Exempted Projects.
	No Additional Documentation: Categorical Exclusion, Level 1. The necessary supporting documentation, including maps and coordination, are attached to this document and will be kept on file in the district. If the project is approved as a CE-1 under Tables 2 or 3 in the CE Manual, provide the number or letter under which this CE-1 is approved. For projects not listed on Tables 2 or 3, but determined to be Level 1 CEs on other criteria, such as the threshold chart above, attach appropriate documentation.
X	Categorical Exclusion, Level 2 through 4 – The proposed action exceeds the thresholds for a CE-1 in the thresholds table above. The project must be documented on the Categorical Exclusion/Environmental Assessment Form. Additional research and documentation are necessary to determine environmental impacts and the type of environmental documentation.
	EA – An Environmental Assessment will be prepared.
	EIS – An Environmental Impact Statement will be prepared.

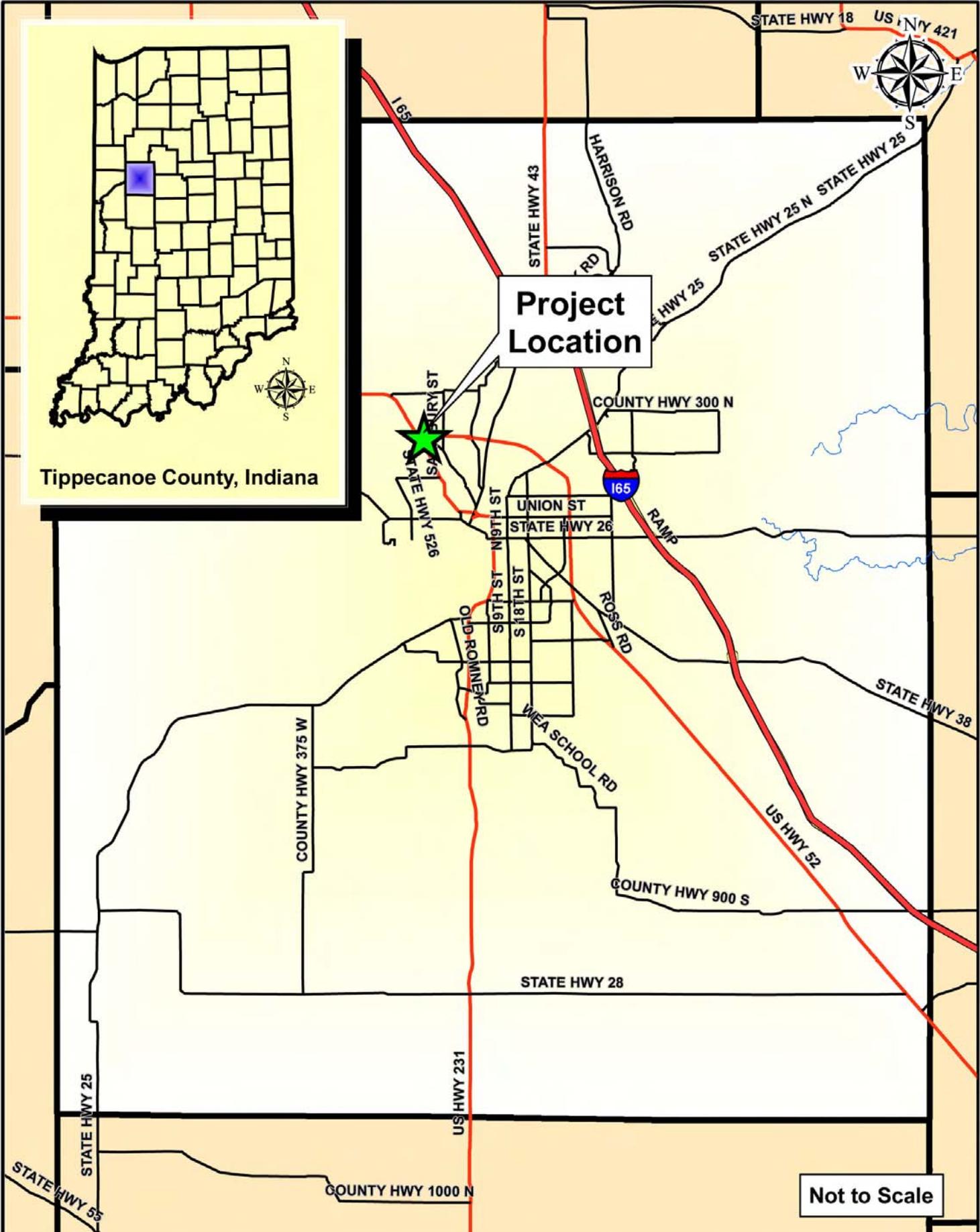
Appendix B: Graphics

- a. Project Location – State Map
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- h. Project Plans



Tippecanoe County, Indiana

Project Location



Not to Scale

Project Location Map



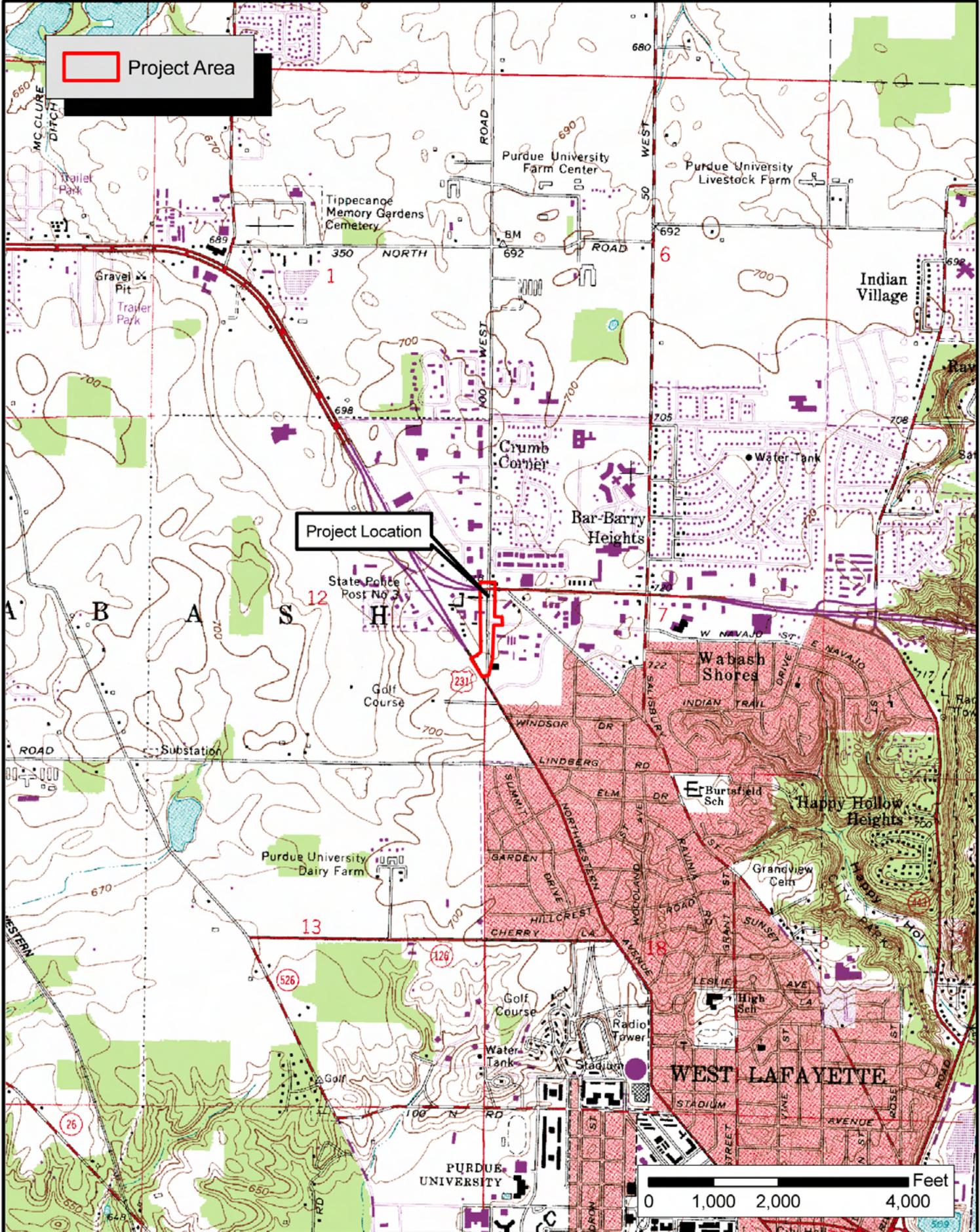
Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 04/18/2008

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USGS Topographic Mapping
West Lafayette Quadrangle

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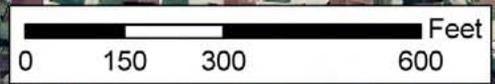
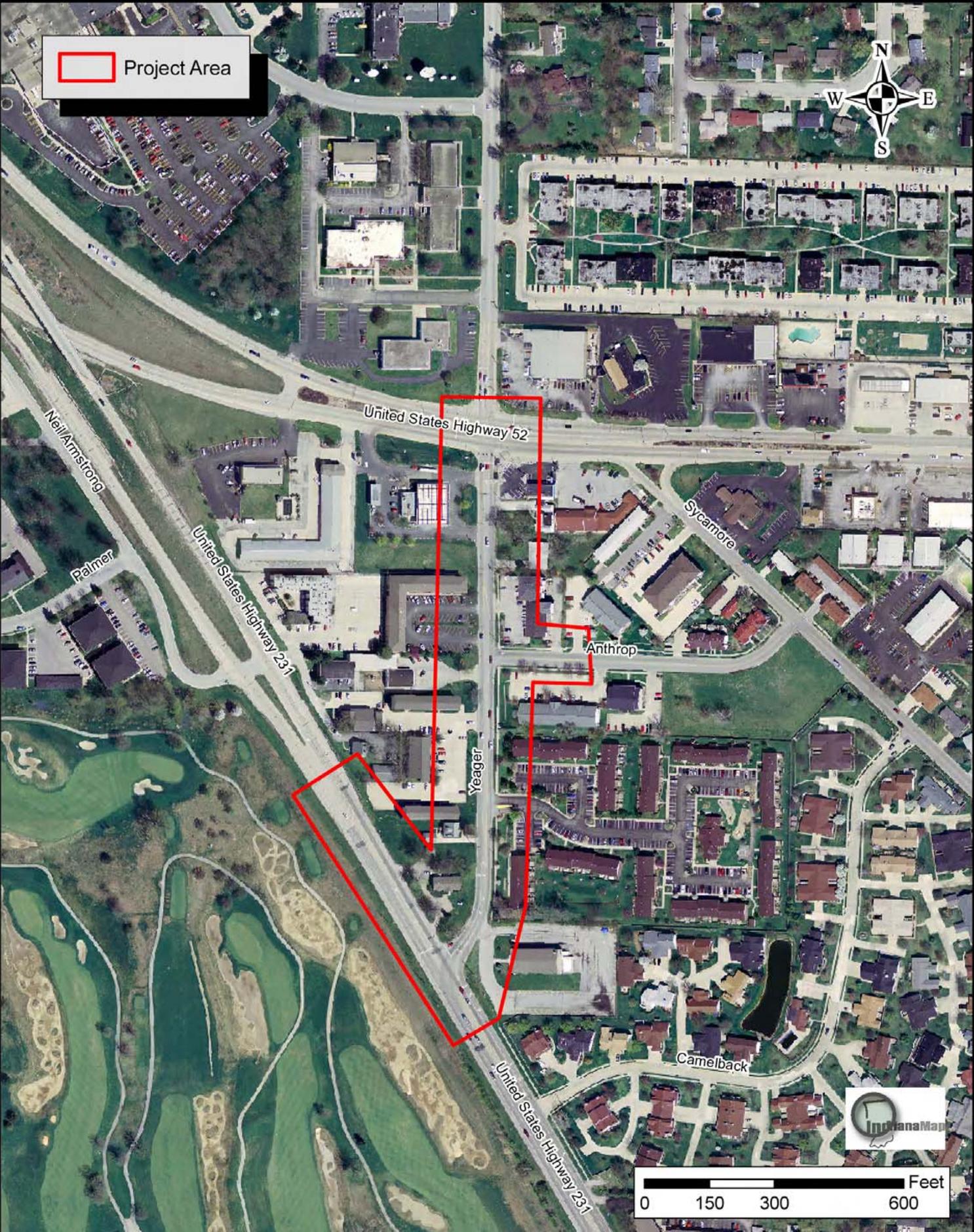
Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 04/18/2008

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 Project Area



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Aerial Photography Map
2005 Aerial Photography

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Yeager Road Improvements

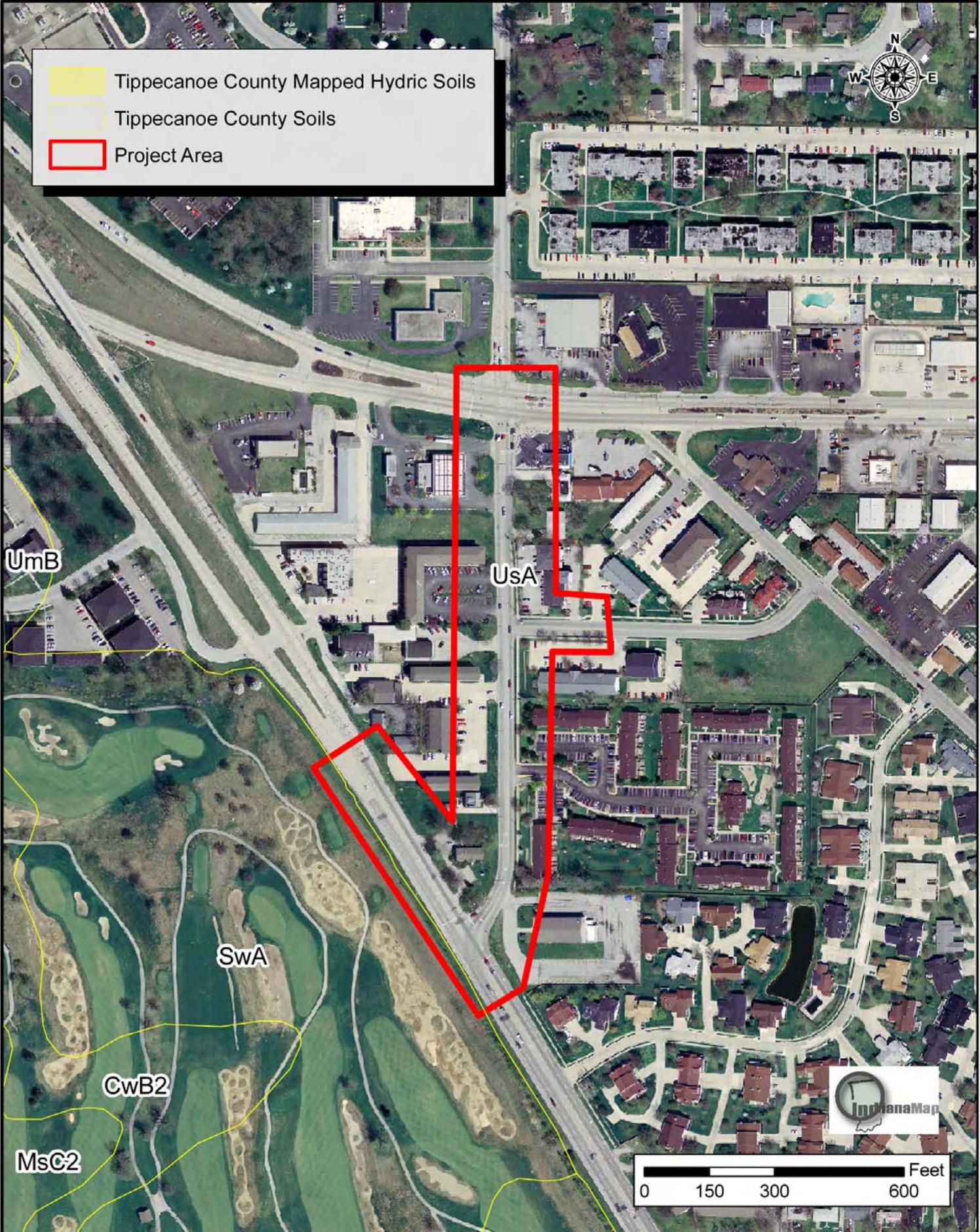
Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 05/13/2009

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	Tippecanoe County Mapped Hydric Soils
	Tippecanoe County Soils
	Project Area



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Tippecanoe County Soils Survey Map

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Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 05/14/2009

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Map Unit Legend

Tippecanoe County, Indiana

Map symbol	Map unit name
Am	Allison silt loam, protected
Ap	Allison silt loam, frequently flooded
AtB2	Alvin-Spinks complex, 2 to 6 percent slopes, eroded
Ba	Battleground silt loam, protected
Bb	Battleground silt loam, frequently flooded
BgA	Beecher silt loam, 0 to 2 percent slopes
BkF	Berks channery silt loam, 25 to 60 percent slopes
BIA	Billett fine sandy loam, gravelly substratum, 0 to 2 percent slopes
BIB2	Billett fine sandy loam, gravelly substratum, 2 to 6 percent slopes, eroded
BmA	Billett fine sandy loam, moderately wet, 0 to 2 percent slopes
BnA	Billett loam, gravelly substratum, 0 to 2 percent slopes
BnB2	Billett loam, gravelly substratum, 2 to 6 percent slopes, eroded
BoA	Bowes silt loam, 0 to 2 percent slopes
BpA	Bowes Variant silt loam, 0 to 2 percent slopes
CaA	Camden silt loam, 0 to 2 percent slopes
CfB	Carmi sandy loam, 2 to 6 percent slopes
CgA	Carmi loam, 0 to 2 percent slopes
Ck	Ceresco sandy loam, gravelly substratum, rarely flooded
Cl	Ceresco loam, gravelly substratum, occasionally flooded
Cm	Chalmers silty clay loam
Co	Cohoctah fine sandy loam, gravelly substratum, rarely flooded
Cp	Cohoctah loam, gravelly substratum, occasionally flooded
CrC	Coloma sand, 6 to 15 percent slopes
CtA	Crosby silt loam, 0 to 2 percent slopes
CwB2	Crosby-Miami complex, 2 to 6 percent slopes, eroded
DmC2	Desker gravelly sandy loam, 6 to 12 percent slopes, eroded
DoC2	Desker sandy loam, kame, 6 to 12 percent slopes, eroded
DpD2	Desker-Rodman complex, kame, 12 to 18 percent slopes, eroded
Du	Drummer soils
Dy	Du Page loam, frequently flooded
EkA	Elston sandy loam, gravelly substratum, 0 to 2 percent slopes
EmA	Elston loam, gravelly substratum, 0 to 2 percent slopes
FcB	Fincastle-Crosby complex, 1 to 3 percent slopes
Hd	Harpster silt loam, pothole
HfB2	High Gap Variant silt loam, 1 to 6 percent slopes, eroded
HfC2	High Gap Variant silt loam, 6 to 12 percent slopes, eroded
HnB	Hononegah loamy sand, 2 to 6 percent slopes
HoA	Hononegah fine sandy loam, 0 to 2 percent slopes
Hv	Houghton muck, undrained
KaA	Kalamazoo loam, 0 to 2 percent slopes
KaB2	Kalamazoo loam, 2 to 6 percent slopes, eroded
KbB2	Kalamazoo silt loam, 2 to 6 percent slopes, eroded
KcB2	Kalamazoo silt loam, kame, 2 to 6 percent slopes, eroded
KcC2	Kalamazoo silt loam, kame, 6 to 12 percent slopes, eroded
KoD2	Kosciusko sandy loam, 12 to 18 percent slopes, eroded
KpC3	Kosciusko gravelly sandy clay loam, 6 to 12 percent slopes, severely eroded
LaA	Lafayette silt loam, 0 to 2 percent slopes
LeA	La Hogue loam, till substratum, 0 to 2 percent slopes
Lm	Lash silt loam, frequently flooded

Map Unit Legend

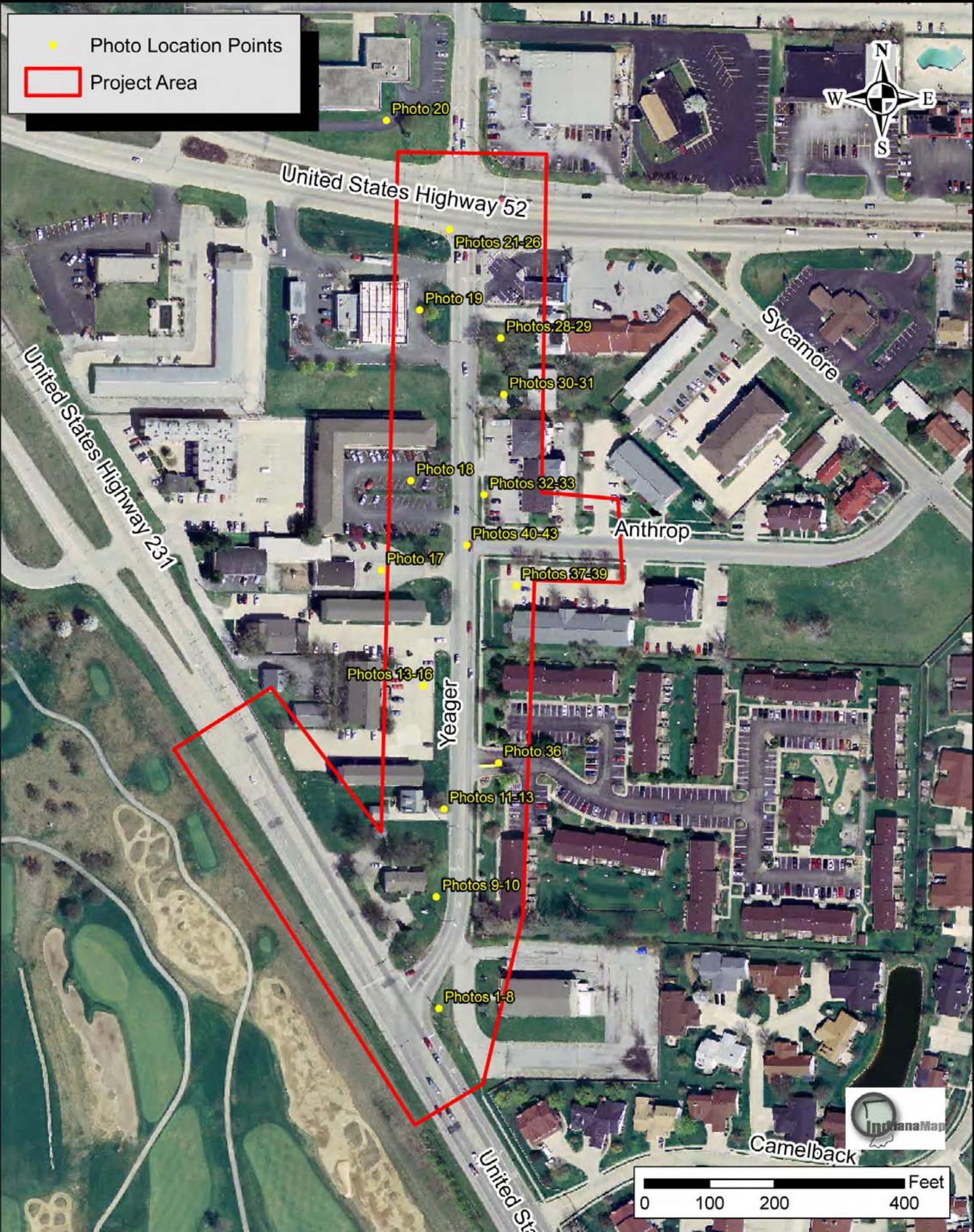
Tippecanoe County, Indiana

Map symbol	Map unit name
LnA	Lauramie silt loam, 0 to 2 percent slopes
LnB2	Lauramie silt loam, 2 to 6 percent slopes, eroded
LoA	Linkville loam, loamy substratum, 0 to 2 percent slopes
LoB	Linkville loam, loamy substratum, 2 to 6 percent slopes
LvB2	Longlois silt loam, 2 to 6 percent slopes, eroded
LwB2	Longlois silt loam, kame, 2 to 6 percent slopes, eroded
M-W	Miscellaneous water
Mb	Mahalasville silty clay loam, gravelly substratum
Mc	Mahalasville silty clay loam, shale substratum
Md	Mahalasville-Treaty complex
MmB2	Marker silt loam, 2 to 6 percent slopes, eroded
MoA	Mellott silt loam, 0 to 2 percent slopes
MsC2	Miami silt loam, 6 to 12 percent slopes, eroded
MsD2	Miami silt loam, 12 to 18 percent slopes, eroded
MtC3	Miami clay loam, 6 to 12 percent slopes, severely eroded
MtD3	Miami clay loam, 12 to 18 percent slopes, severely eroded
Mu	Milford silty clay loam, pothole
MwA	Mulvey silt loam, 0 to 2 percent slopes
Mz	Muskego muck, drained
OaB2	Oakville-Billett moderately wet, complex, 2 to 6 percent slopes, eroded
OgA	Ockley silt loam, 0 to 2 percent slopes
OmB2	Octagon silt loam, 2 to 6 percent slopes, eroded
OmC2	Octagon silt loam, 6 to 12 percent slopes, eroded
OpC3	Octagon clay loam, 6 to 12 percent slopes, severely eroded
Ou	Ouiatenon sandy loam, frequently flooded
Ox	Ouiatenon loamy sand, occasionally flooded
Oy	Ouiatenon fine sandy loam, sandy substratum, frequently flooded
Pc	Palms muck, drained
Pd	Palms muck, gravelly substratum, undrained
Pg	Pella silty clay loam, pothole
Pk	Peotone silty clay loam, pothole
PmB	Pinevillage gravelly sandy loam, 2 to 8 percent slopes, rarely flooded
Pt	Pits, gravel
RaB2	Rainsville silt loam, 2 to 6 percent slopes, eroded
RcA	Raub-Brenton complex, 0 to 1 percent slopes
RdA	Richardville silt loam, 0 to 2 percent slopes
RdB2	Richardville silt loam, 2 to 6 percent slopes, eroded
RdC2	Richardville silt loam, 6 to 12 percent slopes, eroded
RoB	Rockfield silt loam, 1 to 3 percent slopes
RsF	Rodman gravelly loam, 25 to 60 percent slopes
Rz	Ross silt loam, protected
Sd	Saranac silty clay, gravelly substratum, occasionally flooded
Sf	Sawabash silty clay loam, frequently flooded
ShB	Shadeland silt loam, 1 to 4 percent slopes
SmA	Sleeth loam, 0 to 2 percent slopes
Sn	Sloan clay loam, occasionally flooded
So	Sloan Variant silty clay loam, occasionally flooded
SrB	Sparta sand, 2 to 6 percent slopes
SrC	Sparta sand, 6 to 12 percent slopes

Map Unit Legend

Tippecanoe County, Indiana

Map symbol	Map unit name
StC	Spinks fine sand, 6 to 12 percent slopes
SwA	Starks-Fincastle complex, 0 to 2 percent slopes
SyF	Strawn-Rodman complex, 18 to 50 percent slopes
TbA	Tecumseh silt loam, 0 to 2 percent slopes
TcA	Thackery silt loam, 0 to 2 percent slopes
TfB	Throckmorton silt loam, 1 to 3 percent slopes
Tg	Tice silty clay loam, frequently flooded
TmA	Toronto-Millbrook complex, 0 to 2 percent slopes
TnB2	Toronto-Octagon complex, 2 to 6 percent slopes, eroded
TtA	Troxel silty clay loam, 0 to 2 percent slopes
Ua	Udorthents, loamy
UbB	Urban land-Billett, gravelly substratum, complex, 2 to 8 percent slopes
UcA	Urban land-Carmi complex, 0 to 2 percent slopes
UmB	Urban land-Miami complex, 2 to 8 percent slopes
UmC	Urban land-Miami complex, 8 to 15 percent slopes
UsA	Urban land-Starks-Fincastle complex, 0 to 2 percent slopes
W	Water
Wb	Wallkill silt loam, coprogenous earth substratum
We	Washtenaw silt loam
WgA	Waupecan silt loam, 0 to 2 percent slopes
WhA	Waupecan silt loam, moderately wet, 0 to 2 percent slopes
WmA	Waynetown silt loam, 0 to 2 percent slopes
WtA	Wea silt loam, occasionally flooded
WuA	Whitaker loam, till substratum, 0 to 2 percent slopes



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Photo Location Mapping
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Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 08/03/2009

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YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 1: Masonic Temple, east side of Yeager Road



Photo 2: North side of the Masonic Temple



Photo 3: Masonic Temple



Photo 4: South side of the Masonic Temple

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 5: Masonic Temple, 5957 Yeager Road



Photo 6: Looking south along Northwestern Avenue (US 231)



Photo 7: Looking north along Northwestern Avenue (US 231)



Photo 8: Looking north along Yeager from Northwestern Avenue (US 231)

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 9: Allstate, south corner of Yeager and Northwestern Avenue (US 231)



Photo 10: Allstate, south corner of Yeager and Northwestern Avenue (US 231)



Photo 11: 2331 Yeager Road



Photo 12: 2331 Yeager Road

YEAGER ROAD IMPROVEMENTS
Project No. IN200061278
May 1, 2008



Photo 13: 2331 Yeager Road



Photo 14: 2335 Yeager
Highland Courts Apartments



Photo 15: 2335 Yeager
Highland Court Apartments



Photo 16: 2335 Yeager
Highland Court Apartments

YEAGER ROAD IMPROVEMENTS
Project No. IN200061278
May 1, 2008



Photo 17: 2363 Yeager Road
Children's House



Photo 18: 2367 Yeager
Pine Manor Apartments



Photo 19: BP gas station, southwest
corner of Yeager and US 52

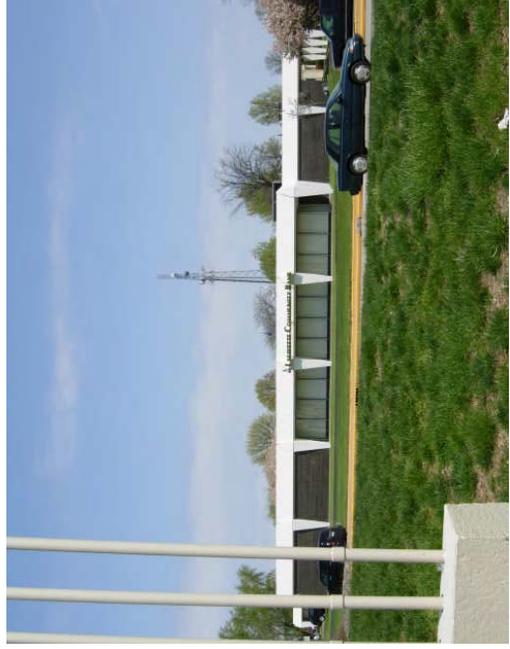


Photo 20: Lafayette Community Bank,
northwest corner of Yeager and US 52

YEAGER ROAD IMPROVEMENTS
Project No. IN200061278
May 1, 2008



Photo 21: Looking west along US 52



Photo 22: Looking east along US 52



Photo 23: Looking north along Yeager
from intersection with US 52



Photo 24: Looking south along
Yeager from US 52 intersection

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 25: ACE Hardware, northeast corner of Yeager and US 52



Photo 26: Frozen Custard Shop, southeast corner of Yeager and US 52



Photo 27: Row marker, southwest corner US 52 and Yeager



Photo 28: 3412 Yeager Road

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 29: 2412 Yeager Road



Photo 30: 2406 Yeager Road



Photo 31: 2406 Yeager Road



Photo 32: ASIA Apartments

YEAGER ROAD IMPROVEMENTS
Project No. IN200061278
May 1, 2008



Photo 33: ASIA Market



Photo 34: Drainage swale, looking south at Richfield Apartments

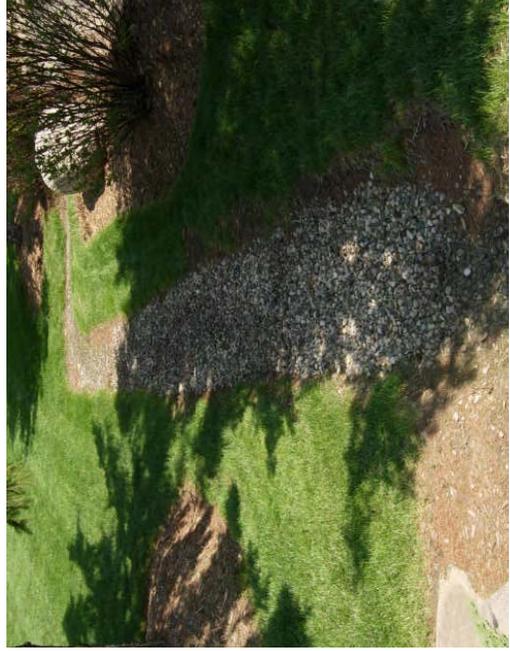


Photo 35: Drainage swale, looking north



Photo 36: Richfield Apartments, Yeager Road

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 37: Apartments, southeast corner of Anthrop Drive and Yeager Road



Photo 38: Apartments, southeast corner of Anthrop Drive and Yeager Road



Photo 39: Apartments, southeast corner of Anthrop Drive and Yeager Road



Photo 40: Looking west along Anthrop towards Yeager Road

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 41: Looking west along Anthrop Drive towards Yeager Road



Photo 42: Looking south along Yeager from Anthrop Drive

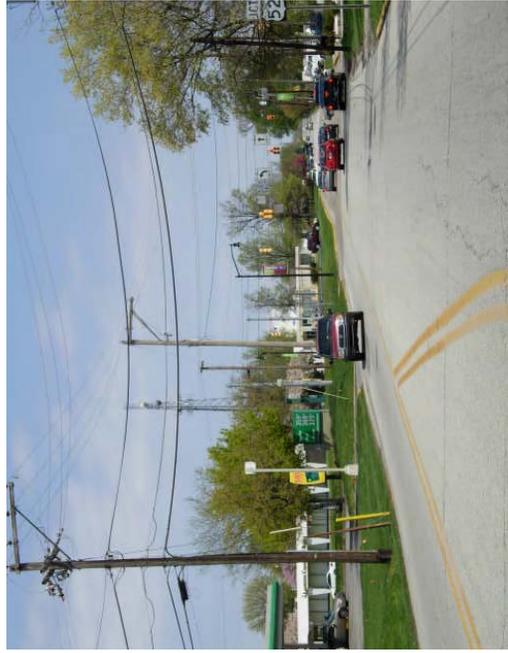


Photo 43: Looking north along Yeager from Anthrop Drive

PROJECT	DESIGNATION
CONTRACT	BRIDGE FILE

**INDIANA
DEPARTMENT OF
TRANSPORTATION**

**ROAD PLANS
YEAGER ROAD RECONSTRUCTION
PROJECT NO. P.E.**

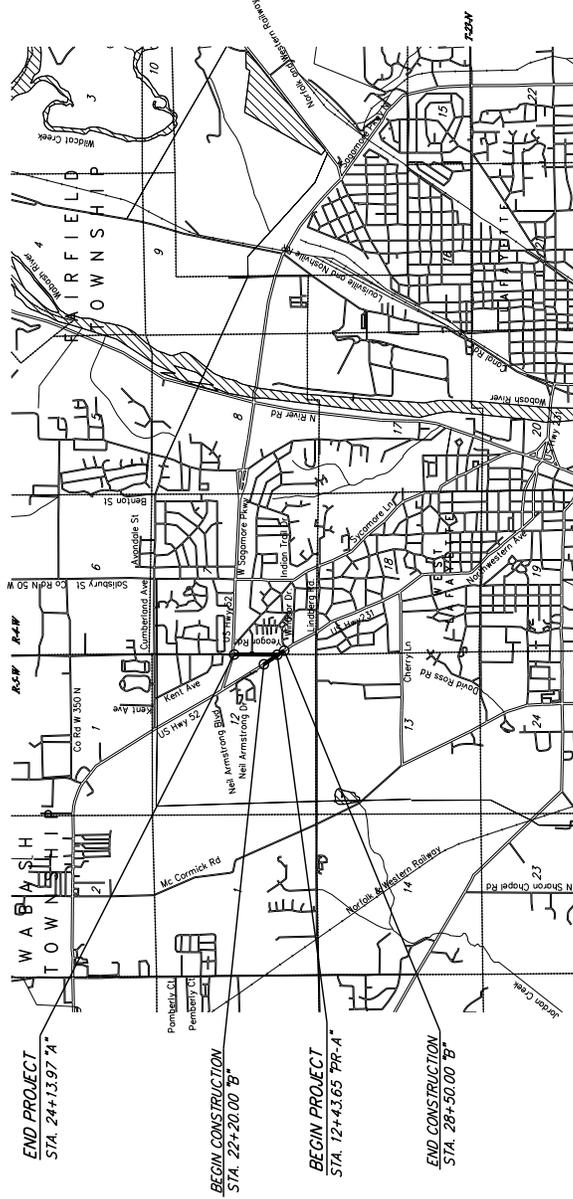
TRAFFIC DATA	YEAGER RD.
A.A.D.T. (2007)	17,121 V.P.D.
A.M.T. (2008)	4,800 V.P.D.
A.P.M.T. (2008)	4,800 V.P.D.
DIRECTIONAL DISTRIBUTION	50 %
TRUCKS	50 %
MAINTENANCE	50 %
DESIGN DATA	YEAGER RD.
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	AR (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	LOCAL AGENCY ARTERIAL
URBAN CLASSIFICATION	URBAN (INTERMEDIATE)
ACCESS CONTROL	NONE
TRAFFIC DATA	NORTHWESTERN AVE. (US231)
A.A.D.T. (2007)	21,714 V.P.D.
A.M.T. (2008)	26,760 V.P.D.
A.P.M.T. (2008)	26,760 V.P.D.
DIRECTIONAL DISTRIBUTION	50 %
TRUCKS	50 %
MAINTENANCE	50 %
DESIGN DATA	NORTHWESTERN AVE. (US231)
DESIGN SPEED	55 M.P.H.
PROJECT DESIGN CRITERIA	AR (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	AR-TERIAL
URBAN CLASSIFICATION	URBAN (INTERMEDIATE)
ACCESS CONTROL	NONE

Project Description: Yeager Road Reconstruction from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52), a distance of 1,170 ft. all in Section 12, T-23-N, R-5-W and Section 7, T-23-N, R-4-W, Wabash Township, Tippecanoe County.

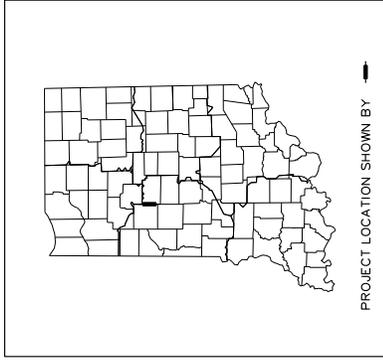
GROSS LENGTH: 0.228 MI.
NET LENGTH: 0.228 MI.

PLAN: LONG: 1"=30'
TRANS: 1"=50'
PROFILE: LONG: 1"=30'
TRANS: 1"=10'

MAX. GRADE: -1.37%



TIPPECANOE COUNTY
LOCATION MAP



PROJECT LOCATION SHOWN BY

SCALE: 1" = 2000'

**UTILITY COORDINATION
PLANS**

DATE: REV 6/3/09

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2008
TO BE USED WITH THESE PLANS

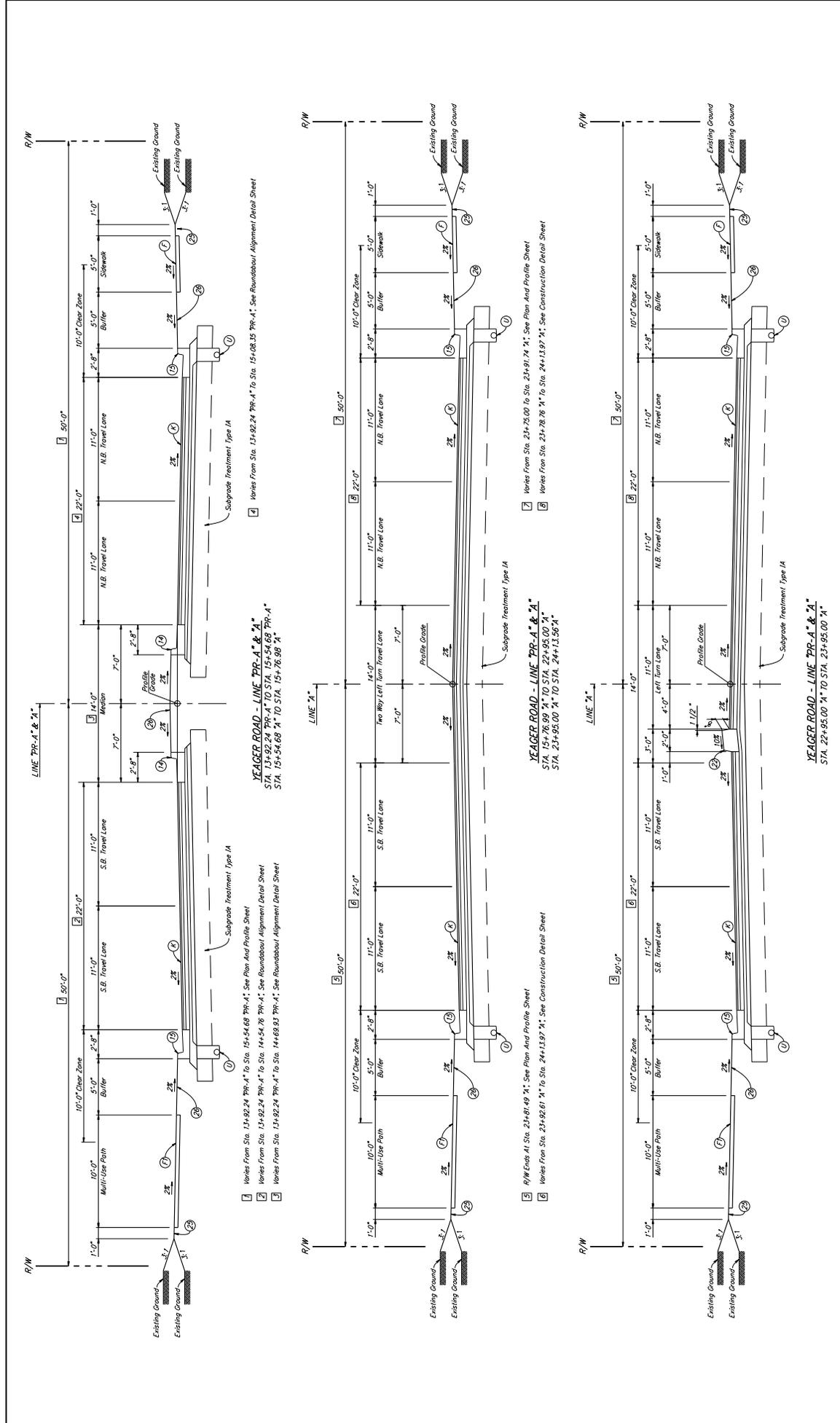
LATITUDE	BRIDGE FILE
LONGITUDE	DESIGNATION NO.
HYDRAULIC UNIT CODE	SHEETS
CONTRACT	PROJECT NO.

PLANS PREPARED BY: American Structurepoint, Inc.	(317) 547-5550	PHONE NUMBER
CERTIFIED BY:	DATE	DATE
APPROVED FOR LETTING:	CHIEF, DIVISION OF DESIGN	DATE

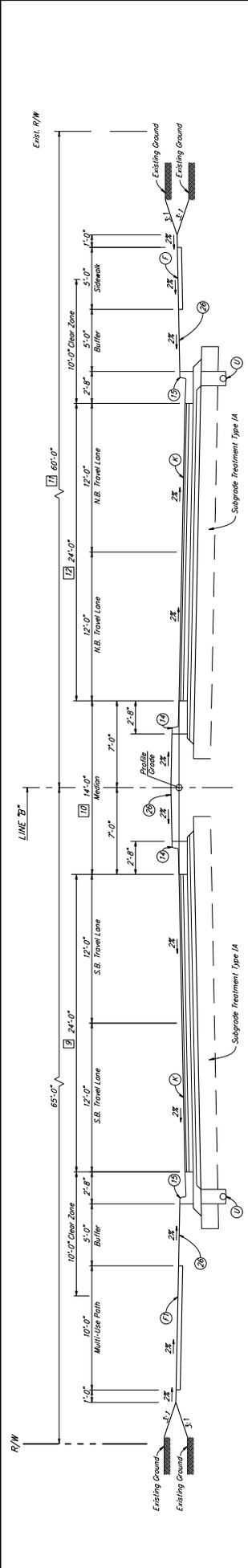
AMERICAN
STRUCTUREPOINT
INC.

1700 BRIDGE AND DIVISION
AVENUE
TEL: 317.547.5550 FAX: 317.543.0270
www.americanstructurepoint.com



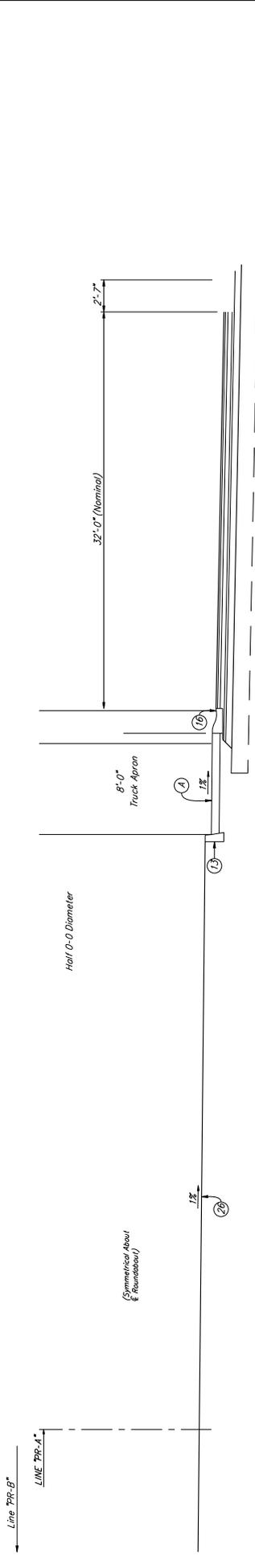


INDIANA DEPARTMENT OF TRANSPORTATION TYPICAL CROSS SECTION LINE "PR-A" & "A"		HORIZONTAL SCALE VERTICAL SCALE DESIGNATION NO. SURVEY BOOK CONTRACT
RECOMMENDED FOR APPROVAL DESIGNED: JMT CHECKED: MPB	DESIGN ENGINEER DRAWN: PJT DATE PLOT	BRIDGE FILE 060696 3 of 46 PROJECT NO.
LEGEND (1) Concrete Curb (2) Modified Concrete Curb & Gutter Type C (3) Concrete Curb & Gutter Type C (4) Concrete Curb & Gutter Type B (5) Modified Concrete Curb & Gutter Type B (6) Full Depth HMA Pavement (7) Multi-Use Path (8) 165 #/SYD HMA Surface, Type B, 9.5 mm on 440 #/SYD HMA Intermediate, Type B, 9.5 mm on 200 #/SYD HMA Intermediate, C19.0 mm (OC) on 600 #/SYD HMA Base, Type B, 25.0 mm (9) Underdrain		



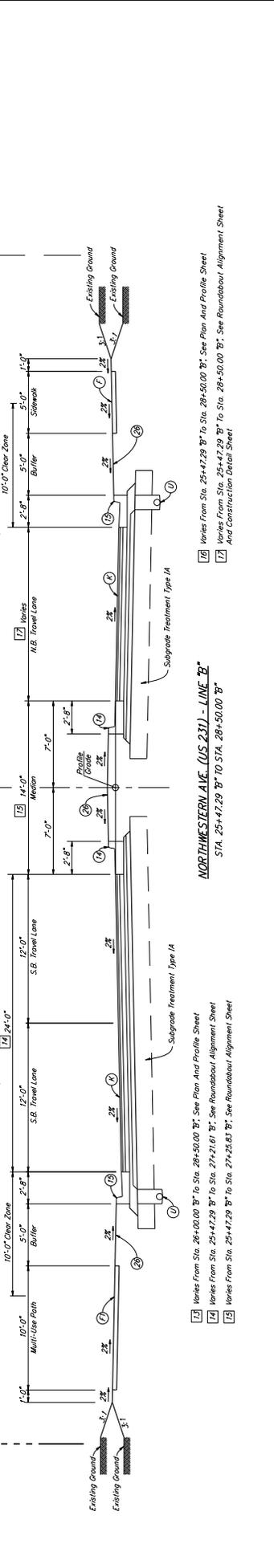
NORTHWESTERN AVE. (US 231) - LINE "B"
 STA. 22+20.00 "B" TO STA. 23+79.30 "B"
 1 Varies From Sta. 21+00.75 "B" To Sta. 21+79.30 "B". See Roundabout Alignment Sheet
 2 Varies From Sta. 21+12.11 "B" To Sta. 21+79.30 "B". See Roundabout Alignment Sheet

3 Varies From Sta. 21+00.75 "B" To Sta. 21+79.30 "B". See Roundabout Alignment Sheet
 4 Varies From Sta. 21+12.11 "B" To Sta. 21+79.30 "B". See Roundabout Alignment Sheet



NORTHWESTERN AVE. (US 231) - LINE "B"
 STA. 25+47.29 "B" TO STA. 28+50.00 "B"
 1 Varies From Sta. 25+47.29 "B" To Sta. 28+50.00 "B". See Plan And Profile Sheet
 2 Varies From Sta. 25+47.29 "B" To Sta. 28+50.00 "B". See Roundabout Alignment Sheet

3 Varies From Sta. 26+00.00 "B" To Sta. 28+50.00 "B". See Plan And Profile Sheet
 4 Varies From Sta. 25+47.29 "B" To Sta. 27+21.67 "B". See Roundabout Alignment Sheet
 5 Varies From Sta. 25+47.29 "B" To Sta. 27+25.83 "B". See Roundabout Alignment Sheet



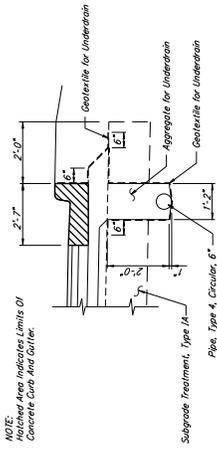
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED:	JMT	DRAWN:	PJT		
CHECKED:	MPB	CHECKED:	MPB		

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL CROSS SECTION	LINE "B" & "PR-A"

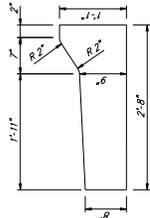
HORIZONTAL SCALE	BRIDGE FILE
N/A	DESIGNATION NO. 0600696
SURVEY BOOK	SHEETS 4 of 46
CONTRACT	PROJECT NO.

LEGEND

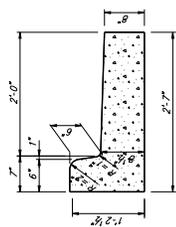
- 1 Concrete Curb
- 2 Modified Concrete Curb & Collar Type C
- 3 Concrete Curb & Collar Type C
- 4 Concrete Curb & Collar Type C
- 5 Modified Concrete Curb & Collar Type B
- 6 Concrete Center Curb Type C
- 7 Multi-Use Path
- 8 Full Depth HMA Pavement
- 9 165 #/SYD HMA Surface, Type B, 9.5 mm on 44 #/SYD HMA Intermediate, Type B, 19.0 mm on 200 #/SYD HMA Intermediate, Type B, 19.0 mm on 200 #/SYD HMA Base, Type B, 25.0 mm
- 10 Underdrain



UNDERDRAIN DETAIL
(SHOWING CURB AND GUTTER SECTION)
Scale: 1/2" = 1'-0"

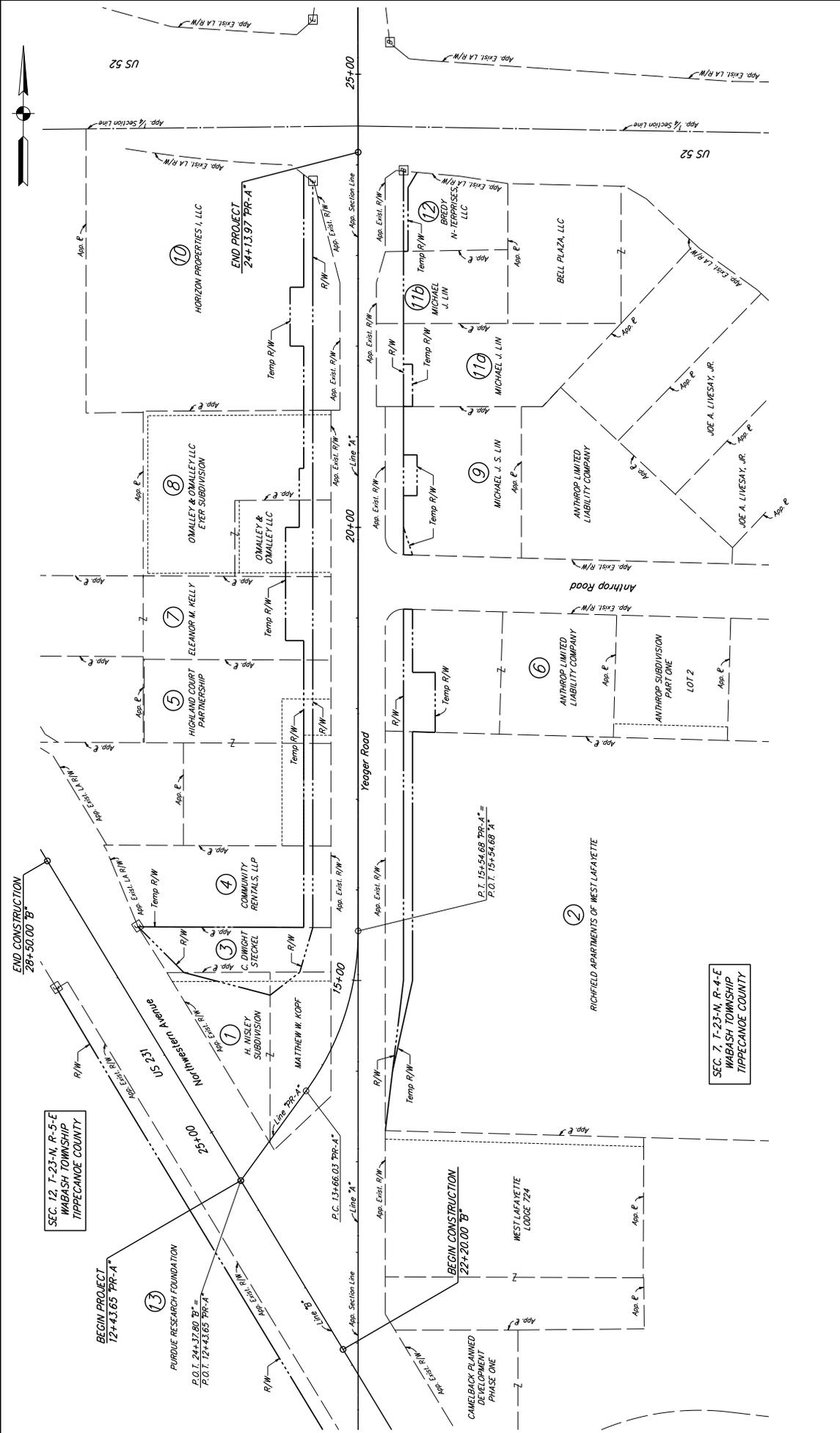


INVERTED COMBINED CONCRETE CURB & GUTTER, TYPE B
Scale: 1" = 1'-0"



2'-7" INVERTED CONCRETE CURB & GUTTER DETAIL
Scale: 1" = 1'-0"

RECOMMENDED FOR APPROVAL _____ DATE _____ DESIGN ENGINEER _____ DESIGNED: JMT DRAWN: PJT CHECKED: MPB CHECKED: MPB		INDIANA DEPARTMENT OF TRANSPORTATION MISCELLANEOUS DETAILS		HORIZONTAL SCALE _____ BRIDGE FILE _____ VERTICAL SCALE N/A DESIGNATION NO. 06060696 SURVEY BOOK _____ SHEETS 5 of 46 CONTRACT _____ PROJECT NO. _____
---	--	--	--	---



SEC. 7, T-23-N, R-4-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

BEGIN PROJECT
12+43.65 P.P.R.-A

PURDUE RESEARCH FOUNDATION
P.O.I. 24+37.80 B.P. =
P.O.I. 12+43.65 P.P.R.-A

13

1

3

4

5

7

8

10

2

6

9

11

12

119

120

121

122

123

END CONSTRUCTION
28+50.00 B

US 231

Northwestern Avenue

15+00

15+00

20+00

20+00

25+00

25+00

US 52

US 52

Anthrop Road

28+50.00 B

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: PJT	
CHECKED: MPB	CHECKED: MPB	

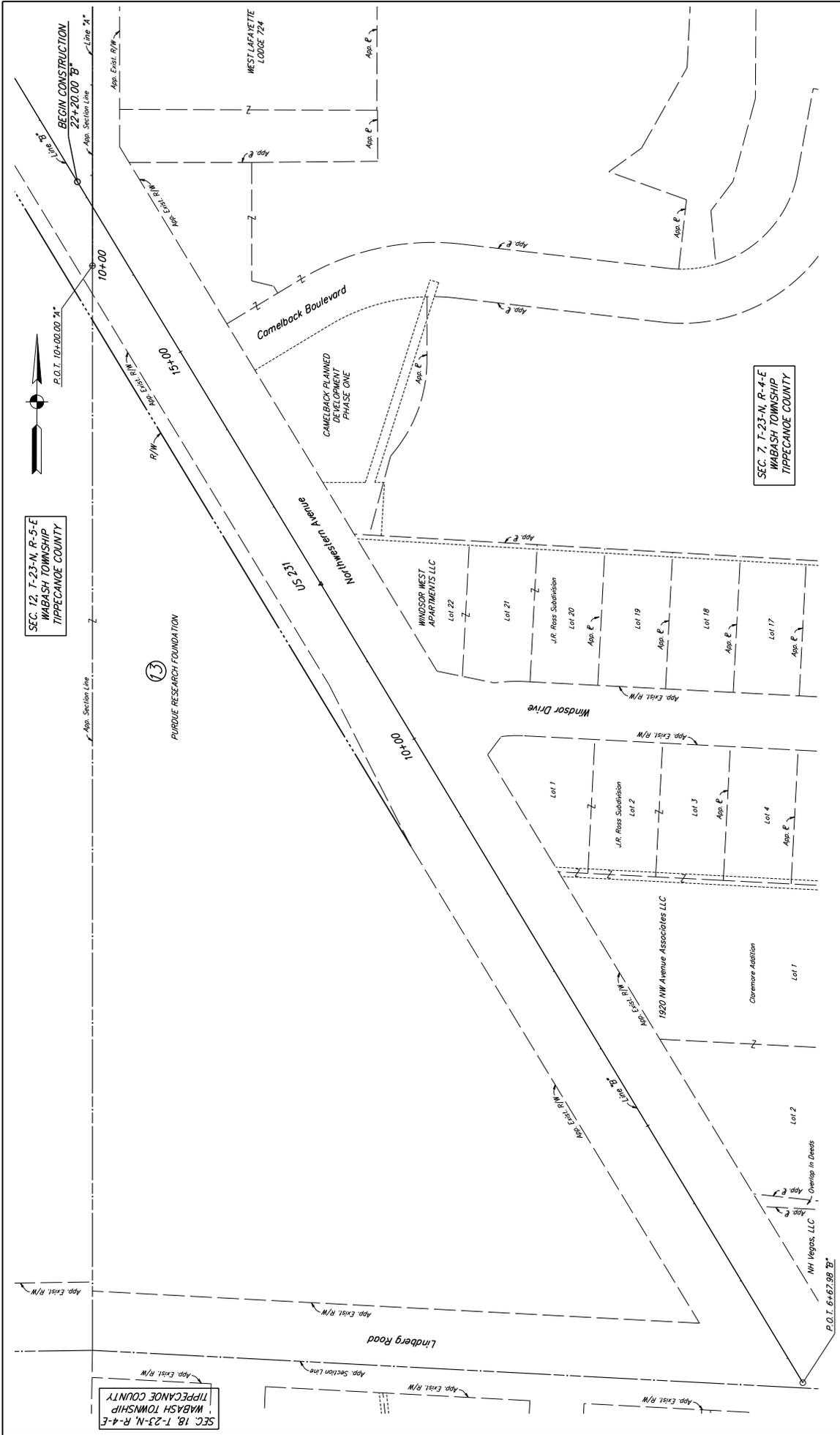
INDIANA
DEPARTMENT OF TRANSPORTATION
PLAT NO. 1

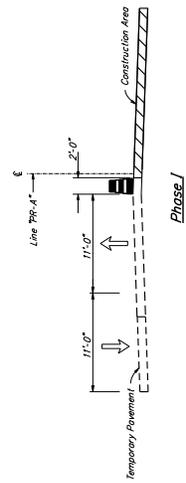
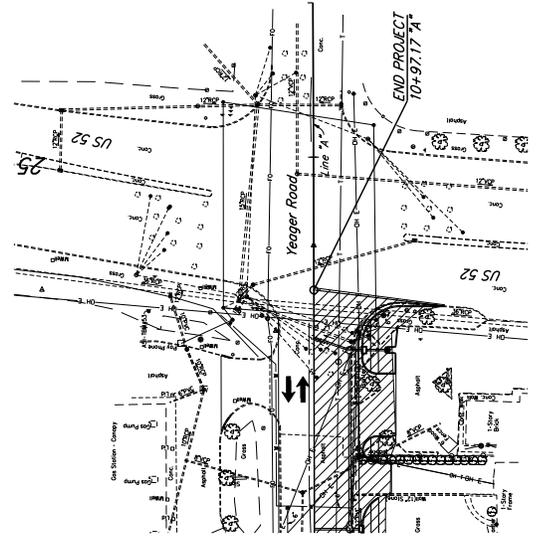
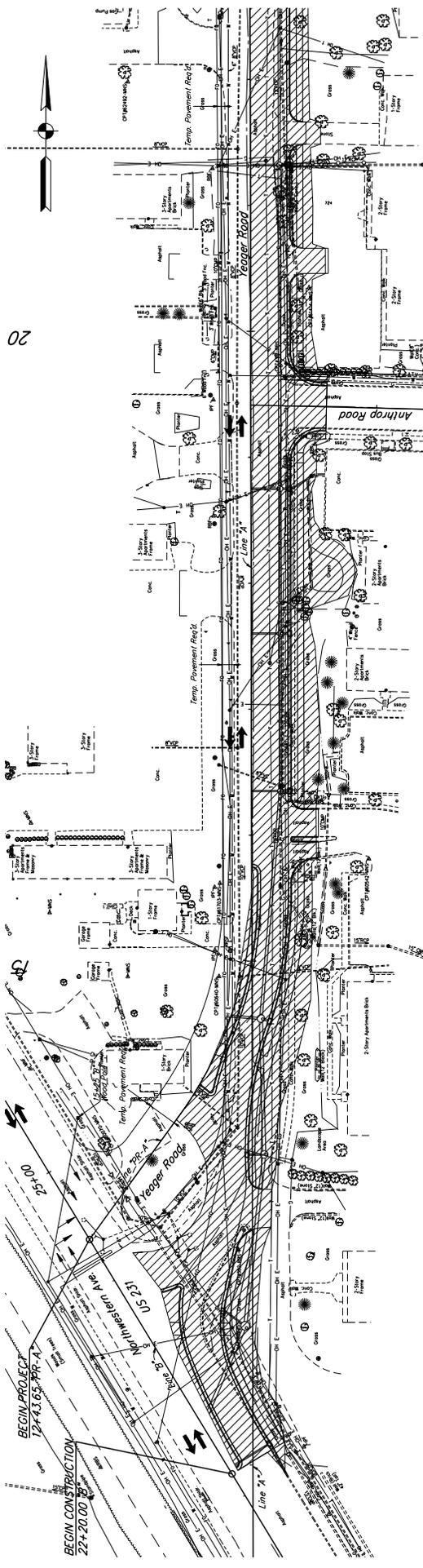
HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK	06060696
CONTRACT	SHEETS
	6 of 46
	PROJECT NO.

LEGEND
 [B] Beg'n L.A. R/W
 [E] End L.A. R/W
 [X] Parcel Number

SEC. 7, T-23-N, R-4-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

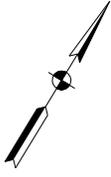
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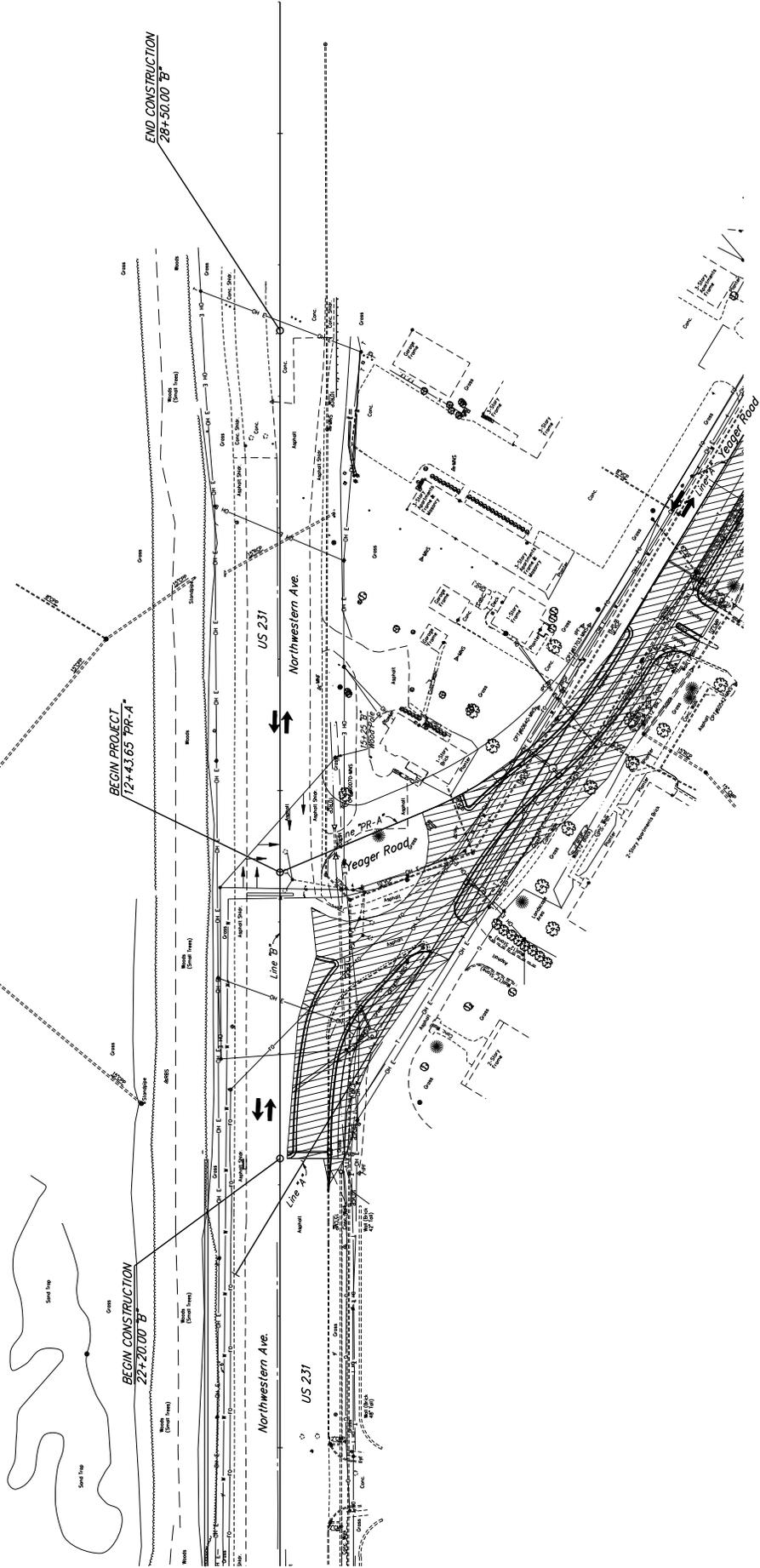
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	4/5/2009	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE
	DESIGNED: JMT	DRAWN: JMT	CHECKED: MPB		VERTICAL SCALE	DESIGNATION NO.
DESIGNED: JMT	DRAWN: JMT	CHECKED: MPB	DATE	MAINTENANCE OF TRAFFIC	N/A	06060696
CHECKED: MPB	CHECKED: MPB			PHASE I	SURVEY BOOK	SHEETS
					CONTRACT	8 of 46
						PROJECT NO.

30



25

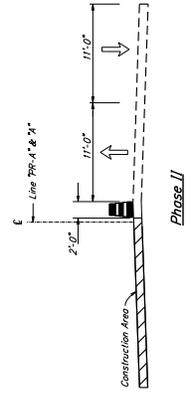
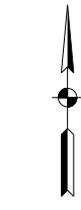
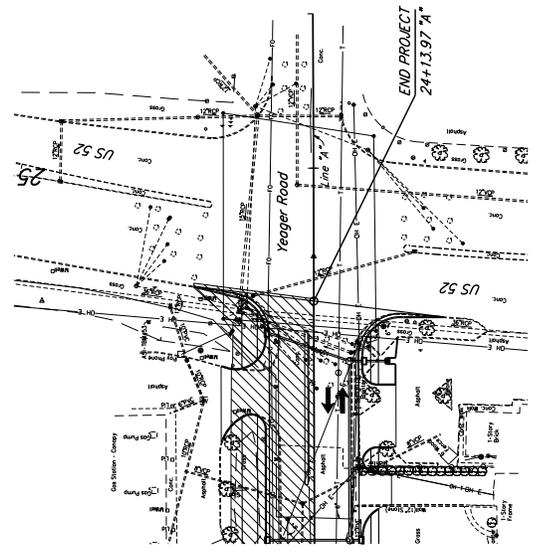
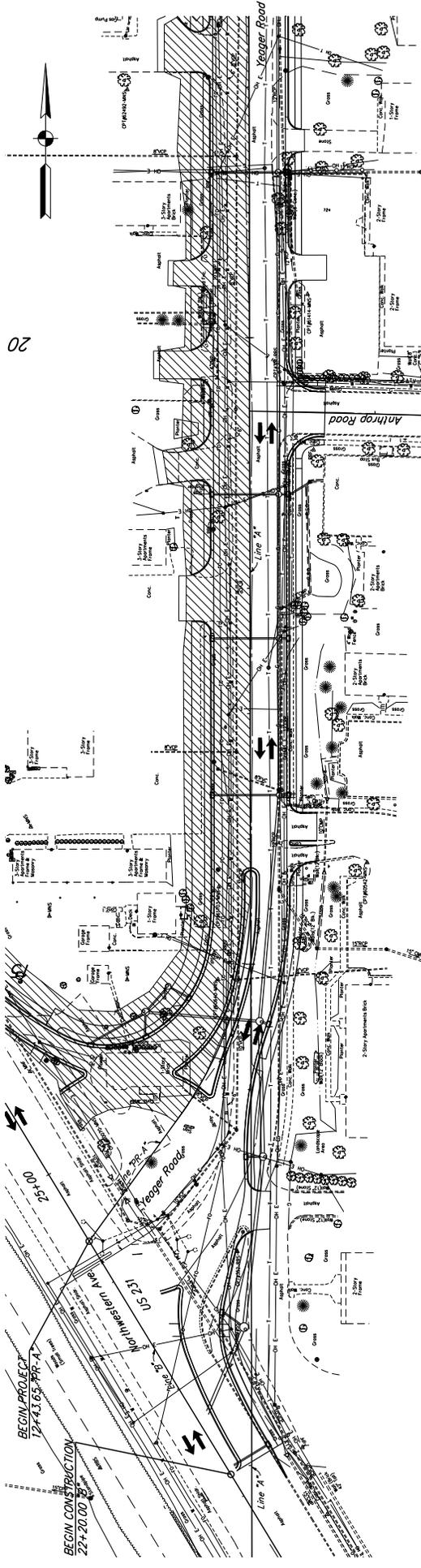
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HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
N/A	06060696
SURVEY BOOK	SHEETS
CONTRACT	9 of 46
	PROJECT NO.

INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE I

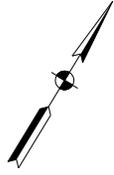
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: JMT	
CHECKED: MPB	CHECKED: MPB	



RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	4/5/2009	DATE	HORIZONTAL SCALE	BRIDGE FILE
	DESIGNED: JMT	DRAWN: JMT	4/5/2009		DESIGNATION NO.
CHECKED: MPB	CHECKED: MPB			VERTICAL SCALE	N/A
				SURVEY BOOK	SHEETS
				CONTRACT	10 of 46
				PROJECT NO.	

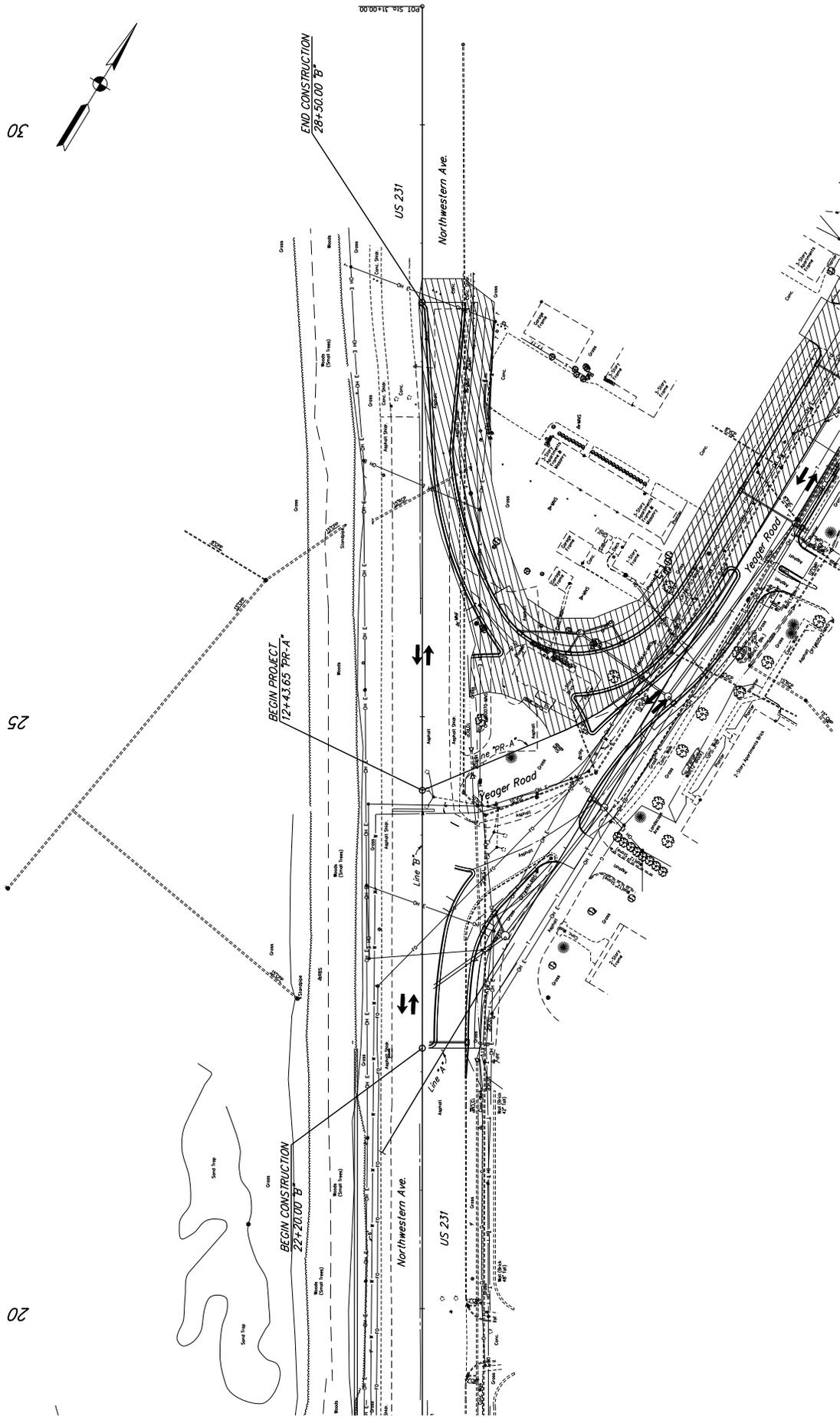
**INDIANA
DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE II**

30

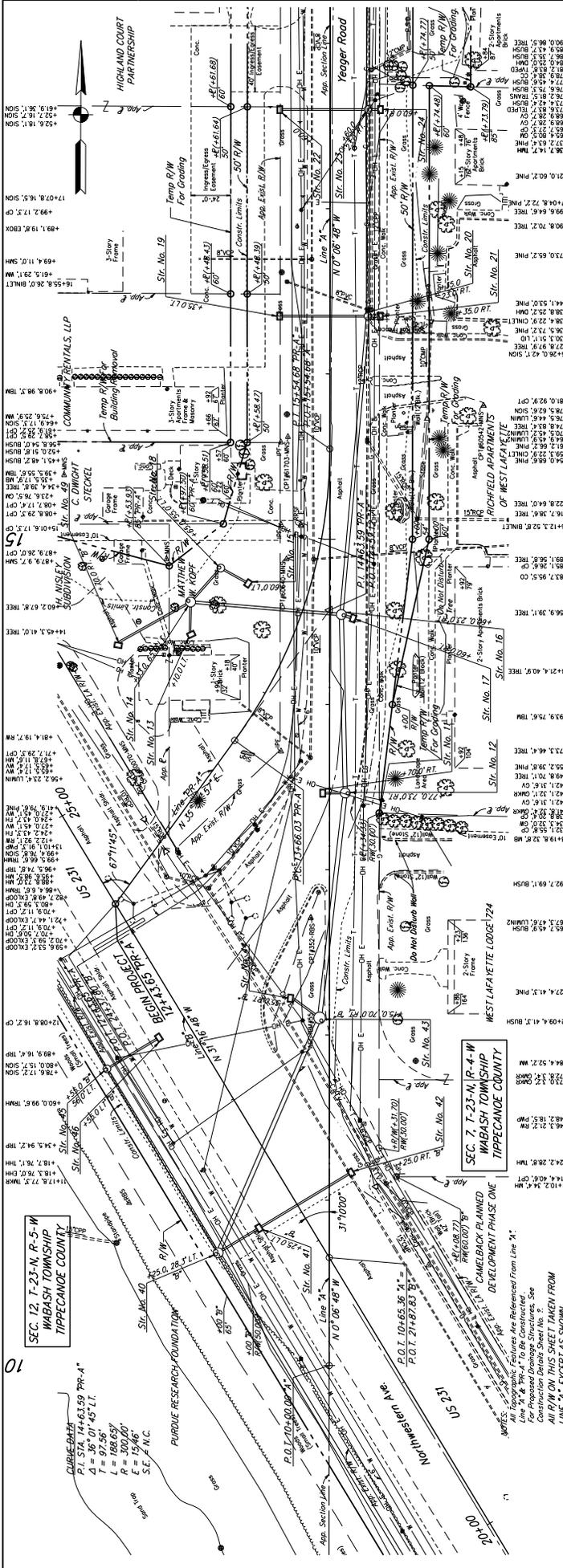


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20



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
DESIGNED: JMT	CHECKED: MPB	DRAWN: JMT	MPB	MAINTENANCE OF TRAFFIC		VERTICAL SCALE	DESIGNATION NO.
				PHASE II		N/A	06060696
						SURVEY BOOK	SHEETS
						CONTRACT	11 of 46
							PROJECT NO.



Stationing	Profile Description	Vertical Curve Data	Profile Elevation
740	740+00		740.00
730	730+00		730.00
720	720+00		720.00
710	710+00		710.00
700	700+00		700.00
690	690+00		690.00

INDIANA
DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
LINE "PR-A" & "A"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: RSC	
CHECKED: MPB	CHECKED: MPB	

HORIZONTAL SCALE: 1" = 10'
 VERTICAL SCALE: 1" = 10'
 BRIDGE FILE NO. 0606986
 SURVEY BOOK NO. 13
 SHEETS OF 46
 CONTRACT PROJECT NO.

10

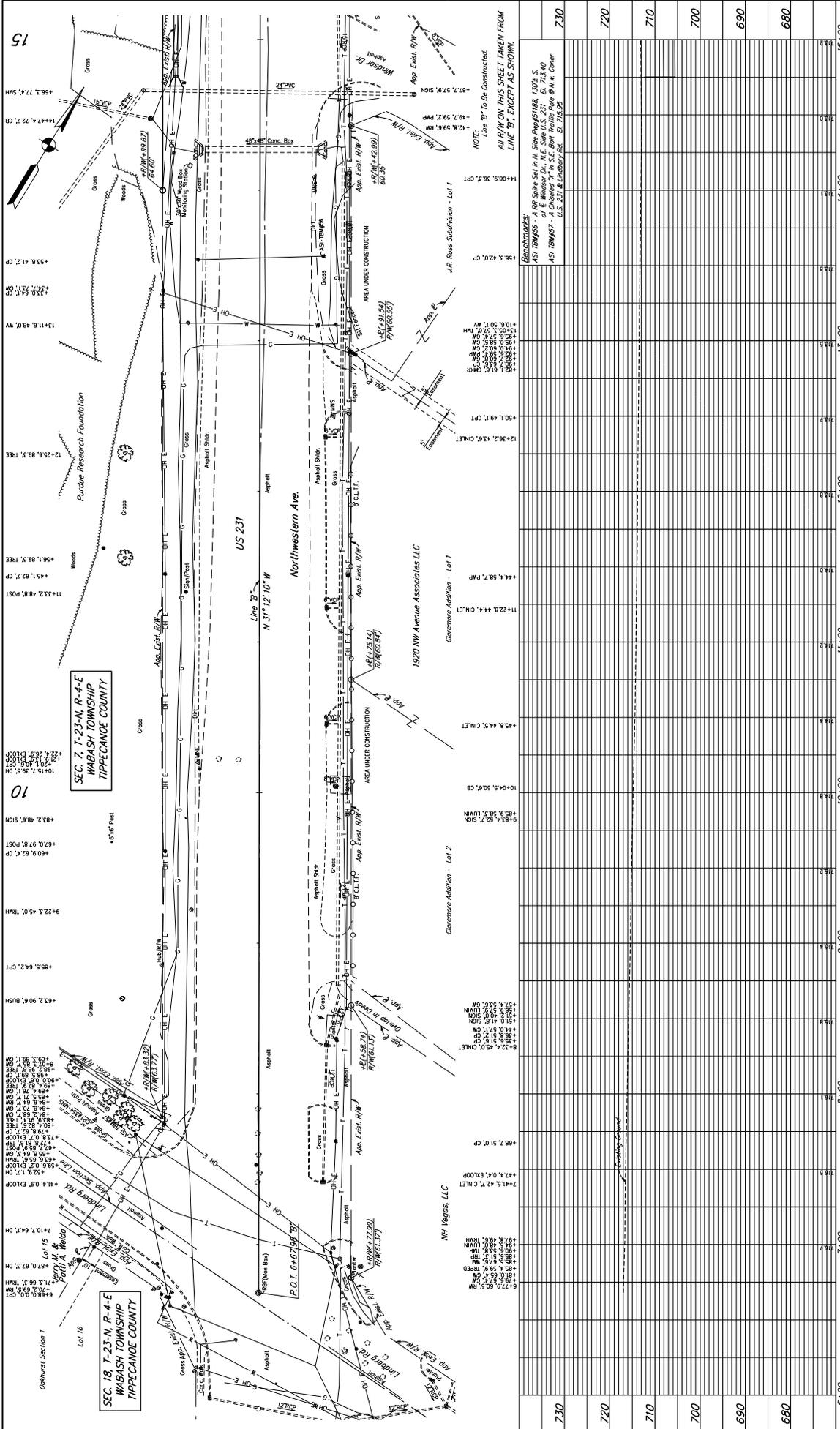
SEC. 12, T-23-N, R-5-W
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

CURVE DATA
 P.I. STA. 14+63.59 PR-A
 Δ = 36° 01' 45" LT
 L = 186.65'
 R = 300.00'
 E = 15.46'
 S.E. = N.C.

SEC. 7, T-23-N, R-4-W
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

DEVELOPMENT PHASE ONE
 CAMELBACK PLANNED
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

NOTES:
 1. All proposed structures shall be constructed in accordance with the Indiana Department of Transportation Standard Specifications for Highway Construction, Section 1000.
 2. All proposed structures shall be constructed in accordance with the Indiana Department of Transportation Standard Specifications for Highway Construction, Section 1000.
 3. All proposed structures shall be constructed in accordance with the Indiana Department of Transportation Standard Specifications for Highway Construction, Section 1000.



15

10

SEC. 7, T-23-N, R-4-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

SEC. 18, T-23-N, R-4-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

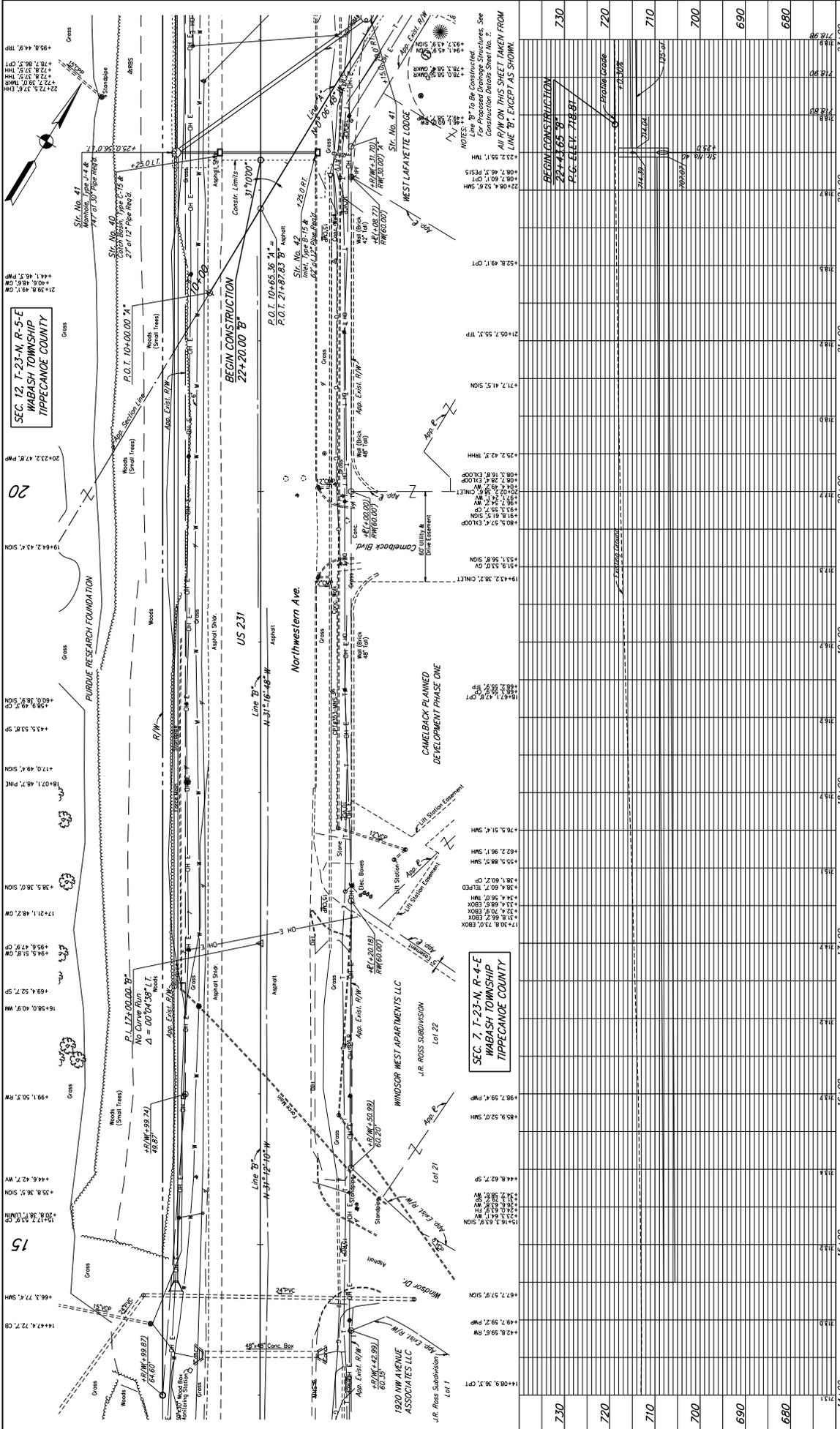
Station	Elevation	Notes
730		ASB (60.5)
720		ASB (60.5)
710		ASB (60.5)
700		ASB (60.5)
690		ASB (60.5)
680		ASB (60.5)
670		ASB (60.5)
660		ASB (60.5)
650		ASB (60.5)
640		ASB (60.5)
630		ASB (60.5)
620		ASB (60.5)
610		ASB (60.5)
600		ASB (60.5)
590		ASB (60.5)
580		ASB (60.5)
570		ASB (60.5)
560		ASB (60.5)
550		ASB (60.5)
540		ASB (60.5)
530		ASB (60.5)
520		ASB (60.5)
510		ASB (60.5)
500		ASB (60.5)
490		ASB (60.5)
480		ASB (60.5)
470		ASB (60.5)
460		ASB (60.5)
450		ASB (60.5)
440		ASB (60.5)
430		ASB (60.5)
420		ASB (60.5)
410		ASB (60.5)
400		ASB (60.5)
390		ASB (60.5)
380		ASB (60.5)
370		ASB (60.5)
360		ASB (60.5)
350		ASB (60.5)
340		ASB (60.5)
330		ASB (60.5)
320		ASB (60.5)
310		ASB (60.5)
300		ASB (60.5)
290		ASB (60.5)
280		ASB (60.5)
270		ASB (60.5)
260		ASB (60.5)
250		ASB (60.5)
240		ASB (60.5)
230		ASB (60.5)
220		ASB (60.5)
210		ASB (60.5)
200		ASB (60.5)
190		ASB (60.5)
180		ASB (60.5)
170		ASB (60.5)
160		ASB (60.5)
150		ASB (60.5)
140		ASB (60.5)
130		ASB (60.5)
120		ASB (60.5)
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100		ASB (60.5)
90		ASB (60.5)
80		ASB (60.5)
70		ASB (60.5)
60		ASB (60.5)
50		ASB (60.5)
40		ASB (60.5)
30		ASB (60.5)
20		ASB (60.5)
10		ASB (60.5)
0		ASB (60.5)

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
LINE "B"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: RJC	MPB
CHECKED: MPB	CHECKED: MPB	MPB

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	060606
SURVEY BOOK	SHEETS
CONTRACT	15 of 46
PROJECT NO.	



SEC. 12, T-23-N, R-5-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

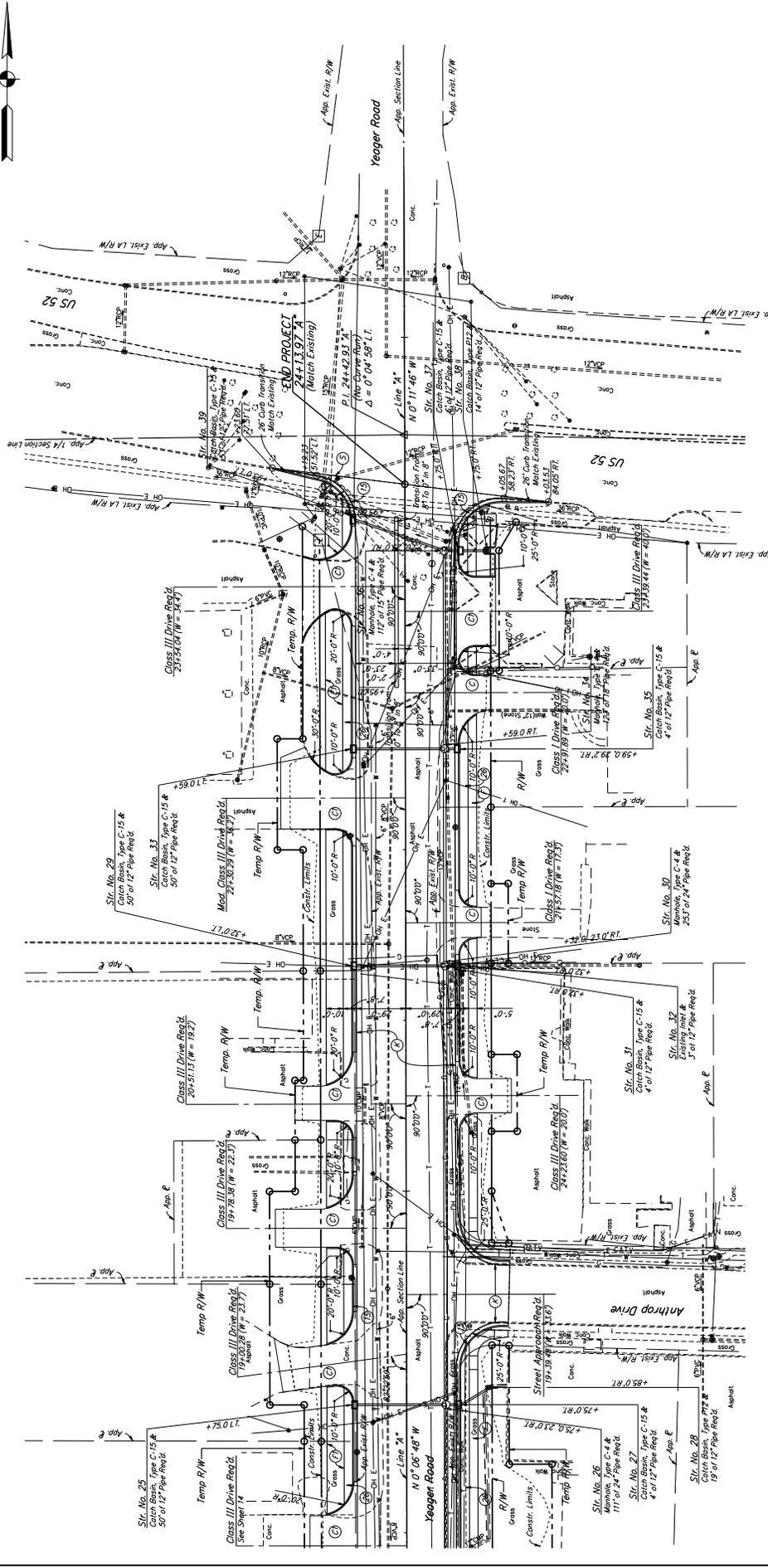
SEC. 7, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: RJC	
CHECKED: MPB	CHECKED: MPB	

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	060606
SURVEY BOOK	SHEETS
CONTRACT	16 of 46
PROJECT NO.	

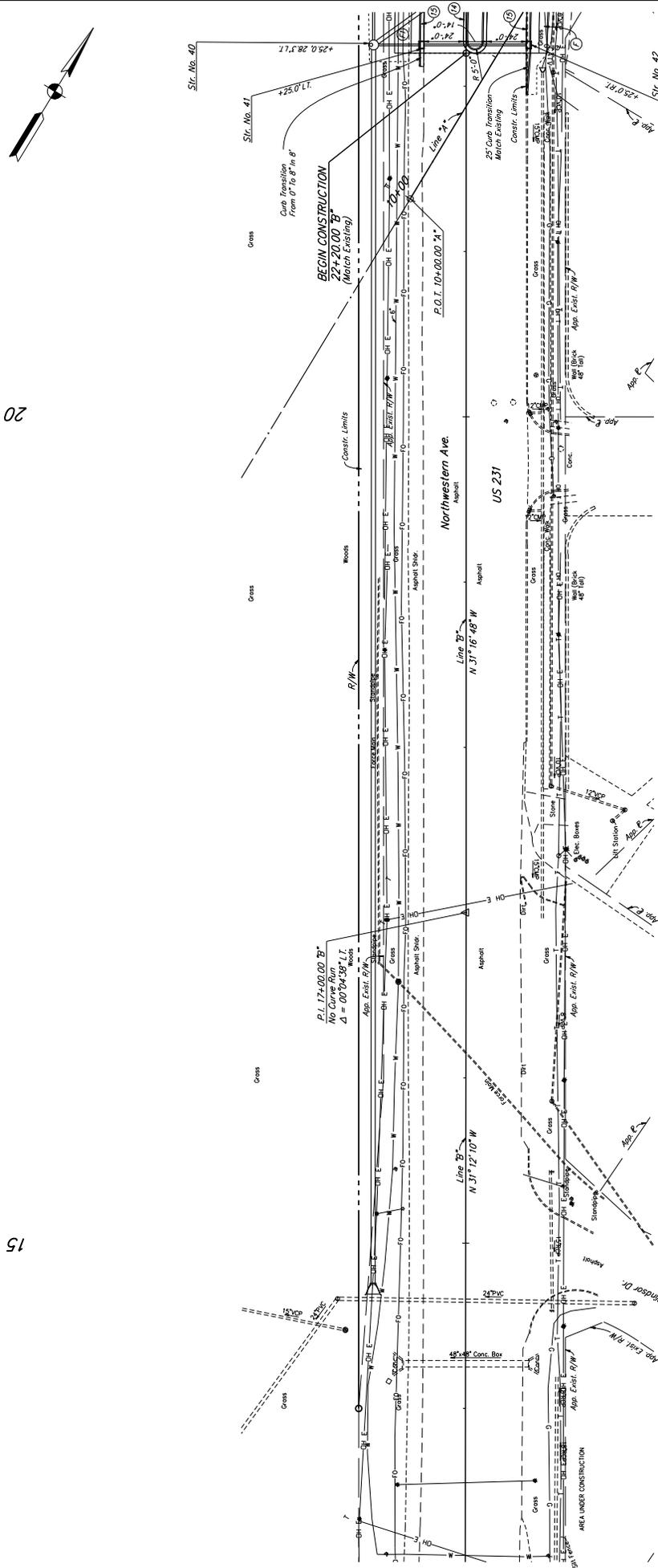
INDIANA	
DEPARTMENT OF TRANSPORTATION	
PLAN AND PROFILE	
LINE "B"	

LEGEND
 [B] Right of Way (R/W)
 [L] End of Right of Way (End R/W)



NOTES:
 All Road Are Along Front Face Of Curb, Except As Noted.
 For Storm Sewer Profile, See Plan And Profile Sheet No. XX.

INDIANA DEPARTMENT OF TRANSPORTATION CONSTRUCTION DETAIL LINE "A"		HORIZONTAL SCALE VERTICAL SCALE DESIGNATION NO. 060696	BRIDGE FILE DESIGNATION NO. 060696
RECOMMENDED FOR APPROVAL DESIGN ENGINEER DATE	DRAWN P.I.T. CHECKED M.P.B.	SURVEY BOOK CONTRACT SHEETS PROJECT NO.	SHEETS 19 of 46 PROJECT NO.
LEGEND			
(1) Concrete Curb (2) Modified Concrete Curb & Gutter Type C (3) Concrete Curb & Gutter (4) Modified Concrete Curb & Gutter Type B (5) Concrete Center Curb Type C (6) Sewing (7) Sodding, Nursery (8) 6" PCPP on Subgrade Treatment Type IIIA (9) 6" PCPP on Subgrade Treatment Type IIIA (10) 4" Concrete Sidewalk (11) Multi-Use Path (12) Concrete Curb Ramp (13) Type A5 (Indicated)	(14) Full Depth HMA Pavement (See Typical Sections) (15) Saw Cut (16) Concrete Curb Ramp (Type A5 Indicated)		



NOTES:
 All Road Are Along Front Face Of Curb, Except As Noted.
 For Storm Sewer Profile, See Plan And Profile Sheet No. 20.

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
N/A	060606
SURVEY BOOK	SHEETS
CONTRACT	PROJECT NO.
20	46

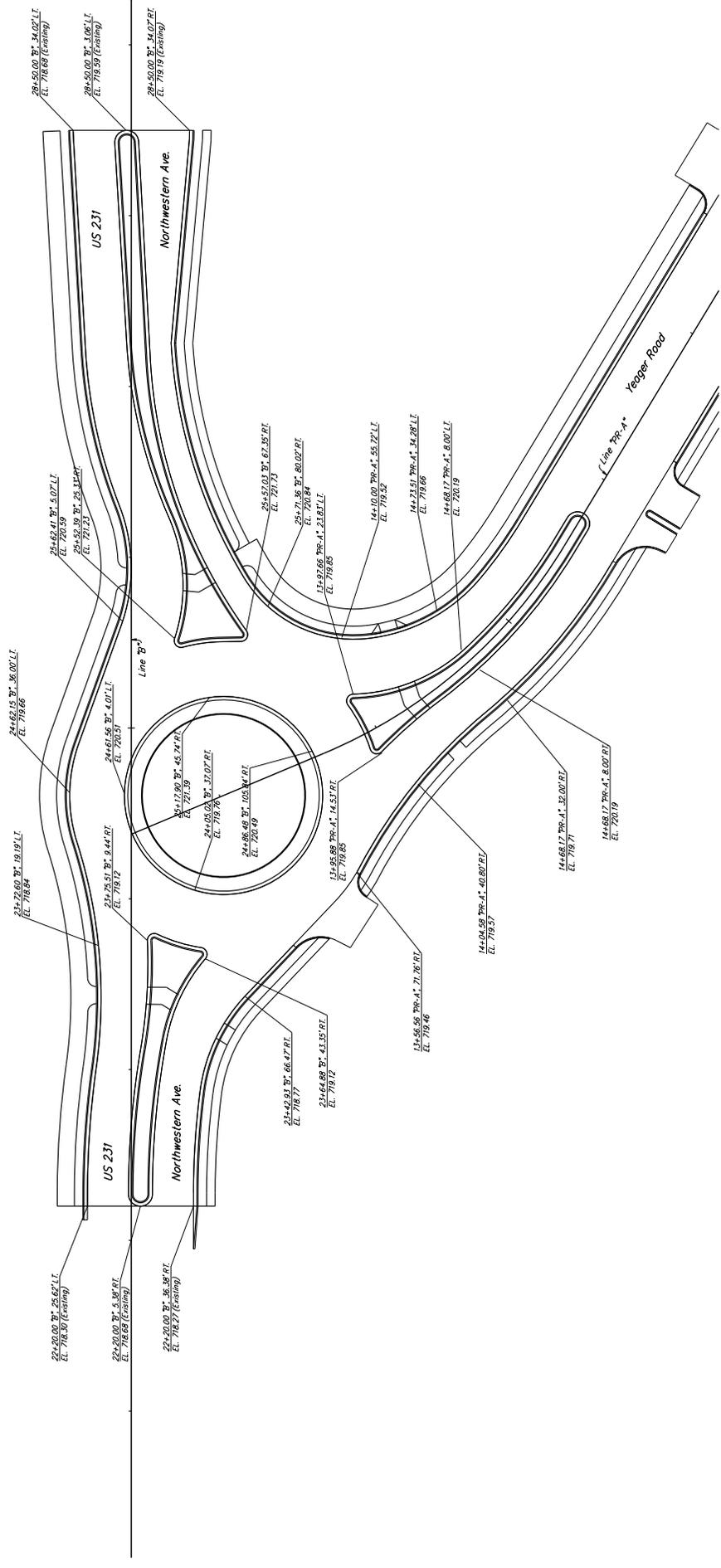
INDIANA
DEPARTMENT OF TRANSPORTATION
CONSTRUCTION DETAIL
LINE "B"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: PBT	
CHECKED: MPB	CHECKED: MPB	

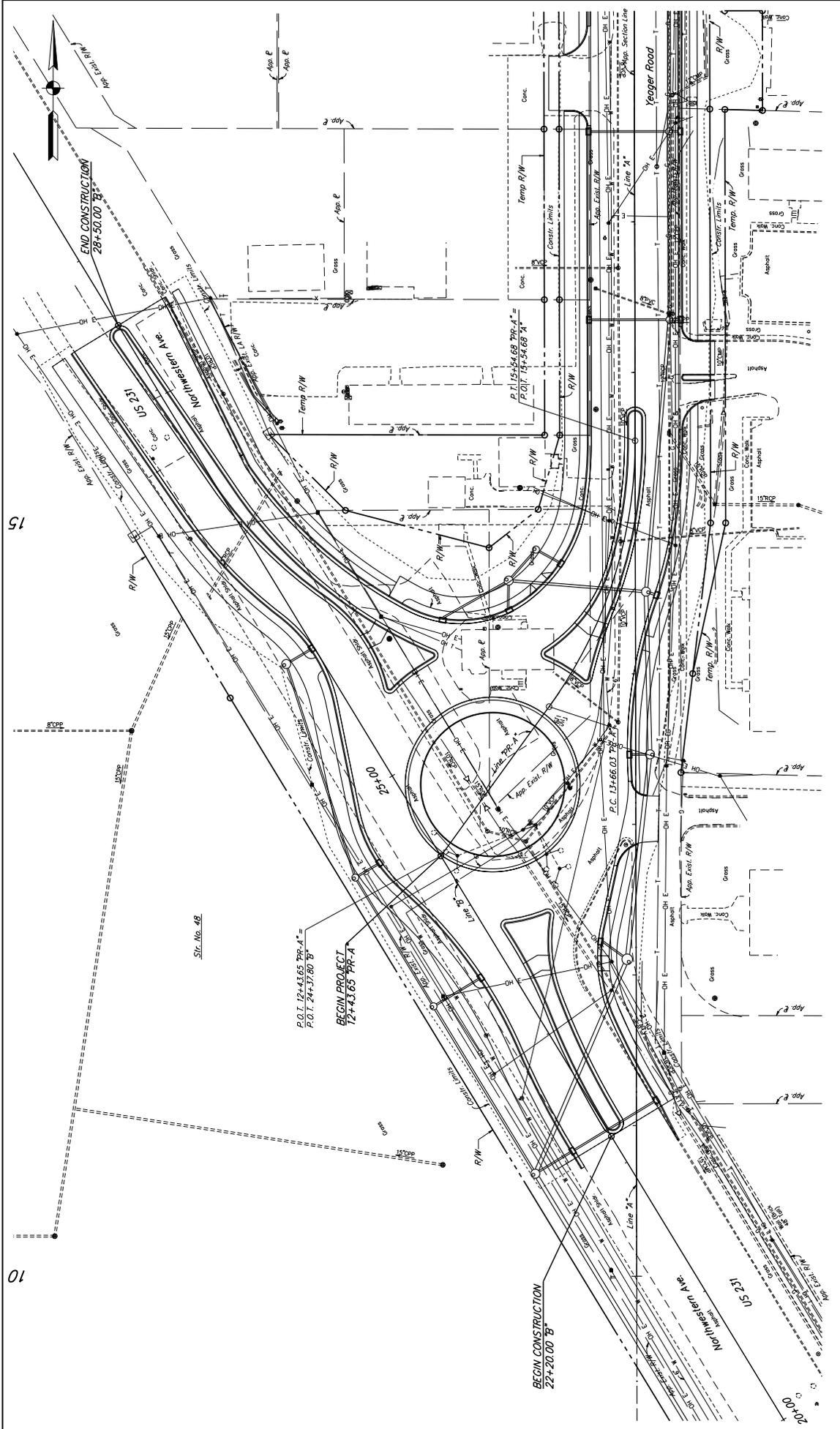
Full Depth HMA Pavement (See Typical Sections)
 Saw Cut
 Concrete Curb Ramp (Type A Indicated)

LEGEND

(1) Concrete Curb	(2) Concrete Center Curb Type C	(3) 9" PCPP on Subgrade Treatment Type IIIA	(4) Full Depth HMA Pavement (See Typical Sections)
(2) Modified Concrete Curb & Gutter Type C	(3) Seeding	(4) 4" Concrete Sidewalk	(5) Saw Cut
(3) Concrete Curb & Gutter	(4) Sodding, Nursery	(5) Multi-Use Path (See Typical Sections)	(6) Concrete Curb Ramp (Type A Indicated)
(4) Modified Concrete Curb & Gutter Type B	(5) 6" PCPP on Subgrade Treatment Type IIIA		



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	BRIDGE FILE
DESIGNED: JMT	CHECKED: MPB	DRAWN: JMT		SPOT ELEVATIONS		VERTICAL SCALE	DESIGNATION NO.
						N/A	0600696
						SURVEY BOOK	SHEETS
						CONTRACT	21 of 46
							PROJECT NO.



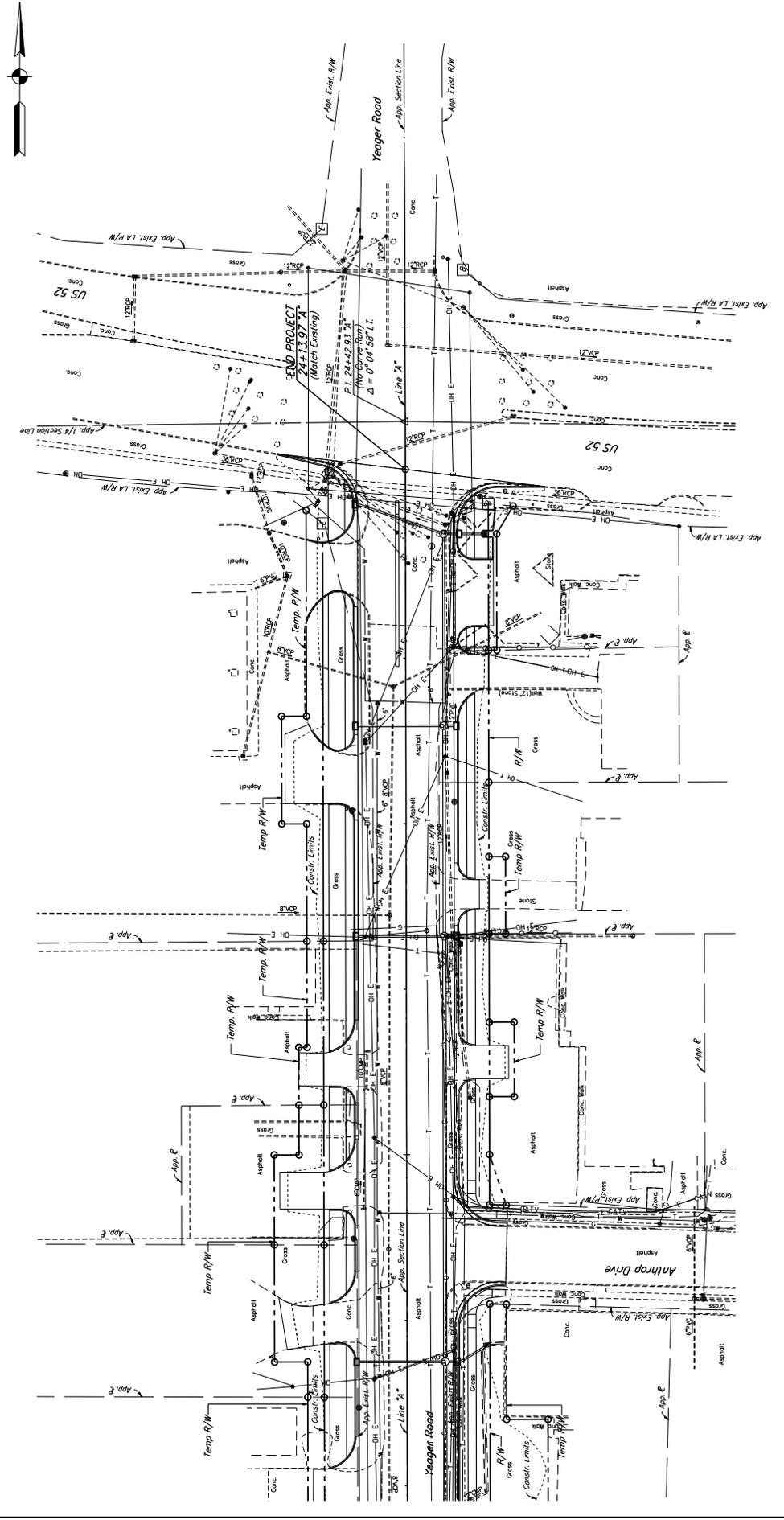
15

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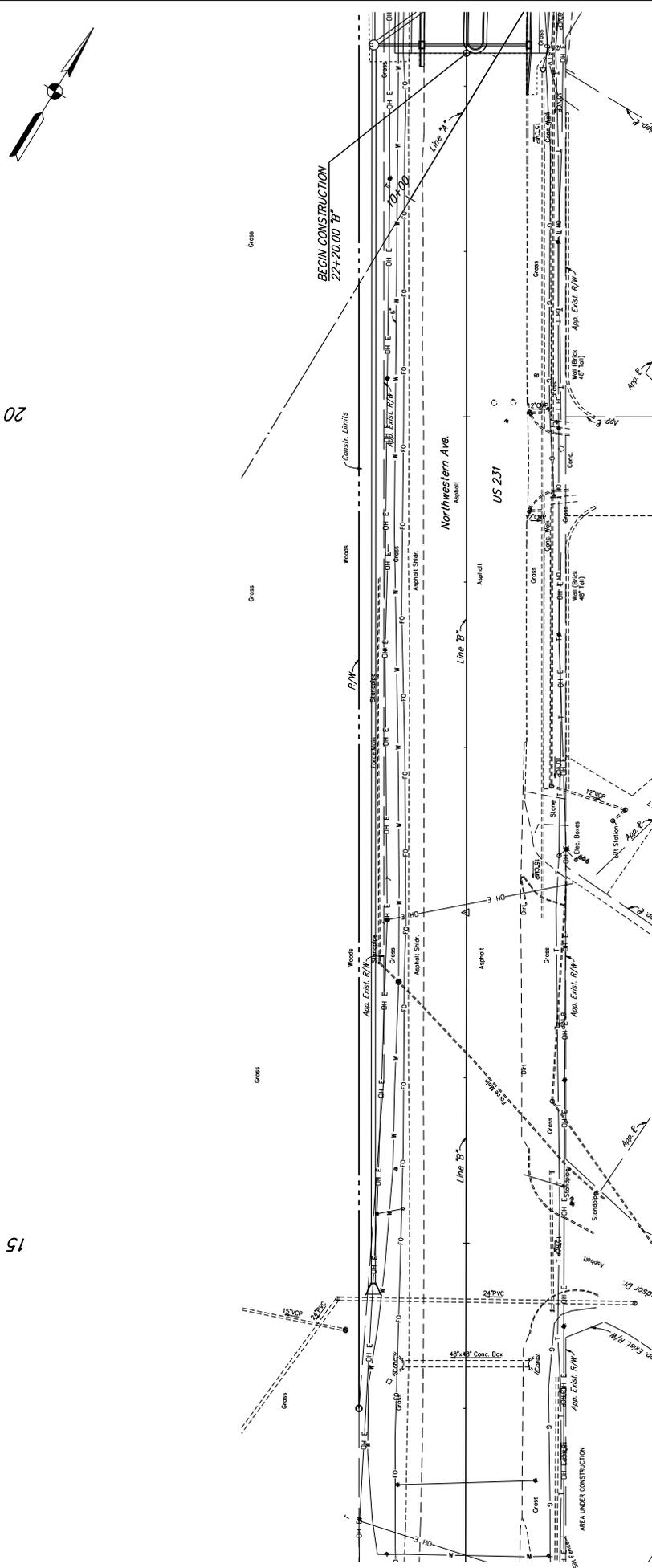
<p>INDIANA DEPARTMENT OF TRANSPORTATION EROSION CONTROL DETAIL LINE "PR-A", "A" & "B"</p>		<p>HORIZONTAL SCALE VERTICAL SCALE DESIGNATION NO. 0600696</p>
<p>RECOMMENDED FOR APPROVAL DESIGNED: JMT CHECKED: MPB</p>	<p>DESIGN ENGINEER DATE DRAWN: PJT CHECKED: MPB</p>	<p>SURVEY BOOK CONTRACT SHEETS PROJECT NO. 23 of 46</p>
<p>NOTES: Temporary Sealing shall be placed on all Disturb Areas. All Proposed Cut Inlets along the construction area shall be protected by a minimum approved by the City of West Lafayette and 2008 West Lafayette Standards and Specifications.</p>		
<p>LEGEND</p> <ul style="list-style-type: none"> Ditch Flow Line Ditch Inlet Projection Check Dam Riprap Riprap Retention 		
<p>6/12/2009 8:36:55 PM P:\INC006\172840 - Bridge\INC006\1728-IBD-EC-A.01.dgn</p>		

20

25



<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>EROSION CONTROL DETAIL</p> <p>LINE "A"</p>		<p>HORIZONTAL SCALE</p> <p>VERTICAL SCALE</p> <p>DESIGNATION NO.</p> <p>SURVEY BOOK</p> <p>CONTRACT</p> <p>BRIDGE FILE</p> <p>DESIGNATION NO.</p> <p>SHEETS</p> <p>PROJECT NO.</p>
<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGNED: JMT</p> <p>CHECKED: MPB</p>		<p>DATE</p> <p>DRAWN: PJT</p> <p>CHECKED: MPB</p>
<p>NOTES:</p> <p>Temporary Seeding shall be placed on all Disturb Areas.</p> <p>All Proposed Cut Limits along the construction area shall be protected by a silt fence approved by the City of West Lafayette and 2008 MS&T Standards and Specifications.</p>		
<p>LEGEND</p> <p> Ditch Inlet Protection</p> <p> Ditch Flow Line</p> <p> Silt Fence</p> <p> Check Dam Riprap</p> <p> Riprap Revetment</p>		



<p>LEGEND</p> <p> Ditch Inlet Projection Ditch Flow Line Check Dam Riprap Riprap Revestment </p>	<p>NOTES:</p> <p>Temporary Staking shall be placed on all Disturb Areas.</p> <p>All Proposed Cut Limits along the construction area shall be protected by a minimum approved by the City of West Lafayette and EOB Road Standards and Specifications.</p>	<p>RECOMMENDED FOR APPROVAL: _____ DATE: _____</p> <p>DESIGNED: JMT DRAWN: PJT</p> <p>CHECKED: MPB CHECKED: MPB</p>	<p>HORIZONTAL SCALE: _____</p> <p>VERTICAL SCALE: N/A</p>	<p>BRIDGE FILE: _____</p> <p>DESIGNATION NO.: 06060696</p>
			<p>INDIANA DEPARTMENT OF TRANSPORTATION</p> <p>EROSION CONTROL DETAIL</p> <p>LINE "B"</p>	<p>SURVEY BOOK: _____</p> <p>CONTRACT: _____</p>

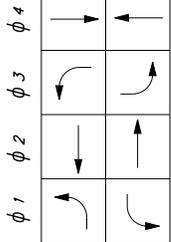


LOOP TAGGING TABLE

LANE	TAG NUMBER
ML	ML3 - 1, 2, 3
MA	MA8 - 1, 2, 3
MP	MP8 - 1, 2, 3
ME	ME5 - 1, 2, 3
MA	MA2 - 1, 2, 3, 4
MP	MP2 - 1, 2, 3, 4
ME	ME1 - 1, 2, 3
EA	EA6 - 1, 2, 3, 4
EB	EB6 - 1, 2, 3, 4
ER	ER6 - 1
SA	SA7 - 1, 2, 3
SA	SA4 - 1, 2, 3

ALL LOOP AMPLIFIERS SHALL BE COUNT CHAIRABLE

PHASE DIAGRAM

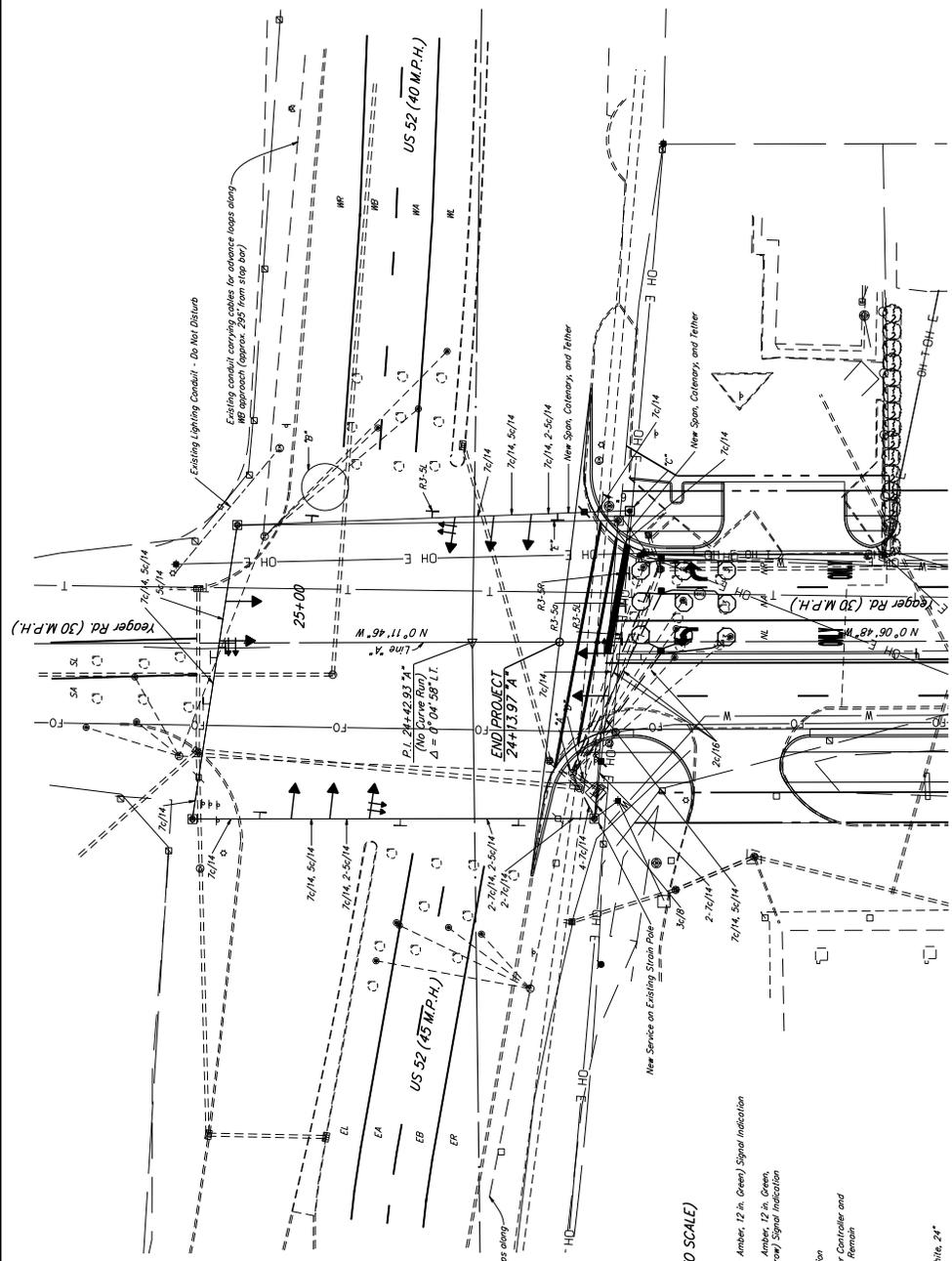


ALL APPROACH FLASHES RED

- Text Legends:**
- * = Adjust Casting to Grade
 - ⊕ = Location of existing conduits for the loop detectors along the NB approach per the as-built signal plan (Last Revision - 03/21/2009) and field verification.
 - ⊖ = Signal Strain Pole and Foundation to be Removed
 - ⊕ = 4" x 7/16" x 3-2/16"
 - ⊖ = Remove Existing Street Name Sign on New Span

SIGNAL DESIGN PLAN FOR REVIEW

INDIANA	
DEPARTMENT OF TRANSPORTATION	
DESIGN ENGINEER	DATE
TRAFFIC SIGNAL DETAIL	
VEEGER ROAD & US 52	
WEST LAFAYETTE, INDIANA	
CONTRACT NO.	DRAWN BY
COMM. NO. 01-079-035	HRS
DES. NO.	SCALE
PROJECT NO.	YEAR SHEET TOTAL
	2009 26



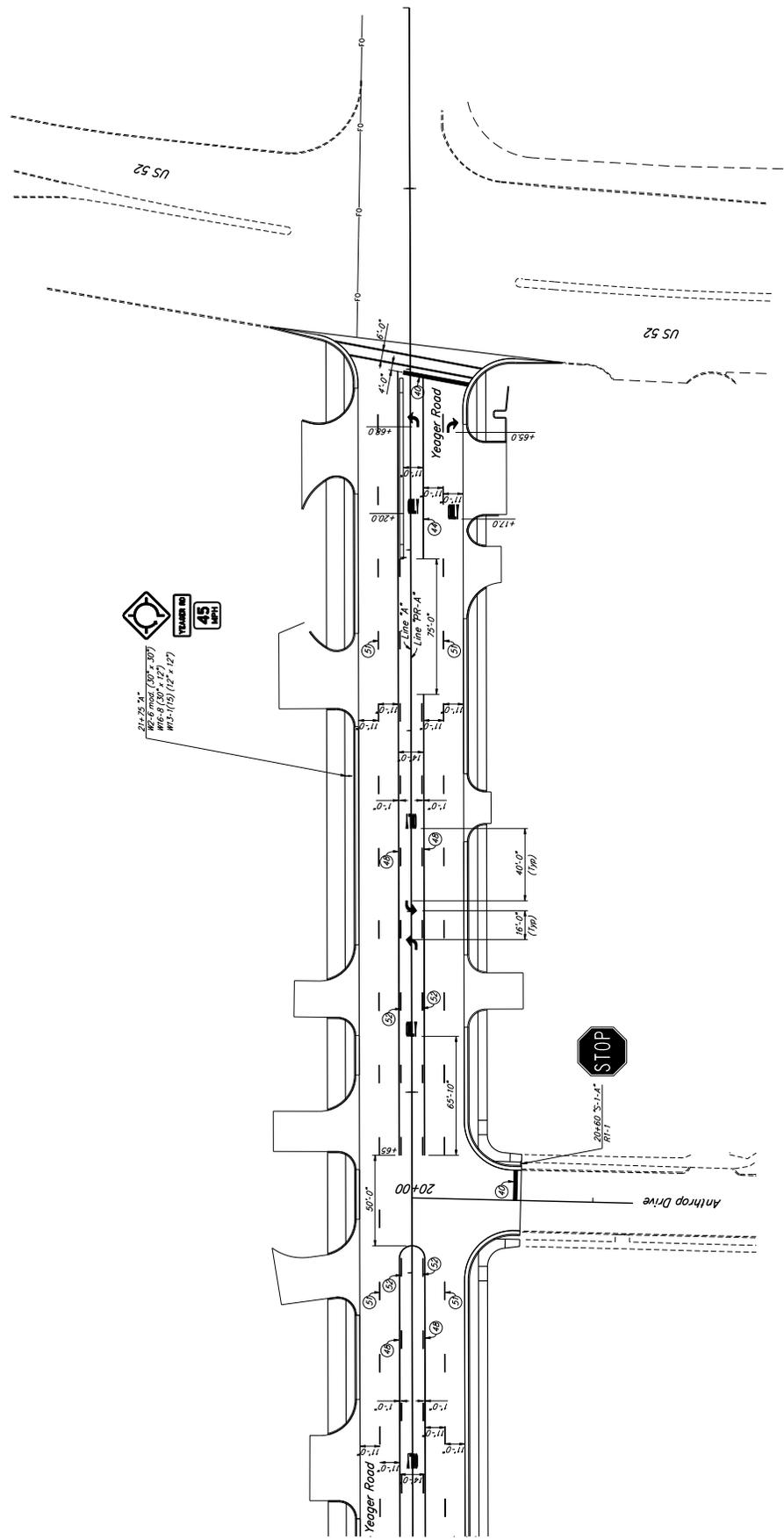
General Notes:

- 2009 INDOT Standard Specifications and the Indiana Revisions to the 2008 Manual of Uniform Traffic Control Devices Shall Govern the Modification of Traffic Signal and Signs Shown on this Plan.
- Existing Loop Detection and Wiring for the EB, NB and SB Approaches Shall Remain in Place. New Loop Detection to be Constructed on NB Approach per Plan.
- All the Signal Conduits Shall Remain in Place Except for the Conduits Crossing Across Veeger Road for the Loop Detection Along the NB Approach
- Provide New Signal Heads on All Spans. Run New Signal Wiring for New Signal Heads through the Existing Conduits. Existing Handholes and Existing Controller Cabinet. All Existing Signal Cables to the Existing Signal Indicators Shall be Removed from the Applicable Existing Conduits.
- North and West Signal Span Shall Remain in Place. Existing Disconnect Hanger and Signs on these Spans Shall Also Remain in Place.
- All Signal Poles and Foundation Shall Remain in Place except for the One on the South-East Corner, which shall be removed.
- New Stop Bar Required for the NB approach. Stopbar Shall be Thermoplastic, Solid, White 24" Lines
- Exact location of the Signal Conduits Shall be Field Verified by the Signal Contractor at the time of Construction.
- For Accurate Counting Purposes, Loops and Pavement Markings Must be Installed in the Location Shown on the Plan.

SIGNAL LEGEND (NOT TO SCALE)

- New 3 Section LED (12 in. Red, 12 in. Amber, 12 in. Green) Signal Indication
- New 2 Section LED (12 in. Red, 12 in. Amber, 12 in. Green, 12 in. Amber Arrow, 12 in. Green Arrow) Signal Indication
- Existing Signal Handhole to Remain
- New 30" Steel Strain Pole & Foundation
- Existing 8 Phase Fully Actuated Master Controller and Cabinet on Existing Foundation to Remain
- New Loop Detector (4 Turn Stripes)
- New Detector Housing
- Existing Detector Housing
- New Stop Bar, Thermoplastic, Solid, White, 24"
- New 2" Galvanized Steel Conduit
- New Overhead Sheet Sign (As Specified on Plan)
- Existing 30" Steel Strain Pole & Foundation to Remain Unless Specified
- Existing Overhead Sheet Sign to Remain (As Specified on Plan)
- Existing Loop Detector to Remain (4 Turn Stripes)
- Existing 2" Conduit to Remain
- Existing Disconnect Hanger to Remain (Span Mounted Junction Box)
- New Signal Span
- Existing Signal Span to Remain

** Existing Loop Detectors and Wiring Shall Remain in Place for Approaches of this Intersection, Except for the NB Veeger Road Approach



31'-25" ±
 162'-8" max. (107' x 307')
 116'-8" (107' x 127')
 112'-11" (112' x 127')

RECOMMENDED FOR APPROVAL: _____ DATE: _____ DESIGN ENGINEER: _____ DESIGNED: JMT DRAWN: PJT CHECKED: MPB CHECKED: MPB		HORIZONTAL SCALE: _____ BRIDGE FILE: _____ VERTICAL SCALE: N/A DESIGNATION NO.: 0600696 SURVEY BOOK: _____ SHEETS: _____ of 46 CONTRACT: _____ PROJECT NO.: _____	
INDIANA DEPARTMENT OF TRANSPORTATION			
PAVEMENT MARKING DETAILS			
4 Pavement Message Markings, Thermoplastic; Line Indicator Arrows, Thermoplastic; Pavement Message Markings, Thermoplastic; Fishhook Arrow Word ("FIELD") Word ("DUE")		5 Line, Thermoplastic; Broken, White, 4 in. 6 Line, Thermoplastic; Broken, White, 6 in. (C Line, J cap) 7 Line, Thermoplastic; Broken, Yellow, 4 in. 8 Transverse Markings, Thermoplastic; Irregular Flare Line	
9 Line, Thermoplastic; Solid, White, 4 in. 10 Line, Thermoplastic; Solid, White, 6 in. 11 Line, Thermoplastic; Solid, White, 8 in. 12 Line, Thermoplastic; Solid, White, 12 in. 13 Line, Thermoplastic; Solid, White, 12 in.		14 Line, Thermoplastic; Solid, Yellow, 4 in. 15 Line, Thermoplastic; Solid, Yellow, 6 in. 16 Line, Thermoplastic; Solid, Yellow, 8 in. 17 Line, Thermoplastic; Solid, Yellow, 12 in.	

Appendix C: Early Coordination

- a. Early Coordination Mailing List
- b. Early Coordination Letter – May 5, 2008
- c. Indiana Department of Transportation, Office of Aviation – May 8, 2009
- d. Natural Resources Conservation Service- May 13, 2009
- e. Tippecanoe County Sherriff's Department- May 22, 2008
- f. US Environmental Protection Agency, Sole Source Aquifer- May 28, 2009
- g. Department of Natural Resources- Division of Outdoor Recreations- June 3, 2008
- h. Indiana Geological Survey- June 13, 2008
- i. Department of Natural Resources- May 8, 2008
- j. US Fish and Wildlife Service- May 13, 2008
- k. US Coast Guard- May 22, 2008
- l. Indiana Department of Environmental Management – May 6, 2008

Saltu	Name	Title	Attn:	Agency/Company	Address 1	Address 2	City	State	Zip
Ms. McCloskey	Ms. Elizabeth McCloskey	, Supervisor		US Fish and Wildlife Service	Northern Indiana Ecological Services Suboffice	PO Box 2616	Chesterton	Indiana	46304-2616
Ms. Hardisty	Ms. Jane Hardisty			US Natural Resources Conservation Service	6103 Lakeside Boulevard		Indianapolis	Indiana	46278
Ms. Hasenmueller	Ms. Nancy Hasenmueller	, Head		Indiana Geological Survey	Indiana University	611 North Walnut Grove	Bloomington	Indiana	47405
Mr. Keefer	Mr. Jim Keefer	, Manager		INDOT - Aeronautics Section	901 Government Center North	100 North Senate Avenue	Indianapolis	Indiana	46204
Mr. Easterly	Mr. Thomas Easterly	, Commissioner		Indiana Department of Environmental Management	Submit electronically - DO NOT MAIL		Indianapolis	Indiana	
Mr. Carter	Mr. Robert Carter	, Director		Indiana Dept. of Natural Resources	264 Government Center West	402 West Washington Street			
Mr. Poy	Mr. Tom Poy	, Sole Source Coordinator		US EPA, Region 5	77 West Jackson Blvd., W-G-15J		Indianapolis	Indiana	46204
Mr. Tally	Mr. Robert Tally, Jr., PE			Federal Highway Administration - US DOT	254 Federal Office Building	575 North Pennsylvania Street	Indianapolis	Indiana	46204
Mr. Shelton	Mr. Doug Shelton	, Chief, Regulatory Office		US Army Corps of Engineers, Louisville District	Regulatory Branch North	PO Box 59	Louisville	Kentucky	40201-0059
Mr. Bloom	Mr. Robert Bloom	, Commander		Ninth Coast Guard District	1240 East 9th Street		Cleveland	Ohio	44199
Ms. Ostby	Ms. Susan Ostby			Division of Outdoor Recreation	402 West Washington Street, Room W271		Indianapolis	Indiana	46204
Mr. Monahan	Mr. Mike Monahan	, District Director		Indiana Department of Transportation	LaPorte District	315 East Boyd Boulevard	LaPorte	Indiana	46352-0429
Ms. Kuhl	Ms. Opal Kuhl	, Director		Tippecanoe County Highway Department	Tippecanoe County Office, First Floor	20 North 3rd Street	Lafayette	Indiana	47901
Sheriff Brown	Mr. Tracy Brown	, Sheriff		Tippecanoe County Sheriffs Department	3640 Duane Road		Lafayette	Indiana	47904
Mr. Bohlin	Mr. Michael D. Bohlin	, Health Officer		Tippecanoe Health Department	Environmental & Food Divisions	20 North 3rd Street	Lafayette	Indiana	47904
Mr. Nail	Mr. Allen Nail	, Director		Tippecanoe County Parks Department	4449 State Road 43 North		Lafayette	Indiana	47906
Mayor Dennis	Mr. John R. Dennis	, Mayor		West Lafayette Engineering Department	West Lafayette City Hall, 2nd Floor	609 Navajo Street	West Lafayette	Indiana	47906
Chief Dombkowski	Mr. Jason Dombkowski	, Chief		City of West Lafayette Mayor's Office	West Lafayette City Hall	610 Navajo Street	West Lafayette	Indiana	47906
Chief Drew	Mr. Philip Drew	, Chief		City of West Lafayette Police Department	711 Navajo Street		West Lafayette	Indiana	47906
Mr. Payne	Mr. Joe Payne	, Superintendent		City of West Lafayette Parks and Recreation Department	531 Navajo Street		West Lafayette	Indiana	47906
Dr. Killion	Dr. Rocky D. Killion	, Superintendent of Schools		City of West Lafayette Community School Corporation	West Lafayette City Hall	609 West Navajo Street	West Lafayette	Indiana	47906
Dr. Wood	Dr. Richard Wood	, Superintendent		Tippecanoe School Corporation	1130 N. Salisbury Street		West Lafayette	Indiana	47906
Mr. Sennett	Mr. Martin Sennett	, General Manager		Greater Lafayette Public Transportation Corporation	21 Elston Road		Lafayette	Indiana	47909
					City Bus	P. O. Box 588	Lafayette	Indiana	47902

May 5, 2008

Names

Re: Yeager Road Improvement Project
West Lafayette, Indiana
Des. No. 0600696

Dear

The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 & 12, Township 23 North, Range 4 West, Wabash Township, Indiana.

Yeager Road is a north-south arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy Purdue event traffic. Presently Yeager Road is a 3-lane arterial with multiple private and commercial driveways.

The alignment for Yeager Road will not change except possibly the intersection with Northwestern Avenue. The pavement is in fair condition; however, the plan is to fully reconstruct the roadway with full-depth pavement. With reconstruction, the roadway cross section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. For non-motorized travel, sidewalks with trail connections will be constructed. The typical section will fit within an 80-foot minimum right-of-way width.

Land use within the immediate vicinity of the project is commercial and residential. Multi-family residential buildings are present within the project limits. Strip right-of-way acquisition is anticipated however no business or residential relocations will be necessary.

You are asked to review the proposed project and provide any comments you may have relative to anticipated impacts of the project on the areas in which you have jurisdiction or special expertise. To facilitate the development of this project, you are asked to reply by Friday, June 27, 2008. If no response is received by that date, it will be assumed you have no comments at the present time. Your timely cooperation in the development of this project will be appreciated. Please contact us at (317) 547-5580 if there are any questions or if additional information is needed.

Very truly yours,
American Structurepoint, Inc.

Brianna J. Winsett
Environmental Specialist

cc: US Fish and Wildlife Service
US Natural Resources Conservation Service
Indiana University Geology Section
INDOT- Aeronautics Section
Indiana Department of Environmental Management, electronic
Indiana Department of Natural Resources, Environmental Review Section
US EPA Groundwater and Drinking Water Branch
Federal Highway Administration
US Ninth Coast Guard
Department of Natural Resources, Division of Outdoor Recreation, electronic
INDOT- LaPorte District
Tippecanoe County Highway Department
Tippecanoe County Sheriffs Department
Tippecanoe Health Department
Tippecanoe County Parks Department
West Lafayette Engineering Department
City of West Lafayette Mayor's Office
City of West Lafayette Police Department
City of West Lafayette Fire Department
City of West Lafayette Parks and Recreation Department
West Lafayette Community School Corporation
Tippecanoe School Corporation
Greater Lafayette Public Transportation Corporation

**Questionnaire for the Indiana Department of Transportation,
Office of Aviation**

Project No: _____ **Des/Bridge No:** 0600696 _____

Project Description:

Yeager Road Improvement Project

West Lafayette, IN

Requested By:

American Structurepoint Inc.

Are there any existing or proposed airports within or near the project limits? Yes

If yes, describe any potential conflicts with air traffic during or after the construction of the project.

This project should have no impact on airspace or air

navigation. However, Purdue Airport is a public-use airport

and is located approximately 12,700 feet south of the

proposed project site.

This information was furnished by:

Name: Justin Klump
Title: Project Manager – INDOT Office of Aviation
Date: 05/08/2008

United States Department of Agriculture



Natural Resources Conservation Service
6013 Lakeside Blvd.
Indianapolis, IN 46268

May 13, 2008

Brianna J. Winsett
Environmental Specialist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Dear Ms. Winsett:

The proposed project to reconstruct Yeager Road in the City of West Lafayette, Tippecanoe County, Indiana, as stated in your letter dated May 5, 2008, will not cause a conversion of prime farmland.

If you need further information, please contact Lisa Bolton at 317-290-3200, extension 342.

Sincerely,

A handwritten signature in black ink that reads "Jane E. Hardisty". The signature is written in a cursive style.

JANE E. HARDISTY
State Conservationist

Helping People Help the Land

An Equal Opportunity Provider and Employer



TIPPECANOE COUNTY SHERIFF'S OFFICE

Tracy A. Brown, Sheriff
2640 Duncan Road
Lafayette, IN 47904

Emergency 911
Office 765-423-9388
FAX 765-423-4155

Communications 765-423-9321
Jail Division 765-423-1655
www.tippecanoesheriff.com

May 22, 2008

American Structurepoint, Inc.
Ms. Brianna J. Winsert, Environmental Specialist
7260 Shadeland Station
Indianapolis, IN 46256

RE: Yeager Road Improvement Project, West Lafayette, Indiana Des. No. 0600696

Dear Ms. Winsert,

I received your letter dated May 5th, in regards to the plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette. I am in full support of the project, however, my one and only concern is that you do not undertake this project at any time during the Purdue University Football Season.

Event traffic can be challenging enough, but to manage that during a time of heavy construction would certainly create havoc with the traffic flow to and from the stadium, not to mention the increased safety issues it could create for motorists, pedestrians, as well as laborers, workers and equipment. The Purdue University Football Season generally goes from mid August to late November or early December. Therefore, I am confident an appropriate project time line could be considered.

Should you require any further information, or assistance, please do not hesitate to contact me at my office number listed above. Thank you for taking the time to address this matter with me.

Sincerely,

Tracy A. Brown, Sheriff
Tippecanoe County, Indiana

TAB/plb



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION 5
77 WEST JACKSON BOULEVARD
CHICAGO, IL 60604-3590

May 28, 2008

Brianna J. Winsett
American Structurepoint Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

**Re: Sole Source Aquifer Inquiry
Yeager Road Improvement Project
Des No. 0600696**

Dear Ms. Winsett:

In reply to your letter about the above referenced project, please note that the location of West Lafayette, Indiana is not located within a Sole Source Aquifer, nor are there any pending petitions or designations.

While this project is not subject to Sole Source Aquifer review, we suggest that during construction and operation appropriate safeguards are in place to ensure that ground water is not endangered. Such safeguards would include securing adequate precautions for fueling/servicing large equipment, utilizing "green infrastructure" where applicable and developing contingency plans to handle the release of any hazardous materials.

Thank you for your cooperation. If you have any further questions please call me at (312) 886-4313.

Sincerely,

A handwritten signature in black ink, appearing to read "Cary McElhinney".

Cary McElhinney
Ground Water and Drinking Water Branch

June 3, 2008

Brianna J. Winsett, Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

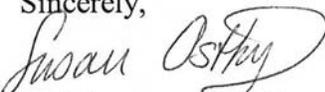
Re: Yeager Road Improvement Project, West Lafayette, Tippecanoe County, Indiana
DES #: 0600696

Dear Ms. Winsett:

This letter is in response to your request for a 6(f)3 determination regarding Yeager Road Improvement Project, West Lafayette, Tippecanoe County, Indiana. Through your description of the project, our department determined there will be no negative effect on any site currently encumbered under 6(f)3 through the federally sponsored Land and Water Conservation Fund (LWCF). There is no LWCF property in the immediate vicinity; therefore there will be no taking of LWCF property out of outdoor recreational use. Thank you for consulting with our department.

If you have other question or concerns please do not hesitate to contact Susan Ostby at 317-232-4074.

Sincerely,


(for) Bob Bronson, Chief

State and Community Outdoor Recreation Planning Section
Division of Outdoor Recreation, IDNR

RJB:sdo

Project No. _____ Des. No. 0600696

Project Description: Yeager Road Improvement Project

Name of Organization requesting early coordination:

American Structurepoint Inc.

QUESTIONNAIRE FOR THE INDIANA GEOLOGICAL SURVEY

1) Do unusual and/or problem () geographic, () geological, () geophysical, or () topographic features exist within the project limits? Describe:
No

2) Have existing or potential mineral resources been identified in this area? Describe:
No

3) Are there any active or abandoned mineral resources extraction sites located nearby? Describe: No

This information was furnished by:

Name: Jennifer Olejnik Title: Geologist

Address: 611 North Walnut Grove Bloomington, IN 47405

Phone: 812-855-1347 Date: 5/30/2008

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Water

Early Coordination/Environmental Assessment

DNR #: ER-13327 **Request Received:** May 8, 2008

Requestor: American Structurepoint, Inc
Brianna J Winsett
7260 Shadeland Station
Indianapolis, IN 46256-3957

Project: Yeager Road improvements from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52), West Lafayette; Des #0600696

County/Site info: Tippecanoe

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

Regulatory Assessment: Formal approval by the Department of Natural Resources under the regulatory programs administered by the Division of Water is not required for this project.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Fish, wildlife, and botanical resource losses as a result of this project should be minimal.

Contact Staff: Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife
Our agency appreciates this opportunity to be of service. Please do not hesitate to contact the above staff member at (317) 232-4160 or 1-877-928-3755 (toll free) if we can be of further assistance.



J. Matthew Buffington
Environmental Supervisor
Division of Fish and Wildlife

Date: July 23, 2008



United States Department of the Interior
Fish and Wildlife Service



Bloomington Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

May 13, 2008

Ms. Brianna J. Winsett
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Project No.: Des. 0600696
Project: Yeager Road Improvements
Location: West Lafayette, Tippecanoe County

Dear Ms. Winsett:

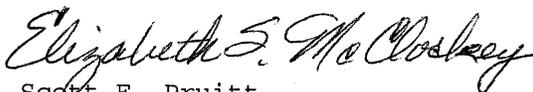
This responds to your letter dated May 5, 2008, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project will have no effect on wetlands or other significant habitat types. Project impacts are expected to be minor in nature. Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised list be published, it will be necessary for the Federal agency to reinitiate consultation.

We appreciate the opportunity to comment at this early stage of project planning. If you have any questions, please contact Elizabeth McCloskey at (219) 983-9753 or elizabeth_mccloskey@fws.gov.

Sincerely yours,


for Scott E. Pruitt
Supervisor

cc: Federal Highway Administration, Indianapolis, IN
Christie Stanifer, Indiana Division of Water, Indianapolis, IN

U.S. Department of
Homeland Security

United States
Coast Guard



Commander
Eighth Coast Guard District

1222 Spruce Street
St. Louis, MO 63103-2832
Staff Symbol: dwb
Phone: (314) 269-2380
Fax: (314) 269-2737
Email: peter.j.sambor@uscg.mil

16591.1
May 22, 2008

Ms. Brianna Winsett
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3957

Subj: YEAGER ROAD IMPROVMENT PROJECT, TIPPECANOE COUNTY

Dear Ms. Winsett:

We have reviewed the information provided in your letter of May 5, 2008 and determined that this project is not a project over which the Coast Guard exercises jurisdiction for bridge administration purposes. A Coast Guard permit is not required.

If there are any questions, please contact Mr. Peter Sambor at the above listed number. We appreciate the opportunity to comment on the project.

Sincerely,

A handwritten signature in black ink, appearing to read "R. Wiebusch".

ROGER K. WIEBUSCH
Bridge Administrator
By direction of the District Commander



Indiana Department of Environmental Management

We make Indiana a cleaner, healthier place to live.

Mitchell E. Daniels, Jr.
Governor

100 North Senate Avenue
Indianapolis, Indiana 46206

Thomas W. Easterly
Commissioner

(317) 232-8603
800) 451-6027
www.IN.gov/idem

City of West Lafayette, Office of Engineer
609 West Navajo Drive
West Lafayette, IN 47906

American Structurepoint, Inc
Brianna J. Winsett
7260 Shadeland Station
Indianapolis, IN 46256

Tuesday, May 06, 2008

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana. The alignment for Yeager Road will not change, except possibly the intersection with Northwestern Avenue. The pavement is in fair condition; however, the plan is to fully reconstruct the roadway with full-depth pavement. With reconstruction, the roadway cross section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. For non-motorized travel, sidewalks with trail connections will be constructed. The typical section will fit within an 80-foot minimum right-of-way width.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to enquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: http://www.idem.IN.gov/enviroreview/hwy_earlyenviroreview.html.

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland

Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE [Permits and Public Notices \(http://www.lrl.usace.army.mil/orf/default.asp\)](http://www.lrl.usace.army.mil/orf/default.asp) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/programs/water/401/reglinks.html>. IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/idem/programs/water/401/overview.html>.
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, visit: <http://www.in.gov/idem/programs/water/401/isowetlands.html>. You also may contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: www.in.gov/idem/programs/water/401/staff.html for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the follow statutes:
 - o IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - o IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - o IC 14-28-1 Flood Control Act 310 IAC 6-1
 - o IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - o IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - o IC 14-29-4 Construction of Channels Act No related code

For Internet links to these Indiana (statutory) Code and Indiana Administrative Code citations, see the table "DNR Statutes Addressing Dredging and Fill Placement" on the IDEM Web site at: <http://www.in.gov/idem/permits/guide/water/dredgingpermits.html#overviewdredg>. Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - o <http://www.in.gov/idem/permits/water/wastewater/wetwthr/storm/rule5.html>

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/permits/water/wastewater/wetwthr/storm/rule5defs.html#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF], pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html>).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-

submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/permits/water/wastewater/wetwthr/storm/rule13.html>.

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for addition project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits. (<http://www.in.gov/idem/permits/guide/water/drinkingwater.html>)
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits. (<http://www.in.gov/idem/permits/guide/water/wvconstructionpermits.html>)

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (www.in.gov/idem/permits/guide/air/openburning.html#maintenance) under specific conditions (www.in.gov/idem/permits/guide/air/air/openburning.html#conditionsallowed). You also can seek an open burning variance from IDEM. See: www.in.gov/idem/permits/guide/air/openburning.html#variances).

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized. See: www.in.gov/idem/permits/guide/air/fugitivedust.html.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: http://www.in.gov/idem/your_environment/radon/health.html.)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that

radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit:

http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf.) It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>, http://www.in.gov/idem/your_environment/radon/, or <http://www.epa.gov/radon/index.html>.

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdiv/44593.pdf>.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: www.in.gov/idem/guides/permit/waste/asbestosremoval.html.

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/idem/guides/permit/waste/leadabatement.html>.
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.ai.org/legislative/iac/t03260/a00020.pdf.) New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: www.in.gov/idem/permits/guide/air/index.html, or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD atdem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/permits/guide/waste/index.html>.
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.

- 5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
- 6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/idem/permits/guide/waste/ust.html>.

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period. For additional information and forms: www.in.gov/idem/permits/guide/landdevelopment/notification.html.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at http://www.idem.IN.gov/enviroreview/hwy_earlyenviroreview.html, is used.

Sincerely,



Thomas W. Easterly
Commissioner

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

Project Description

The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana. The alignment for Yeager Road will not change, except possibly the intersection with Northwestern Avenue. The pavement is in fair condition; however, the plan is to fully reconstruct the roadway with full-depth pavement. With reconstruction, the roadway cross section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. For non-motorized travel, sidewalks with trail connections will be constructed. The typical section will fit within an 80-foot minimum right-of-way width.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Date: _____

Signature of the INDOT
Project Engineer or Other Responsible Agent _____

Date: 12/18/09

Signature of the
For Hire Consultant

A handwritten signature in black ink that reads "Brianna Chapman". The signature is written in a cursive style and is positioned above a horizontal line.

Brianna J. Winsett

Representing the Company: American Structurepoint, Inc

Appendix D: Section 106 Coordination, Findings, Determination and Finding
a. Section 106 Documentation and Findings



AMERICAN
STRUCTUREPOINT
INC.

Section 106 Documentation FHWA Indiana Division

YEAGER ROAD IMPROVMENTS
NORTHWESTERN AVENUE TO SAGAMORE PARKWAY
CITY OF WEST LAFAYETTE, TIPPECANOE COUNTY, INDIANA
DES. NO. 0600696
FEDERAL PROJECT NO.: PENDING

PREPARED FOR:

Indiana Department of Transportation
Office of Environmental Sciences
Cultural Resources Section
Room N642 Indiana Government Center North
100 North Senate Avenue
Indianapolis, Indiana 46204

PREPARED BY:

American Structurepoint, Inc.
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July 16, 2009
Updated September 24, 2009



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- A. List of Consulting Parties
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**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR SECTION 800.5 (c)
YEAGER ROAD IMPROVEMENT PROJECT
WEST LAFAYETTE, TIPPECANOE COUNTY, INDIANA
DES NO.: 0600696;
FEDERAL PROJECT NO.: PENDING**

1. DESCRIPTION OF THE UNDERTAKING

[Provided by American Structurepoint, Inc.]: The Indiana Department of Transportation (INDOT), with the Federal Highway Administration (FHWA) providing funding for the project and thereby acting as the lead federal agency in this Section 106 undertaking is conducting a Section 106 Study for the Yeager Road Improvement Project. The project area is in Wabash Township and lies within the Urban Area Boundary of the City of West Lafayette and within the city limits on land that is well developed with commercial and residential structures.

Yeager Road from Northwestern Avenue to Sagamore Parkway West is an important arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy event traffic. The success of this project is vital to the City of West Lafayette and the Purdue Community.

The alignment for Yeager Road will not change and presently is a 3-lane arterial with numerous private and commercial driveways. The pavement is in fair condition however the plan is to fully reconstruct the roadway with full depth pavement. With reconstruction, the roadway cross section will be upgraded to a full urban cross section with concrete curb and gutters and improved storm drainage facilities to adequately handle the storm water runoff. Reconstruction will also allow inclusion of adequate facilities for non-motorized travel including sidewalks with trail connections where necessary.

At present the roadway is a 3-lane facility with two 12-foot through lanes and a center two-way left turn lane. The east side of the road is flanked by curb, gutter, and sidewalk and the west by a grass shoulder and remnants of open drainage ditch that is silted full until largely nonfunctioning. The roadway has been resurfaced to the point where even the gutter pan is paved reducing its capacity and the pavement cross slope flattened to the point where adequate cross pavement drainage fails to occur. The lack of an adequate drainage system to convey stormwater runoff during heavy rains allows the roadway to be quickly inundated making travel difficult and unsafe.

The above conditions lend credence to the need to fully reconstruct the pavement and restore the ability of the roadway and surrounding area to be adequately drained during rainfall events. Adequate draining pavements with storm sewers ensure safe travel for motorists and prolong the life expectancy of the roadway. It is clear from our observations that the existing conditions are beyond rehabilitation and replacement is the only prudent option.

The proposed urban typical section will provide a 3-lane divided urban arterial road section with 14-foot-minimum two way left turn lane, a 12-foot through lane in each direction, and 2-foot curb/gutters. An enclosed storm drainage system will provide for the collection and conveyance of stormwater runoff to appropriate release points at both ends of the project. Sidewalks on both sides or possibly a multi-use path on one side is to be provided. Each is to be separated from the pavement by a 5-foot-minimum grassed buffer. This typical section will fit within an 80-foot minimum right-of-way width. The pavement design will also include subsurface underdrains connected to storm sewer system.

Drainage: The project area is generally flat with no natural drainage features available for out-letting stormwater. Stormwater runoff collected by curb inlets will drain to a central trunk line located outside the pavement. We anticipate being able to tie into the existing storm drain line in Sagamore Parkway. This line drains east and outlets eventually to the Wabash River. Presently a portion of the project drains south towards Northwestern Avenue, which may be retained. A goal of this project is to provide adequate roadway drainage as well as meeting the requisite mandate towards better quality control of stormwater runoff (BMP) generated by this project.

In 2009, the project was expanded to include an outlet of the storm drainage to the existing culvert crossing Northwestern Avenue into Purdue University's Kampen Golf Course inlet/legal drain. This may involve extending the path of the drainage outlet and the construction of a detention basin or expansion of a proposed detention basin by a developer on the northeast side of Northwestern Avenue (presently delayed due to economic conditions).

The existing land use within the project corridor is a mix of residential and commercial properties. Nearly half of the APE consists of apartment complexes. The terrain is mostly level, with variations for created drainage access. (See photographs in Appendix D.)

The APE has been drawn (and expanded in February 2009) to encompass properties on all sides of the undertaking and with a viewshed of it. (See map of APE in Appendix C.)

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

In order to identify archaeological resources, a Phase Ia records check and field reconnaissance were conducted. The report (Stillwell, 8/27/08) stated, "All of the project area had been disturbed by residential and/or commercial development...no further archaeological work is warranted." (See report summary in Appendix G.) This report was transmitted to the State Historic Preservation Officer (SHPO) on September 11, 2008.

In order to identify aboveground resources, project historians reviewed the National Register of Historic Places (NR), the Indiana Register of Historic Sites and Structures (SR), State Historic Architectural and Archaeological Research Database (SHAARD), the *Tippecanoe County, Indiana, Interim Report*, and the Indiana Historic Sites and Structures Inventory (IHSSI). Additionally, historic maps and aerial photographic maps were reviewed. Further, the historians conducted research in the Indiana State Library and online, looking for both general historic information on West Lafayette and Tippecanoe County and for specific information on the Yeager Road area.

An historian and a researcher conducted a site survey of the APE and photographed and recorded survey notes about all properties greater than 50 years of age. During the site visit and documentary research, the historians evaluated the area for historic districts, looking for concentrations of architecturally similar dwellings, contiguous collections of buildings and/or properties indicating a connection based on a historic theme (in this case related to suburban development) within the context of local history. Those resources that did not meet at least one of the NR criteria and/or did not retain integrity were recommended to be ineligible for listing in the NR during this evaluation process.

A Historic Property Report (Weintraut & Associates, November 2008) was prepared that recommended one property eligible. (See report summary in Appendix G.)

The Early Coordination Letter and the Historic Property Report were transmitted to the following potential consulting parties: Federal Highway Administration, Indiana Department of Natural Resources (Division of Historic Preservation and Archaeology and Environmental Review Coordinator), Cultural Resources Section (Office of Environmental Sciences) Indiana Department

of Transportation, Indiana Historical Society, Tippecanoe County Historical Association, Tippecanoe County Historian, and Historic Landmarks Foundation of Indiana (Western Regional Office). (See Appendix A. for list of consulting parties.)

In January 2009, the project was expanded. Based on the new project description, the APE was expanded. (See map in Appendix C.) Weintraut & Associates undertook an additional survey on February 9, 2009. Historians surveyed an additional five properties that were at least 50 years of age and conducted directed research. They prepared an Additional Information Report that recommended no additional properties as eligible for listing in the NR. (See report summary in Appendix G.)

Also after the change in project area, Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey for the additional right of way required for the proposed improvements (Stillwell, 3/16/09). This report recommended that “the proposed undertaking will not affect any archaeological properties” and “no further archaeological work is warranted.” (See report summary in Appendix G.)

3. DESCRIBE AFFECTED HISTORIC RESOURCES

There is one property that will be affected by the undertaking: the Yeager Road Masonic Temple.

Masonic Temple (157-333-WA-4), 2212 Yeager Road, was constructed circa.1957. From a concrete block foundation, the imposing rectilinear two-story building ascends with four walls of limestone veneer accented by seven, angled buttresses on both the north and south elevations. All openings are original, with aluminum framed glass block windows featuring awning sash inserts and concrete sills. Built at the height of the post World War II fraternal order membership boom, which peaked in the late 1950s, this lodge may be interpreted as a local example of a large national social movement during the post war era.¹

With its association to a popular post-World War II social movement considered a significant contribution to the broad patterns of history, the Yeager Road Masonic Temple is recommended as eligible for inclusion on the NR under Criterion A. The structure is recommended as eligible under Criterion C for as it retains a high degree of integrity in unaltered original design, and embodies distinctive characteristics of the International Style while simultaneously reflecting traditional vernacular architecture.

4. DESCRIBE THE UNDERTAKING'S EFFECTS ON HISTORIC PROPERTIES

The project involves the reconstruction of an existing important arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy event traffic. The project would include full-depth pavement as well as the upgrade of the roadway to a full urban cross-section that will include new curbs, gutters, storm drains, sidewalks and trail connections. An additional 20 to 35 feet of right-of-way will be required along the route from approximately 15 property owners. (See plans in Appendix E.)

The undertaking will not take right of way from the Masonic Lodge; however, the undertaking will be moved closer to it and it will be clearly visible from the property. There will be no other effect.

¹ Indiana Freemasons, “Freemasonry FAQ,” <http://www.tryfreemasonry.com/info/faq.php> (Accessed June 2, 2008).

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT -- INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

The criteria of no adverse effect apply to the Yeager Road Masonic Temple.

According to CFR 800.5(a)(1), the criteria of adverse effect applies when the “undertaking [will] alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity ...”

Masonic Temple (157-333-WA-4) 2212 Yeager Road

In regards to the Yeager Road Masonic Temple, the criteria of adverse effect do not apply.

There will be no “[p]hysical destruction of or damage to all or part of the property.” CFR 800.5(a)(2)(i) does not apply.

There will be no “[a]lteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access”. CFR 800.5(a)(2)(ii) does not apply.

The building will not be removed from its historic location, so CFR 800.5(a)(2)(iii) does not apply.

Under CFR 800.5(a)(2)(iv), there will be no change “of physical features within the property’s setting that contribute to its historic significance”, and thus this criterion does not apply. The change of road alignment will not adversely affect the setting.

Under CFR 800.5(a)(2)(v), there will be no “[i]ntroduction of visual, atmospheric or audible components that diminish the integrity of the property’s significant historic features”, so therefore this criterion does not apply.

There are no elements of the proposed preferred alternative that would contribute to the “[n]eglect of a property which causes its deterioration”, and thus CFR 800.5(a)(2)(vi) does not apply.

There will be no “[t]ransfer, lease, or sale of [the] property as part of the proposed project, and thus CFR 800.5(a)(2)(vii) does not apply.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

On October 15, 2008 SHPO sent a letter regarding the archaeological field reconnaissance report (Stillwell, 8/27/08) stating, “Based upon the documentation available to the staff of the Indiana SHPO, we have not identified any archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the proposed project area.” SHPO further stated, “If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. (See correspondence in Appendix F.)

On November 7, 2008 SHPO sent a letter stating, “...we concur with Weintraut & Associates’ assessment that the Yeager Road Masonic Temple is eligible for inclusion in the National Register of Historic Places under Criterion A, for its association with a “popular post-World War II fraternal organization movement.” SHPO further stated, “...we believe that there may be effects on the characteristics of the above identified historic property...” and asked for a scaled site plan, showing existing and proposed conditions in regard to the property with an indication of how much right-of-way would be acquired for the undertaking in relation to the proposed historic

property boundary. SHPO also asked for additional photographs, "...depicting the surroundings and viewshed of the Masonic building and key them to a map or site plan." (See correspondence in Appendix F.)

On January 7, 2009 SHPO sent a letter responding to supplementary documentation (sent December 9, 2008 and received December 15, 2008) regarding improvements to Yeager Road stating, "...based on the information provided to our office, we do not believe the characteristics that qualify the identified historic property for inclusion in the National Register of Historic Places will be diminished as a result of this project." (See correspondence in Appendix F.)

On June 10, 2009, SHPO responded the "change in the scope of work and an archeological field reconnaissance report" (Stillwell, 3/27/09) concurred with the finding of the Additional Information Report for aboveground resources. (See correspondence in Appendix F.)

On July 13, 2009 SHPO sent a letter "intended to supersede our letter dated June 10, 2009." The letter also stated, "Based upon the documentation available to the staff of the Indiana SHPO, we have not identified any historic buildings, structures, districts, objects, or currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the probable area of potential effects." (See correspondence in Appendix F.)

A public notice of No Adverse Effect was published in the *West Lafayette, Journal and Courier* on August 19, 2009; the public was given 30 days to review and comment on FHWA's Findings and Determinations of APE, Eligibility and Effects. Public comment ended September 19, 2009.

On August 28, 2009 Historic Landmarks Foundation of Indiana (HLFI) responded to the documentation of "No Adverse Effect" by stating, "We concur that the proposed plan should not have an adverse effect on the qualifying characteristics of the Masonic Temple." (See correspondence in Appendix F.)

On September 2, 2009 SHPO sent a letter responding to INDOT's finding of "no adverse effect" stating, "...we concur with the INDOT's August 7, 2009 finding, on behalf of the FHWA, that there are no historic buildings, structures, districts, objects, or archaeological resources within the area of potential effects that will be adversely affected by the above indicated project." (See correspondence in Appendix F.)

No additional consulting party comments were received.

Appendix A
List of Consulting Parties

Section 106 Mailing List

Salu	Name	Title	Attn:	Agency/Company	Address 1	Address 2	City	State	Zip	Consulting Party Y/N?
Mr. Tally	Mr. Robert F. Tally, Jr., PE	Division Administrator		Federal Highway Administration, INDOT	254 Federal Office Building	575 North Pennsylvania Street	Indianapolis	Indiana	46204	Y
Mr. Carter	Mr. Robert Carter	State Preservation Officer	Attn: Mr. Ron McAhron	Indiana Department of Natural Resources, Division of Historic Preservation and Archaeology	274 Indiana Government Center West	402 West Washington Street	Indianapolis	Indiana	46204	Y
Mr. Koeppel	Mr. Christopher Koeppel	Director		Cultural Resources Section, Office of Environmental Sciences, INDOT	642 Government Center North	100 North Senate Avenue	Indianapolis	Indiana	46204	N
Mr. Herbst	Mr. John Herbst	President		Indiana Historical Society		450 West Ohio Street	Indianapolis	Indiana	46202	N
Mr. Hupier	Mr. Kyle Hupier	Director	Attn: Ms. Christie Stanifer	Indiana Department of Natural Resources, Environmental Review Coordinator	264 Indiana Government Center West	402 West Washington Street	Indianapolis	Indiana	46204	N
Ms. Atwell	Ms. Kathy Atwell	Executive Director		Tippecanoe County Historical Association		1001 South Street	Lafayette	Indiana	47901	N
Mr. Schueler	Mr. Paul Schueler			Tippecanoe County Historian		801 Hitt Street	Lafayette	Indiana	47901	N
Mr. Kleckner	Mr. Tommy Kleckner	Director	Tom Bladuf	Western Regional Office, Historic Landmarks Foundation of Indiana		643 Wabash Avenue	Terre Haute	Indiana	47807	Y

IN20070861

From: [Tom Balduf](#)
To: [Winsett, Brianna;](#)
Subject: RE: Des.
No. 0600696 Yeager Road Reconstruction West Lafayette, Tippecanoe County. Project No. IN20061278
Date: Wednesday, February 18, 2009 9:02:33 AM

Brianna,

Yes, I would be pleased to be included on the consulting parties listing. Tom

Thomas J. Balduf
Community Preservation Specialist
Western Regional Office
Historic Landmarks Foundation of Indiana
444 South 6th Street
Terre Haute, IN 47807
812-232-4534
Fax: 812-234-0156
west@historiclandmarks.org

From: Winsett, Brianna [<mailto:BWinsett@structurepoint.com>]
Sent: Thursday, February 12, 2009 4:06 PM
To: Tom Balduf
Cc: Bosse, Briana
Subject: RE: Des.No. 0600696 Yeager Road Reconstruction West Lafayette, Tippecanoe County.
Project No. IN20061278

Tom,

Thanks for your inquiry. No final decision has been made regarding the Yeager Road Project. The Section 106 process was set back because additional land was added to the area of potential effect. Review of the added parcel is underway and the Section 106 process will be reopened in the next few months.

My records do not indicate that you or your organization wished to become a consulting party for this project, however since you have shown interest in the project would you like to be included as a consulting party? Invitation to become a consulting party was sent on October 28, 2008

Feel free to contact me with any additional questions.

Brianna J. Winsett

Environmental Scientist
bwinsett@structurepoint.com
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256
317.547.5580 | office
317.543.0270 | fax
www.structurepoint.com

From: Tom Balduf [<mailto:west@historiclandmarks.org>]
Sent: Tuesday, February 03, 2009 5:00 PM
To: Winsett, Brianna
Subject: Des.No. 0600696 Yeager Road Reconstruction West Lafayette, Tippecanoe County. Project No.

IN20061278

Attention: Brianna Winsett

I have to apologize for not getting any comments back to you on this project. I know the time for comments has come and gone, but I only recently began the process of reviewing Section 106 projects and will be more quick to respond on upcoming consultative reviews. Right now, I am just cleaning up backlogged paperwork and will be ready for the next "installments". Just out of curiosity, what was the final decision on this particular project?

Yours truly,
Tom

Thomas J. Balduf
Community Preservation Specialist
Western Regional Office
Historic Landmarks Foundation of Indiana
444 South 6th Street
Terre Haute, IN 47807
812-232-4534
Fax: 812-234-0156
west@historiclandmarks.org

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<http://www.emaildisclaimers.com/>

Appendix B
Federal Highway Administration's Section 4(f)
Compliance Requirements (for Historic Properties) and
Section 106 Findings and Determinations, Area of Potential
Effect, and Effect Finding.

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4 (F) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
YEAGER ROAD IMPROVEMENT PROJECT
TIPPECANOE COUNTY, WEST LAFAYETTE, INDIANA
DES NO.: 0600696
FEDERAL PROJECT NO.: PENDING**

**AREA OF POTENTIAL EFFECT
(Pursuant to 36 CFR Section 800.4(a)(1))**

The Area of Potential Effects (APE) has been drawn to encompass properties on both sides of the undertaking. (See APE Map in Appendix C..)

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))**

There are no properties listed in the NR within the APE of this undertaking; one individual aboveground resource is recommended eligible for listing in the NR: the Masonic Temple at 2212 Yeager Road.

Masonic Temple, a rectilinear two-story building with limestone veneer constructed circa 1957, is marked by seven, angled buttresses on both the north and south elevations. All openings are original, with aluminum framed glass block windows featuring awning sash inserts and concrete sills. The main entryway is protected by a three-sided concrete angled portico extending from the wall underneath the words "Masonic Temple" carved into concrete, and a cast concrete relief of the Masonic symbol. A two-level stepped concrete parapet crowns the façade. The Masonic Temple may be understood as a local example of a large national social movement. With its association to a popular post-World-War-II social movement considered a significant contribution to the broad patterns of history, the Masonic Temple is recommended as eligible under Criterion A and under Criterion C for as it retains a high degree of integrity in unaltered original design and embodies distinctive characteristics of the International Style while simultaneously reflecting traditional vernacular architecture.

EFFECT FINDING

(Pursuant to 36 CFR 800.4(d)(1))

Masonic Temple will be affected visually by the change in setting, but this effect will not be adverse.

INDOT acting on FHWA's behalf, has determined that a finding of No Adverse Effect is appropriate for this undertaking.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Masonic Temple - This undertaking will not convert property from Masonic Temple, a section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is No Adverse Effect; therefore no Section 4(f) evaluation is required for Masonic Temple. **INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."**

Consulting parties will be provided a copy of INDOT's, acting on FHWA's behalf, findings and determinations in accordance with INDOT and FHWA's Section 106 procedures. Comments will be accepted for 30-days upon receipt of the findings.



Staffan Peterson
Administrator
INDOT Cultural Resources

August 7, 2009

Approved Date

Appendix C
Maps depicting the Project Area and Area of Potential
Effect.

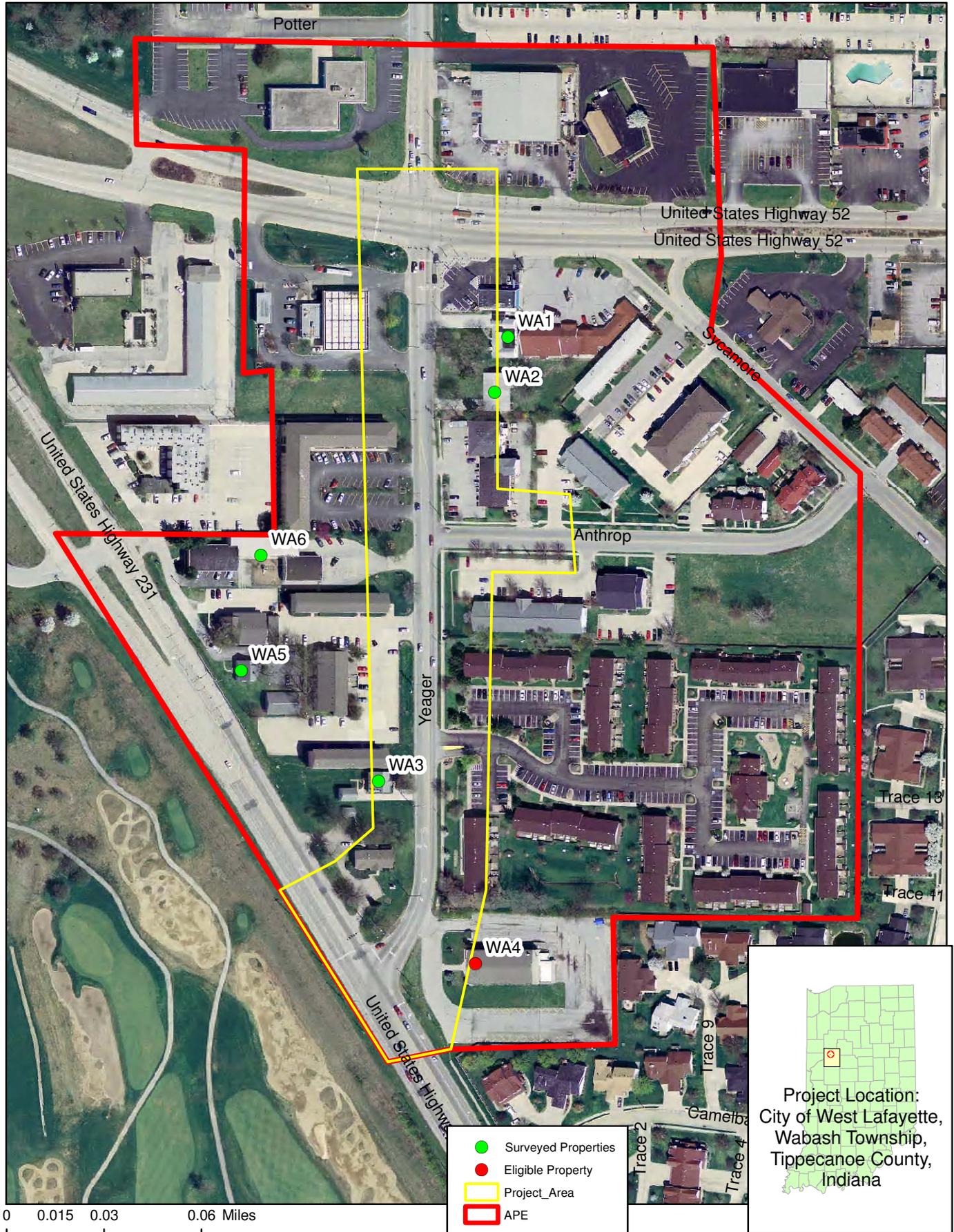
Yeager Road Improvement Project

DES. No

2005 Aerial Photo



June 25, 2009



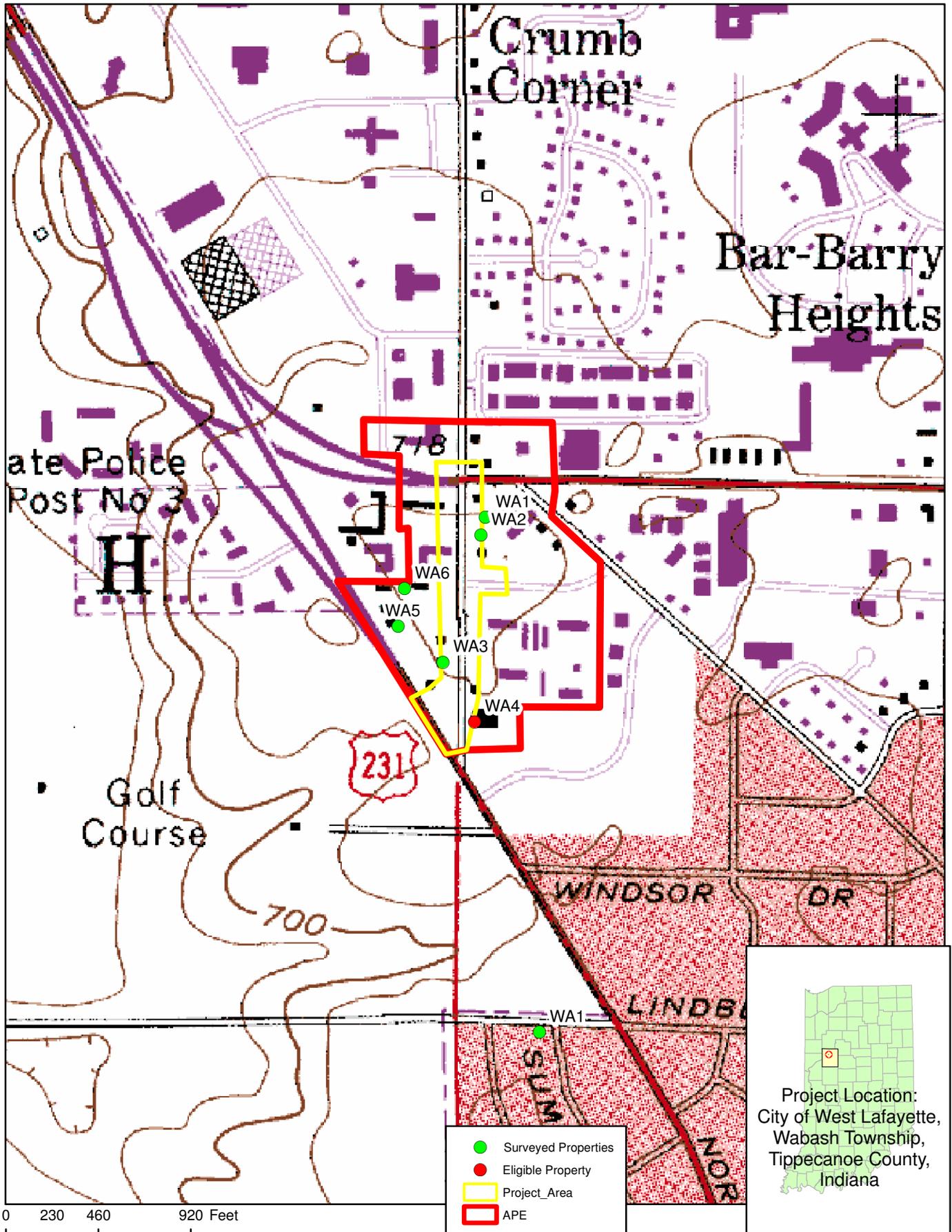
Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Yeager Road Improvement Project

DES. No

Portion of the USGS 7.5' series, Indiana topographic quadrangle

June 25, 2009



Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

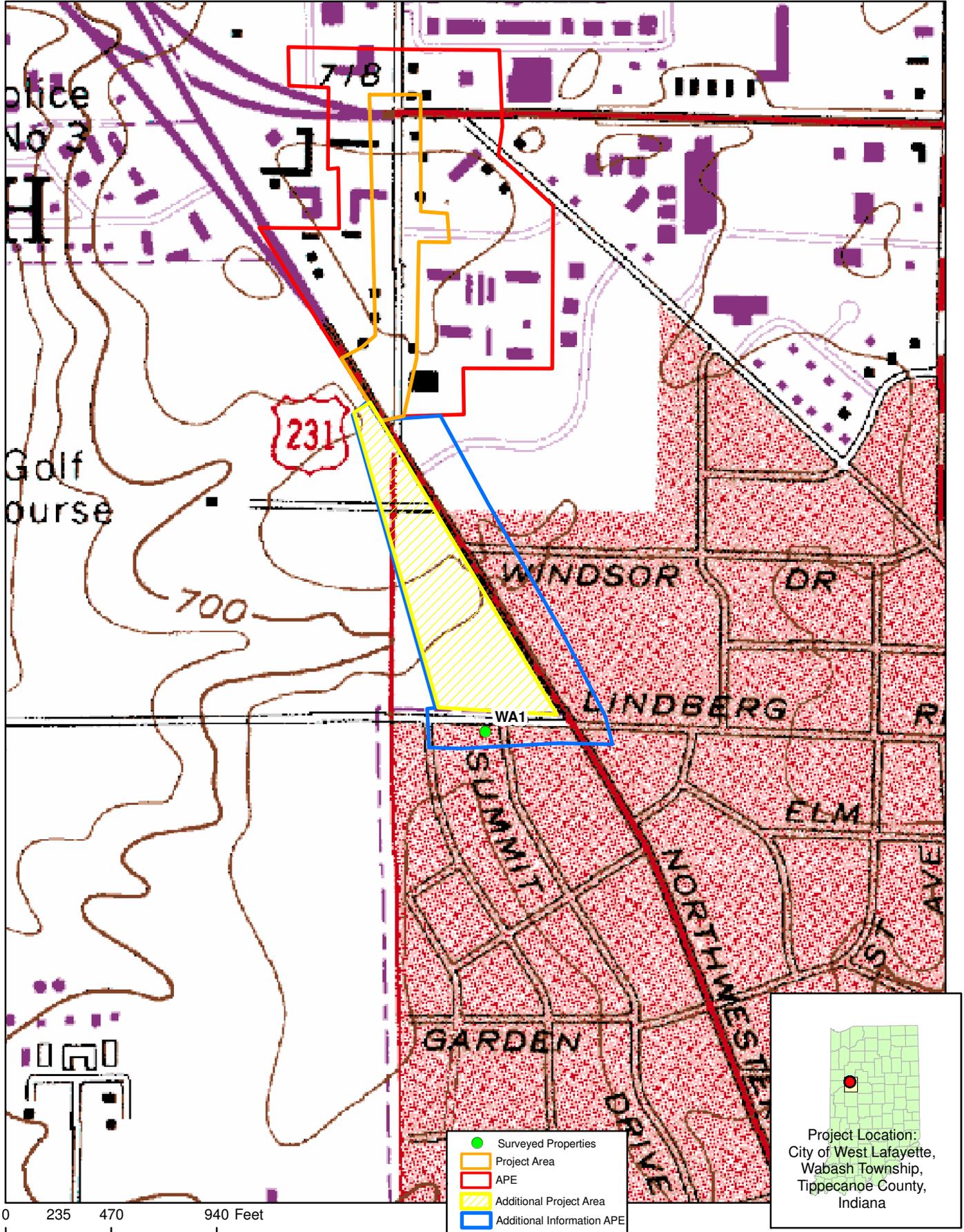
Yeager Road Improvement Project Additional Information 2005 Aerial Photo



Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Yeager Road Improvement Project Additional Information

Portion of USGS 7.5 minute Series Map (1:24,000)



Note: Information shown on this map is not warranted for accuracy or merchantability. GIS data used to create this map are from the best known sources existing at this time. However, experience shows that many national datasets are not all inclusive. Use of this map should be limited to planning, and should not replace field review or background checks with other sources. It is intended to serve as an aid in graphic representation only. This map does not represent a legal document.

Appendix D

Photographs

YEAGER ROAD IMPROVEMENTS

Project No. IN20061278

May 1, 2008



Photo 1: Masonic Temple, east side of Yeager Road



Photo 2: North side of the Masonic Temple



Photo 3: Masonic Temple



Photo 4: South side of the Masonic Temple

YEAGER ROAD IMPROVEMENTS

Project No. IN20061278

May 1, 2008



Photo 5: Masonic Temple, 5957 Yeager Road



Photo 6: Looking south along Northwestern Avenue (US 231)



Photo 7: Looking north along Northwestern Avenue (US 231)

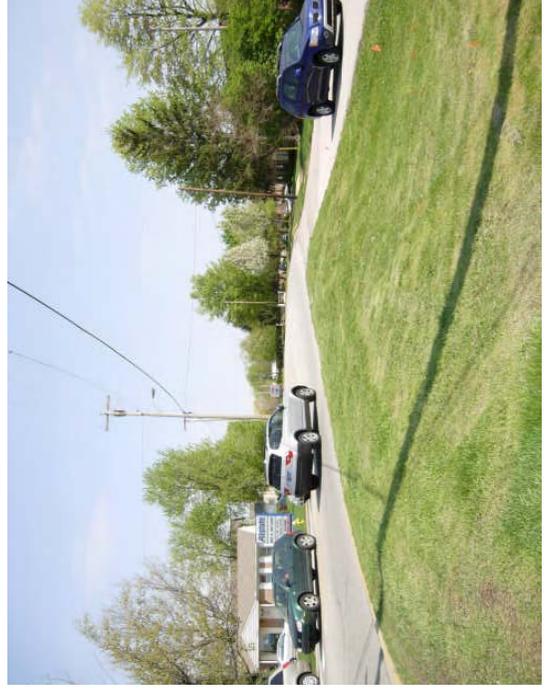


Photo 8: Looking north along Yeager from Northwestern Avenue (US 231)

YEAGER ROAD IMPROVEMENTS

Project No. IN20061278

May 1, 2008



Photo 9: Allstate, south corner of Yeager and Northwestern Avenue (US 231)



Photo 10: Allstate, south corner of Yeager and Northwestern Avenue (US 231)



Photo 11: 2331 Yeager Road



Photo 12: 2331 Yeager Road

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008



Photo 13: 2331 Yeager Road



Photo 14: 2335 Yeager
Highland Courts Apartments



Photo 15: 2335 Yeager
Highland Court Apartments

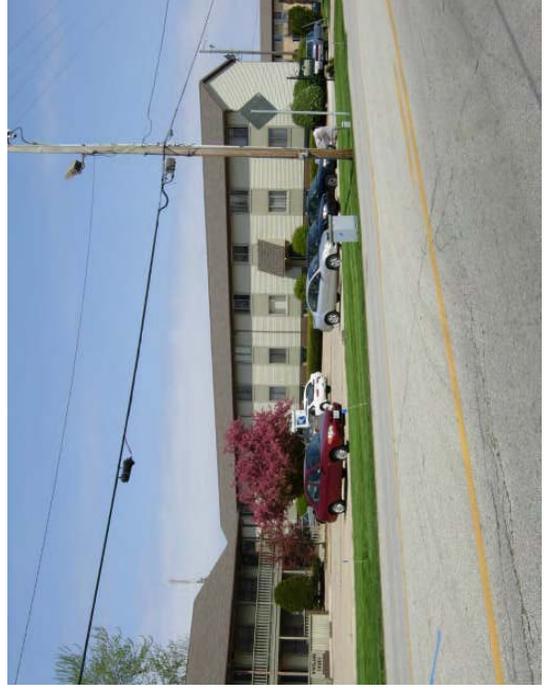


Photo 16: 2335 Yeager
Highland Court Apartments

YEAGER ROAD IMPROVEMENTS

Project No. IN20061278

May 1, 2008



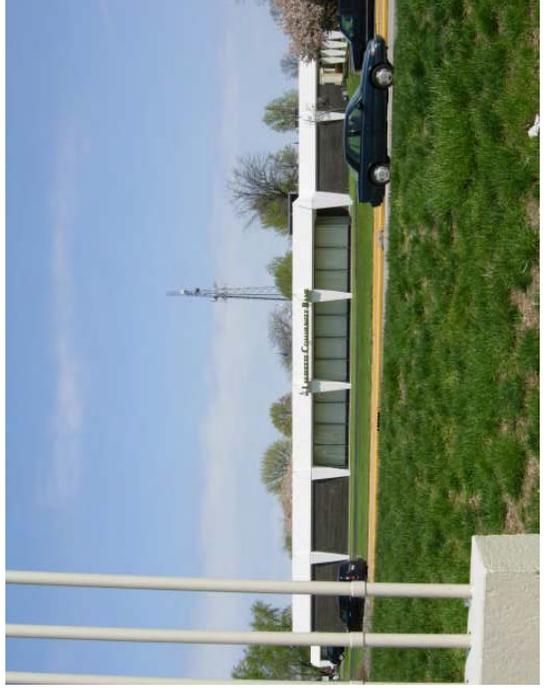
**Photo 17: 2363 Yeager Road
Children's House**



**Photo 18: 2367 Yeager
Pine Manor Apartments**



**Photo 19: BP gas station, southwest
corner of Yeager and US 52**



**Photo 20: Lafayette Community Bank,
northwest corner of Yeager and US 52**

YEAGER ROAD IMPROVEMENTS
Project No. IN20061278
May 1, 2008

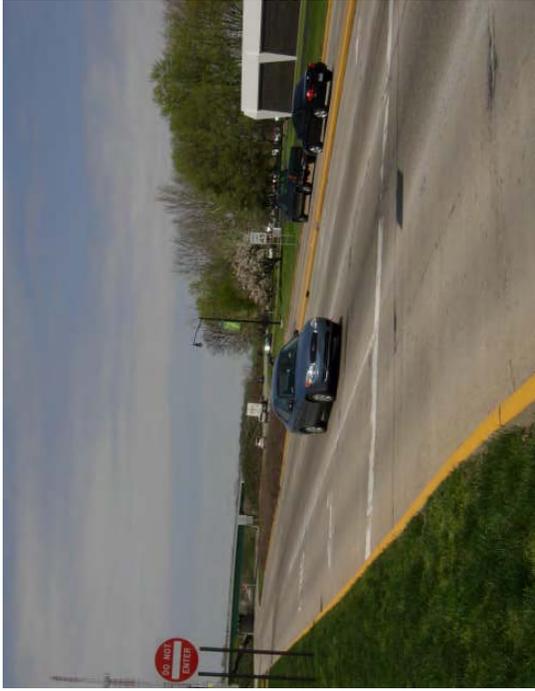


Photo 21: Looking west along US 52



Photo 22: Looking east along US 52

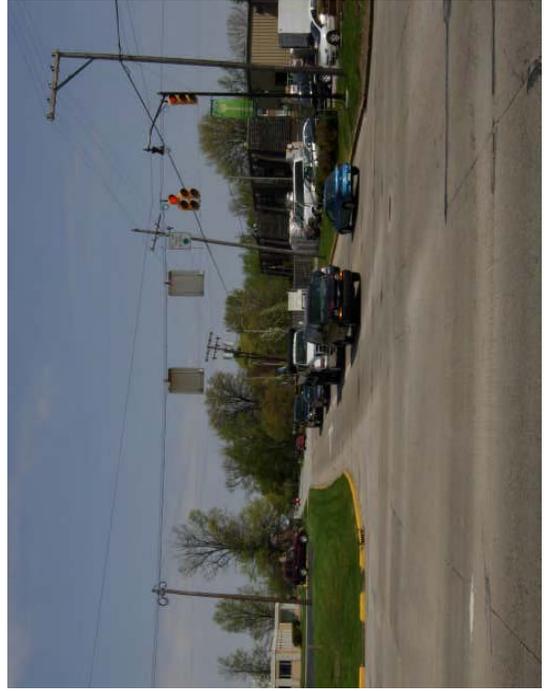


Photo 23: Looking north along Yeager from intersection with US 52



Photo 24: Looking south along Yeager from US 52 intersection



2212 YEAGER ROAD, MASONIC LODGE



2324 NORTHWESTERN AVE.



2331 YEAGER ROAD, SOUTH ELEVATION



2336 NORTHWESTERN AVE. FACADE



2406 YEAGER ROAD



2412 YEAGER ROAD



NW CORNER, YEAGER ROAD & SAGAMORE PARKWAY



SAGAMORE PARKWAY FUNERAL HOME



SE CORNER OF YEAGER ROAD & SAGAMORE PARKWAY WEST



1110 Lindberg Road.JPG



1111 Lindberg Road View to S.JPG



1111 Lindberg Road View to SE.JPG



1201 Lindberg (WA-1) View to the S.JPG



1201 Lindberg Road (WA-1) Close Up View of W elevation.JPG



1201 Lindberg Road (WA-1) Close Up View to S.JPG



1201 Lindberg Road (WA-1) View to S.JPG



1201 Lindberg Road (WA-1) View to SE.jpg



1201 Lindberg Road (WA-1) View to SW.JPG



1201 Lindberg Road (WA-1) View to W.JPG



1217 Lindberg Road View to S.jpg



1217 Lindberg Road View to SE.jpg



1827 Northwestern Ave. View to SE.JPG



1827 Northwestern Ave. View to W.JPG



1849 Sheridan Road View to E.jpg



1849 Sheridan Road View to SE.jpg



1849 Sheridan Road View to SW.jpg



Golf Course View to ENE from WA-1.JPG



Golf Course View to N from E side of 1849 Sheridan Road.jpg



Golf Course View to N from intersection Lindberg & Sheridan.jpg



Golf Course View to N from Lindberg Road.jpg



Golf Course View to N from WA-1.JPG



Golf Course view to W from Northwest Avenue.JPG



Intersection of Lindberg and Northwestern View to E.JPG



NE corner of Lindberg and Northwestern Ave...JPG



NE side of Northwestern Ave. View to E.JPG



NE side of Northwestern Ave. View to NW from NE corner,
Lindberg and Northwestern Ave...JPG



NE side of Northwestern Ave. View to SE.JPG



NE side of Northwestern Avenue near Yeager Road View to N.JPG



NW and NE corners of intersection Lindberg Road and Northwest Ave...JPG



NW and SE corners, intersection Lindberg and Northwestern Ave..JPG



NW corner, intersection Linberg and Northwestern Ave..JPG



SE and SW corners of intersection Lindberg and Northwestern Ave..JPG



SE corner, intersection of Lindberg and Northwestern.JPG



SW and NW corners, intersection of Lindberg and Northwestern Ave..JPG



View to E from Lindberg and Summit intersection.JPG

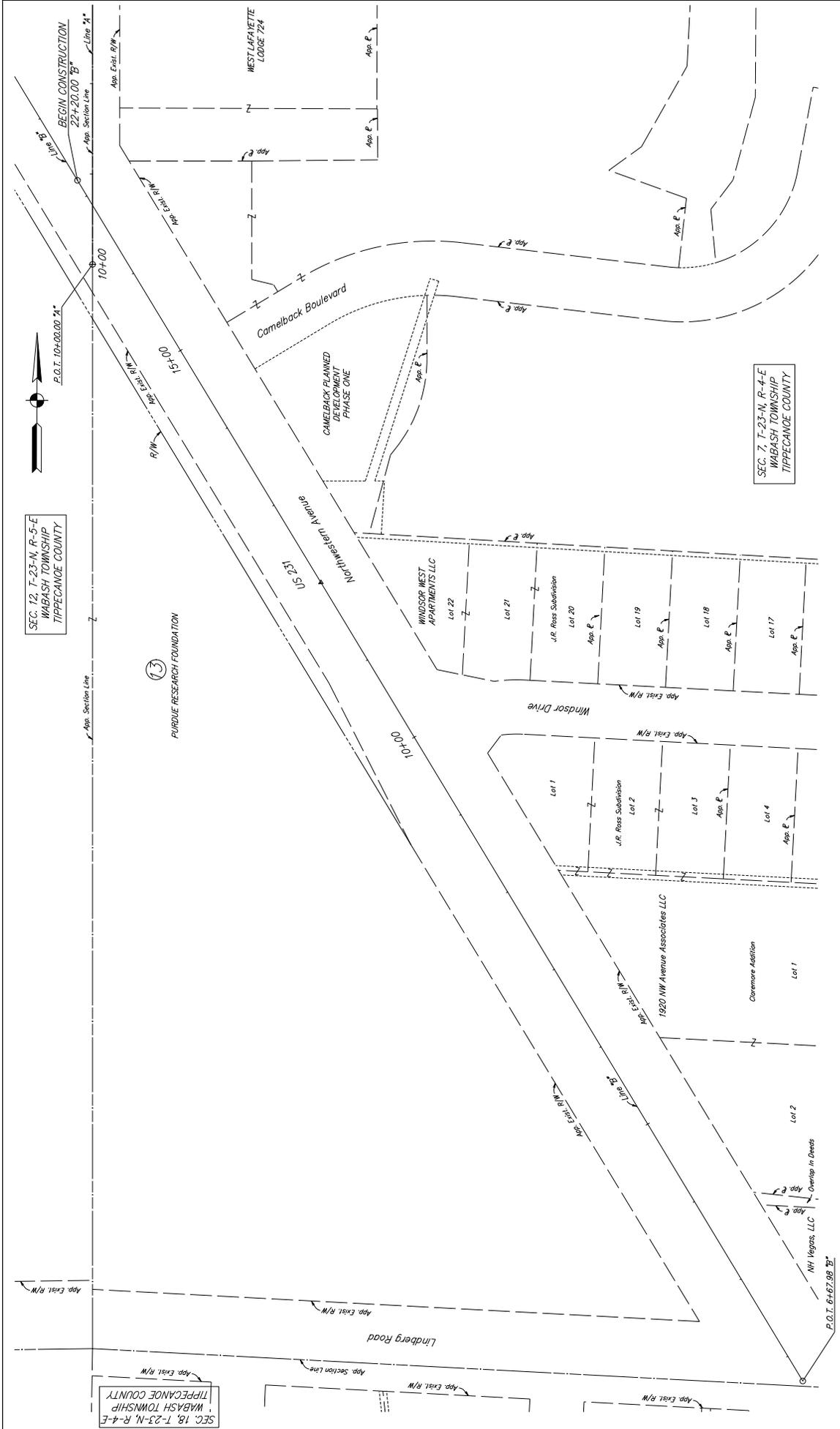


View to NW from NW corner of intersection
Lindberg and Northwestern Ave.JPG



View to S at Lindberg and Summit intersection.jpg

Appendix E Project Plans



SEC. 12, T-23-N, R-5-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY



BEGIN CONSTRUCTION
 22+20.00 75'
 App. Section Line
 Line M

PURDUE RESEARCH FOUNDATION

1200 NW Avenue Associates LLC
 Caremore Addition
 Lot 1

J.R. Ross Subdivision
 Lot 1
 Lot 2
 Lot 3
 Lot 4

WINDSOR WEST APARTMENTS LLC
 Lot 22
 Lot 21
 Lot 20
 Lot 19
 Lot 18
 Lot 17

SEC. 18, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

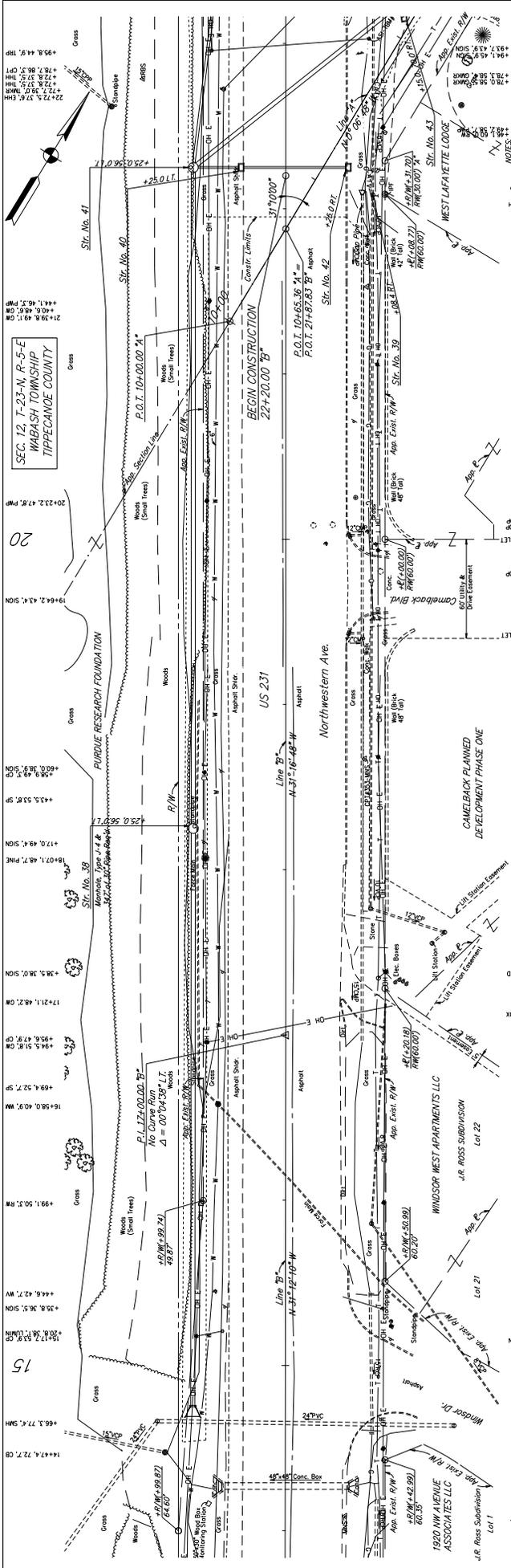
1820 NW Avenue Associates LLC
 Caremore Addition
 Lot 2

J.R. Ross Subdivision
 Lot 1
 Lot 2
 Lot 3
 Lot 4

WINDSOR WEST APARTMENTS LLC
 Lot 22
 Lot 21
 Lot 20
 Lot 19
 Lot 18
 Lot 17

SEC. 7, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE VERTICAL SCALE SURVEY BOOK CONTRACT	BRIDGE FILE DESIGNATION NO. SHEETS PROJECT NO.
RECOMMENDED FOR APPROVAL DESIGN ENGINEER DATE		PLAT NO. 1	
DESIGNED: JMT CHECKED: MPB	DRAWN: PAT CHECKED: MPB		
LEGEND [B] Beg. L.A. R/W [L] End L.A. R/W [X] Force Number			



Line B to Be Constructed
For Proposed Drainage Structures, See
AN R/W ON THIS SHEET TAKEN FROM
LINE B, EXCEPT AS SHOWN.

730	720	710	700	690	680	670	660	650	640	630	620	610	600	590	580	570	560	550	540	530	520	510	500	490	480	470	460	450	440	430	420	410	400	390	380	370	360	350	340	330	320	310	300	290	280	270	260	250	240	230	220	210	200	190	180	170	160	150	140	130	120	110	100	90	80	70	60	50	40	30	20	10	0
-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	-----	----	----	----	----	----	----	----	----	----	---

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: RJC	
CHECKED: MPB	CHECKED: MPB	

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	060606
SURVEY BOOK	SHEETS
CONTRACT	27 of 60
PROJECT NO.	

INDIANA
DEPARTMENT OF TRANSPORTATION
PLAN AND PROFILE
LINE "B"

LEGEND
[Symbol] Right L.A. R/W
[Symbol] End L.A. R/W

15
20
175
180
185
190
195
200
205
210
215
220
225
230

SEC. 12, T-23-N, R-5-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

SEC. 7, T-23-N, R-4-E
WABASH TOWNSHIP
TIPPECANOE COUNTY

1920 NW AVENUE ASSOCIATES LLC
J.R. ROSS SUBDIVISION Lot 1

WINDSOR DR.
J.R. ROSS SUBDIVISION Lot 21

1920 NW AVENUE ASSOCIATES LLC
J.R. ROSS SUBDIVISION Lot 22

CAMELBACK PLANNED DEVELOPMENT PHASE ONE

PURDUE RESEARCH FOUNDATION

US 231
Northwestern Ave.

Camelback Blvd

Appendix F

Correspondence



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



January 7, 2009

Ms. Linda Weintraut, Ph.D.
Weintraut & Associates, Inc.
33 East Cedar Street
Zionsville, Indiana 46077

Federal Agency: Federal Highway Administration ("FHWA")

Re: Response to request for additional project information regarding improvements to Yeager Road
(Designation #0600696; DHPA #4999)

Dear Ms. Weintraut:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated December 9, 2008 and received on December 15, 2008, for the above indicated project in Lafayette, Tippecanoe County, Indiana.

As stated in our letter dated November 7, 2008, we concur with Weintraut & Associates' assessment that the Yeager Road Masonic Temple is eligible for inclusion in the National Register of Historic Places under Criterion A, for its association with a "popular post-World War II fraternal organization movement."

However, based on the information provided to our office, we do not believe the characteristics that qualify the identified historic property for inclusion in the National Register of Historic Places will be diminished as a result of this project.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

Upon completing its own identification and evaluation efforts, it would be appropriate for the Indiana Department of Transportation ("INDOT"), on behalf of the FHWA, to analyze the information that has been gathered from the Indiana SHPO, the general public, and any other consulting parties and make the necessary determinations and findings. Please refer to the following comments for guidance:

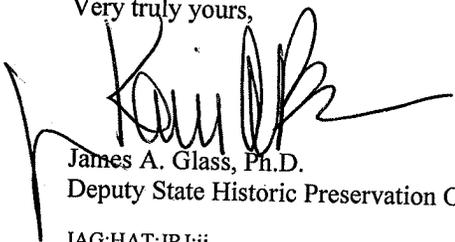
- 1) If the INDOT believes that a finding of "no adverse effect" accurately reflects its assessment, then it shall provide documentation of its finding as set forth in 36 C.F.R. § 800.11(e) to the Indiana SHPO, notify all consulting parties, and make the documentation available for public inspection (36 C.F.R. §§ 800.5[b-c] and 800.2[d][2]).
- 2) If, on the other hand, the INDOT believes that a finding of "adverse effect" accurately reflects its assessment, then it shall forward the information to the 36 C.F.R. § 800.11(e) documentation to the FHWA. The FHWA shall provide notification to the Advisory Council on Historic Preservation by providing the documentation in 36 C.F.R. § 800.11(e) as stated in 36 C.F.R. § 800.6(a)(1). Additionally, the FHWA may proceed to provide documentation of its finding as set forth in 36 C.F.R. § 800.11(e) to the Indiana SHPO, all consulting parties, and make the documentation

available for public inspection and proceed to seek ways to avoid, reduce and mitigate effects as stated in 36 C.F.R. § 800.6 (a)(2-5).

Please be advised that prior to the INDOT / FHWA approving and issuing a finding, the 36 C.F.R. § 800.11 documentation must be submitted to INDOT for review and comment.

A copy of the revised 36 C.F.R. Part 800 that went into effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues please contact Dr. Rick Jones at (317) 233-0953 or rjones@dnr.IN.gov. If you have questions about buildings or structures please contact Holly Tate at (317) 234-3919 or htate@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA #4999.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:HAT:JRJ:jj

cc: Christopher Koeppel, Indiana Department of Transportation



October 15, 2008

Christopher Koepfel
Cultural Resources Section
Office of Environmental Services
Indiana Department of Transportation
100 North Senate Avenue, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration

Re: Archaeological field reconnaissance report (Stillwell, 8/27/08) regarding improvements to Yeager Road
(Designation #0600696; DHPA #4999)

Dear Mr. Koepfel:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated September 11, 2008 and received on September 17, 2008, for the above indicated project in West Lafayette, Tippecanoe County, Indiana.

Based upon the documentation available to the staff of the Indiana SHPO, we have not identified any archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the proposed project area. In regards to the archaeological field reconnaissance report, we note that the current number of archaeological sites recorded in Tippecanoe County is 1142. In addition, substantial research has been conducted in the county and the quadrangle regarding cultural occupations that is not referenced in the report.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

The above comments are strictly in regards to archaeological resources, since no information was provided about buildings and structures. Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

A copy of the revised 36 C.F.R. Part 800 that went into effect on August 5, 2004 may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues please contact Rick Jones, Ph.D. at (317) 233-0953 or rjones@dnr.IN.gov. If you have questions about buildings or structures please contact Karie Brudis at (317) 233-8941 or kbrudis@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA #4999.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:KAB:JRJ:jj

cc Larry N. Stillwell, Archaeological Consultants of Ossian

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



November 7, 2008

Brianna J. Winsett
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256-3957

Federal Agency: Federal Highway Administration ("FHWA")

Re: Historic property report (Weintraut, 7/08) regarding improvements to Yeager Road (Designation #0600696; DHPA #4999)

Dear Ms. Winsett:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated October 28, 2008 and received on October 29, 2008, for the above indicated project in Lafayette, Tippecanoe County, Indiana.

In regard to buildings and structures, we concur with Weintraut & Associates' assessment that the Yeager Road Masonic Temple is eligible for inclusion in the National Register of Historic Places under Criterion A, for its association with a "popular post-World War II fraternal organization movement."

Based on the information provided to our office, we believe that there may be effects on the characteristics of the above identified historic property that qualify it for inclusion in or eligibility for the National Register (*see* 36 C.F.R. § 800.16[i]). To enable us to provide views on the effects 36 C.F.R. § 800.4(d)(2) for your agency, please provide the following information:

- 1) Please provide a site plan, drawn to scale, showing existing and proposed conditions in regard to the Yeager Road Masonic Temple. Please indicate how much right-of-way will be acquired in relation to the boundary as submitted in Appendix 4 of the Historic Property Report (Weintraut, 7/08).
- 2) Please provide additional photographs depicting the surroundings and viewshed of the Masonic building and key them to a map or site plan.

Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

A copy of the revised 36 C.F.R. Part 800 that went into effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions regarding our comments please contact Holly Tate at (317) 234-3919 or htate@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA #4999.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:HAT:hat

cc: Christie Stanifer, Indiana Department of Transportation, Division of Water



Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



June 10, 2009

Brianna W. Chapman
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration

Re: Change in the scope of work and an archaeological field reconnaissance report (Stillwell, 3/27/09)
regarding improvements to Yeager Road (Designation #0600696; DHPA #4999)

Dear Ms. Chapman:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated May 13, 2009 and received on May 14, 2009, for the above indicated project in Lafayette, Tippecanoe County, Indiana.

Based upon the documentation available to the staff of the Indiana SHPO, we have not identified any historic buildings, structures, districts, objects, or currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the probable area of potential effects. In regards to the archaeological field reconnaissance report, we note that the current number of archaeological sites recorded in Tippecanoe County is 1145. In addition, substantial research has been conducted in the county and the quadrangle regarding cultural occupations that is not referenced in the report.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

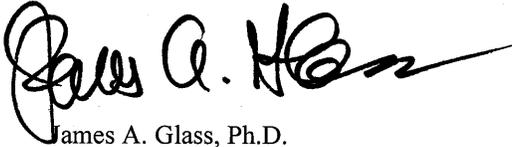
At this time, it would be appropriate for the Indiana Department of Transportation ("INDOT"), on behalf of the FHWA, to analyze the information that has been gathered from the Indiana SHPO, the general public, and any other consulting parties and make the necessary determinations and findings. Please refer to the following comments for guidance:

- 1) If the INDOT believes that a determination of "no historic properties affected" accurately reflects its assessment, then it shall provide documentation of its finding as set forth in 36 C.F.R. § 800.11 to the Indiana SHPO, notify all consulting parties, and make the documentation available for public inspection (36 C.F.R. §§ 800.4[d][1] and 800.2[d][2]).
- 2) If, on the other hand, the INDOT finds that an historic property may be affected, then it shall notify the Indiana SHPO, the public and all consulting parties of its finding and seek views on effects in accordance with 36 C.F.R. §§ 800.4(d)(2) and 800.2(d)(2). Thereafter, the INDOT may proceed to apply the criteria of adverse effect and determine whether the project will result in a "no adverse effect" or an "adverse effect" in accordance with 36 C.F.R. § 800.5.

Please be advised that prior to INDOT approving and issuing a finding, the 36 C.F.R. § 800.11 documentation must be submitted to INDOT for review and comment.

A copy of the revised 36 C.F.R. Part 800 that went into effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues please contact Dr. Rick Jones at (317) 233-0953 or rjones@dnr.IN.gov. If you have questions about buildings or structures please contact Holly Tate at (317) 234-3919 or htate@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA #4999.

Very truly yours,

A handwritten signature in black ink, appearing to read "James A. Glass". The signature is fluid and cursive, with a long horizontal stroke extending to the right.

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:HAT:JRJ:jj

cc: Staffan Peterson, Indiana Department of Transportation
Larry N. Stillwell, Archaeological Consultants of Ossian

DNR

Indiana Department of Natural Resources

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director



July 13, 2009

Brianna W. Chapman
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

Federal Agency: Federal Highway Administration

Re: Change in the scope of work, historic property report (Weintraut 3/09), and an archaeological field reconnaissance report (Stillwell, 3/27/09) regarding improvements to Yeager Road (Designation #0600696; DHPA #4999)

Dear Ms. Chapman:

This letter is intended to supersede our letter dated June 10, 2009.

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated May 13, 2009 and received on May 14, 2009, for the above indicated project in Lafayette, Tippecanoe County, Indiana.

Based upon the documentation available to the staff of the Indiana SHPO, we have not identified any historic buildings, structures, districts, objects, or currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the probable area of potential effects. In regards to the archaeological field reconnaissance report, we note that the current number of archaeological sites recorded in Tippecanoe County is 1145. In addition, substantial research has been conducted in the county and the quadrangle regarding cultural occupations that is not referenced in the report.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

At this time, it would be appropriate for the Indiana Department of Transportation ("INDOT"), on behalf of the FHWA, to analyze the information that has been gathered from the Indiana SHPO, the general public, and any other consulting parties and make the necessary determinations and findings. Please refer to the following comments for guidance:

- 1) If the INDOT believes that a determination of "no historic properties affected" accurately reflects its assessment, then it shall provide documentation of its finding as set forth in 36 C.F.R. § 800.11 to the Indiana SHPO, notify all consulting parties, and make the documentation available for public inspection (36 C.F.R. §§ 800.4[d][1] and 800.2[d][2]).
- 2) If, on the other hand, the INDOT finds that an historic property may be affected, then it shall notify the Indiana SHPO, the public and all consulting parties of its finding and seek views on effects in accordance with 36 C.F.R. §§ 800.4(d)(2) and 800.2(d)(2). Thereafter, the INDOT may proceed to apply the criteria of adverse effect and determine whether the project will result in a "no adverse effect" or an "adverse effect" in accordance with 36 C.F.R. § 800.5.

Brianna W. Chapman
July 13, 2009
Page 2

Please be advised that prior to INDOT approving and issuing a finding, the 36 C.F.R. § 800.11 documentation must be submitted to INDOT for review and comment.

A copy of the revised 36 C.F.R. Part 800 that went into effect on August 5, 2004, may be found on the Internet at www.achp.gov for your reference. If you have questions about archaeological issues please contact Dr. Rick Jones at (317) 233-0953 or rjones@dnr.IN.gov. If you have questions about buildings or structures please contact Holly Tate at (317) 234-3919 or htate@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA #4999.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:HAT:JRJ:jj

cc: Staffan Peterson, Indiana Department of Transportation
Larry N. Stillwell, Archaeological Consultants of Ossian



HISTORIC
LANDMARKS
FOUNDATION OF
INDIANA

Western Regional Office
444 South 6th Street
Terre Haute, IN 47807
812 232 4534
Fax: 812 234 0156

west@historiclandmarks.org
www.historiclandmarks.org

August 28, 2009

Brianna W. Chapman
Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256-3657

Re: Yeager Road Improvements
City of West Lafayette, Tippecanoe County, Indiana
Des. No. 0600696, DHPA No. 4999
Project No. IN20061278

Dear Ms. Chapman,

Thank you for providing me with the information for the proposed Yeager Road reconstruction project in West Lafayette, particularly the portion involving the intersection with Northwestern Avenue. I appreciate your consideration in involving our organization in the review of this project. We certainly agree that the Masonic Temple on Yeager Road appears to have characteristics that would support eligibility for the National Register of Historic Places. Avoidance of any impacts to that historic structure is important and definitely should be a goal included in the reconstruction. We concur that the proposed plan should not have an adverse effect on the qualifying characteristics of the Masonic Temple.

Our concern in the project area is only with the built environment, of course, and does not include possible archaeological sites. I look forward to receiving updates on this project and in particularly any design changes that would have the potential to adversely affect the Masonic Temple. If you have any questions regarding this letter or if changes are made to the current project, please let me know.

Sincerely,

Tom Balduf
Community Preservation Specialist
Western Regional Office



Indiana Department of Natural Resources

Mitchell E. Daniels, Jr., Governor
Robert E. Carter, Jr., Director

Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



September 2, 2009

Staffan Peterson, Administrator
Cultural Resources Section
Office of Environmental Services
Indiana Department of Transportation
100 North Senate Avenue, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration (“FHWA”)

Re: Notification of the Indiana Department of Transportation's finding of “no adverse effect” on behalf of the Federal Highway Administration regarding improvements to Yeager Road (Designation #0600696; DHPA #4999)

Dear Mr. Peterson:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the “Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated August 18, 2009 and received on August 20, 2009, for the above indicated project in Lafayette, Tippecanoe County, Indiana.

As previously indicated, we do not believe the characteristics that qualify the identified historic property for inclusion in the National Register of Historic Places will be diminished as a result of this project. Additionally, based upon the documentation available to the staff of the Indiana SHPO, we have not identified any archaeological resources listed in or eligible for inclusion in the National Register of Historic Places within the proposed project area.

Therefore, we concur with the INDOT’s August 7, 2009 finding, on behalf of the FHWA, that there are no historic buildings, structures, districts, objects, or archaeological resources within the area of potential effects that will be adversely affected by the above indicated project.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Dr. Rick Jones at (317) 233-093 or rjones@dnr.IN.gov. If you have questions about buildings or structures please contact Shawn Niemi at (317) 234-6705 or sniemi@dnr.IN.gov.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:SEN:JRJ:jj

cc: Brianna W. Chapman, American Structurepoint, Inc.

Public Notice

The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana.

Yeager Road is a north-south arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy Purdue event traffic. Presently, Yeager Road is a 3-lane arterial with multiple private and commercial driveways.

The scope of the project includes widening and reconstructing 1,300 feet of Yeager road to a 5-lane arterial road. The proposed improvement will include the addition of two through lanes, 12 feet wide, and one 16-foot left-turn-only lane. New gutters and curb will be installed along both sides of the road, and a new multi-use path will be along the west side of the road. Drainage improvements will also be constructed. A modern roundabout will be constructed at the existing intersection of Northwestern Avenue and Yeager Road. The existing Yeager Road intersection with Northwestern Avenue will require reconstruction to accommodate the added travel lanes and improve the substandard skew and curvature to meet design standards.

One property within the project limits is identified as eligible for listing on the National Register of Historic Places. The Masonic Temple is located on Yeager Road near its intersection with Northwestern Avenue (US 231). The proposed project will have no adverse effect on the Masonic Temple.

The proposed action will not impact items listed or eligible for listing on the National Register of Historic Places. The Indiana Department of Transportation (INDOT) on behalf of the Federal Highway Administration (FHWA) has issued a No Adverse Effect for the project. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.6(a)(4), the documentation specified in the 36 CFR 800.11(e) is available for inspection at American Structurepoint, Inc. This documentation serves as the basis for the Federal Highway Administration's No Adverse Effect Finding. The views of the public on this finding are being sought. Please reply to the contact listed below.

Please reply no later than September 18, 2009. Please direct any questions or comments to:

Brianna W. Chapman
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
Phone: (317) 547-5580
bchapman@structurepoint.com

Journal and Courier
217 N. 6th Street
Lafayette, IN 47901

American Structurepoint Inc

(Governmental Unit)

Ad order #

Tippecanoe County, Indiana

1160119

PUBLISHER'S CLAIM

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152. Columns wide equals 152.00 equivalent
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 in proof in excess of two) \$

TO CLAIM \$ 48.34

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Purpose of publication and penalties of Ch. 155, Acts 1953, I hereby certify that the foregoing account is just and correct, that the amount shown is justly due, after allowing all just credits, and the no part of the same has been paid.
 8/24/09, Clerk

PUBLISHER'S AFFIDAVIT

Personally appeared before me, a notary public in and for said county and state, the undersigned Brandy Stevens who, being duly sworn, says that she is Clerk of the Journal and Courier a daily newspaper of general circulation printed and published in the English language in the city of Lafayette in state and county aforesaid, and that the printed matter attached hereto is a true copy, which was duly published in said paper 1 time(s), the date(s) of publication being as follows :

8/19/2009

Brandy Stevens
 Subscribed and sworn to before me this : **Monday, August 24, 2009**

NOTARY PUBLIC

800.6(a)(4) Pursuant to 36 CFR 800.6(a)(4) the documentation specified in the 36 CFR 800.11(e) is available for inspection at American Structurepoint, Inc. This documentation serves as the basis for the Federal Highway Administration's No Adverse Effect Finding. The views of the public on this finding are being sought. Please reply to the contact listed below. Please reply no later than September 18, 2009. Please direct any questions or comments to: Brianna W. Chapman American Structurepoint, Inc. 7260 Shadeland Station Indianapolis, Indiana 46256 Phone: (317) 547-5580 bchapman@structurepoint.com 8/19/09, No. 1160119

Appendix G Report Summaries

**An Archaeological Field Reconnaissance of Additional Right-of-Way for the
Proposed Yeager Road Improvements (Des. No. 0600696) in Lafayette,
Tippecanoe County, Indiana**

by
Larry N. Stillwell
Principal Investigator



Submitted by:

Archaeological Consultants of Ossian
P.O. Box 2374
Muncie, IN 47307

Submitted to:

American Structurepoint, Inc.
Indianapolis, IN

March 27, 2009

Archaeological Consultants of Ossian *Cultural Resource Management Report 09FR29*

Abstract

On August 15, 2008, personnel from Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey for the proposed Yeager Road Improvements (Des. No. 0600696) in West Lafayette, Tippecanoe County Indiana. The survey was conducted in an urban setting on somewhat poorly drained soils near the Wabash River. Shovel testing failed to locate any cultural materials. All of the project area had been disturbed by residential and/or commercial development. It is the opinion of the archaeologist that the proposed undertaking will not affect any properties eligible for listing on the National Register of Historic Places. Thus, no further archaeological work is warranted. Project clearance is recommended.

Abstract

On March 16, 2009, personnel from Archaeological Consultants of Ossian conducted an archaeological reconnaissance survey of additional right-of-way required for the proposed Yeager Road Improvements (Des. No. 0600696) in West Lafayette, Tippecanoe County Indiana. Additional right-of-way required for the project was estimated at approximately 10.0 acres. The survey was conducted in an urban setting on well drained and somewhat poorly drained soils near the Wabash River. Shovel testing failed to locate any cultural materials. The project area had either been disturbed by commercial development or was situated within wetlands. It is the opinion of the archaeologist that the proposed undertaking will not affect any archaeological properties eligible for listing on the National Register of Historic Places. Thus, no further archaeological work is warranted. Project clearance is recommended.

Historic Property Report
Yeager Road Improvement Project
DES No.: 0600696
Federal Project No.: Pending

**In the City of West Lafayette,
Wabash Township, Tippecanoe County, Indiana**

Prepared for
**Federal Highway Administration/
Indiana Department of Transportation**

Prepared by
WEINTRAUT & ASSOCIATES HISTORIANS, INC.
Principal Investigator: Dr. Linda Weintraut
Authors: Kelly Molloy, Gary F. Quigg
33 E. Cedar Street
Zionsville, Indiana
(317) 733-9770
(Linda@weintrautinc.com)

July 2008

Executive Summary: Yeager Road Improvement Project City of West Lafayette, Tippecanoe County, Indiana

The study area in which the undertaking is generally located includes Tippecanoe County, Indiana. Specifically, the location is in Wabash Township within the City of West Lafayette. The land is urban and developed with commercial and residential structures. Yeager Road is a 3-lane north/south arterial that connects Northwestern Avenue (US 231) with Sagamore Parkway (US 52).

The City of West Lafayette is proposing to reconstruct the pavement and restore the ability of the roadway and surrounding area to be adequately drained during rainfall events. The rehabilitation of Yeager Road will occur from Northwestern Avenue to Sagamore Parkway West and will involve full-depth pavement replacement as well as the upgrade of the roadway to a 5-lane arterial section that will include new curbs, gutters, storm drains, sidewalk and multi-use path.

The Area of Potential Effects (APE) has been drawn to encompass properties on all sides of the undertaking. (See APE Map in Appendix 2). The APE was expanded or contracted based on the possibility of impact of the undertaking on properties.

Project historians who meet or exceed the Secretary of Interior's standards for Section 106 work identified and evaluated historic properties within the APE for this project. Historic properties were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (Revised January 2001), Final Rule on Revision of Current Regulations, December 12, 2000, and incorporating amendments effective August 5, 2004.

Six properties were identified or evaluated for this project. Historians are recommending one above ground resources as eligible for listing in the National Register of Historic Places (NR). There are no properties currently listed on the NR within the APE of this undertaking.

Historic Property Report

Additional Information

Yeager Road Improvement Project Additional Information

DES No.: 0600696

Federal Project No.: Pending

**In the City of West Lafayette,
Wabash Township, Tippecanoe County, Indiana**

Prepared for
**Federal Highway Administration/
Indiana Department of Transportation**

Prepared by
WEINTRAUT & ASSOCIATES, INC.
Principal Investigator: Dr. Linda Weintraut
Author: Gary F. Quigg
P.O. Box 5034
Zionsville, Indiana 46077
(317) 733-9770
(Linda@weintrautinc.com)

March 2009

Executive Summary:

Yeager Road Improvement Project, Additional Information City of West Lafayette, Tippecanoe County, Indiana

This report provides additional information to the Historic Property Report for the Yeager Road Improvement Project (Weintraut & Associates, June 2008).

The Indiana Department of Transportation (INDOT) is proposing to outlet the storm drainage to the existing culvert crossing Northwestern Avenue into Purdue University's Kampen Golf Course inlet/legal drain. This may involve extending the path of the drainage outlet and the construction of a detention basin or expansion of a proposed detention basin by a developer on the northeast side of Northwestern Avenue (presently delayed due to economic conditions).

This report provides additional information to the Yeager Road Improvement Project, which will reconstruct the pavement and restore the ability of Yeager Road and the surrounding area to be adequately drained during rainfall events. The rehabilitation of Yeager Road will occur from Northwestern Avenue to Sagamore Parkway West and will involve full-depth pavement as well as the upgrade of the roadway to a full urban cross-section that will include new curbs, gutters, storm drains, sidewalks and trail connections.

The Area of Potential Effects (APE) has been drawn to encompass properties on all sides of the undertaking. (See APE Map in Appendix 2.)

Project historians who meet or exceed the Secretary of Interior's standards for Section 106 work identified and evaluated historic properties within the APE for this project. Historic properties were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and CFR Part 800 (Revised January 2001), Final Rule on Revision of Current Regulations, December 12, 2000, and incorporating amendments effective August 5, 2004.

Five properties, fifty years old or older, were identified or evaluated for this Additional Information Report. Of these, one property was rated as Contributing. Historians are recommending no additional aboveground resources as eligible for listing in the National Register of Historic Places (NR). There are no properties currently listed on the NR within the APE.

Appendix E: Red Flag and Hazardous Materials

- a. Hazardous Materials Site Visit Form
- b. Red Flag Investigation
- c. Phase 1 Initial Site Assessment

HAZARDOUS MATERIALS SITE VISIT FORM

Des # 0600696

Project #

Road: Yeager Road
improvements to Yeager Road.

Type of Road Project: Intersection improvements and road

Description of area (either general location or exact location of parcel): The project area is located in the East ½ of the southeast ¼ of Section 12, Township 23 North, Range 5 West and the West ½ of the southwest ¼ of Section 7, Township 23 North, Range 4 West.

Person completing this Field Check : Brianna W. Chapman

1. **Has a Red Flag Investigation been completed?** Yes No

Notes:

2. **Right-of-Way Requirements:**

No New ROW **Strip ROW** Minor Take Whole Parcel Take Information Not Available

Notes: The proposed project is on the existing alignment, requiring 1.54 additional acres of permanent right-of-way acquired from 15 parcels of residential and commercial land use.

3. **Land Use History and Development:** (Industrial, Light Industry, Commercial, Agricultural, Residential, Other – also, indicate source of data: visual inspection, aerial photos, U.S.G.S. topo maps, etc.)

Setting (rural or urban): Urban

Current Land Uses: Residential, Commercial, and Recreational

Previous Land Uses: Agricultural

Adjacent Land Uses: Residential, Recreational

Describe any structures on the property: Three 2 ½ story residential buildings, One story commercial building, gas station, two-story stone Masonic Temple, three apartment complexes.

Source of Data: 2005 aerial photography, Visual inspection, Lafayette West USGS quad map

4. Visual Inspection:	Property	Adjoining Property	Property	Adjoining Property
Storage Structures:			Evidence of Contamination:	
Underground Tanks	<u>7</u>	<u>18</u>	Junkyard	_____
Surface Tanks	_____	_____	Auto Graveyard	_____
Transformers	_____	_____	Surface Staining	_____
Sumps	_____	_____	Oil Sheen	_____
Ponds/Lagoons	_____	_____	Odors	_____
Drums	_____	_____	Vegetation Damage	_____
Basins	_____	_____	Dumps	_____
Landfills	_____	_____	Fill Dirt Evidence	_____
Other	_____	_____	Vent pipes or fill pipes	_____
			Other	_____

5. **Is a Phase I, Initial Site Assessment required?**
(Write additional notes on back)

Yes **No**



AMERICAN
STRUCTUREPOINT
INC.

September 24, 2009

Mr. David Buck
Mayor of West Lafayette
609 West Navajo Street
West Lafayette, Indiana 47906-1937

Re: Yeager Road
Yeager Road between US 231 and US 52
West Lafayette, Tippecanoe County, Indiana
DES. No. 0600696
Project No. IN20061278

Dear Mr. Buck:

In accordance with the *INDOT LPA Process Guidance Document for Local Federal-Aid Projects* (January 2009), the Local Public Agency is responsible for preparation of a Red Flag Investigation (RFI). The purpose of the RFI is to provide an overview of environmental conditions and constraints within the proposed project corridor and to define areas for additional studies or environmental consideration. The RFI consists of a review of readily available Geographic Information System (GIS) data layers provided by IndianaMap, the Indiana Geologic Survey, and additional data sources including state and federal databases as appropriate. Records for infrastructure, natural resources and hydrology, hazardous materials, and geology are reviewed within a half-mile radius around the proposed project corridor.

In conjunction with the Early Coordination with local, state, and federal agencies, American Structurepoint, Inc., completed an RFI for the proposed reconstruction of Yeager Road, located in West Lafayette, Tippecanoe County, Indiana. Below is a summary of the findings of the RFI.

The proposed project is located along Yeager Road in West Lafayette, Indiana. More specifically, the project area is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana. The project limits are from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52), a distance of approximately 1,300 feet. The road reconstruction (non-highway) project will include specific improvements as follows. The existing three-lane configuration will be reconstructed adding two additional through lanes, one in each direction. The center two-way left turn lane will be maintained, as there are numerous private driveways along the project corridor. The road cross-section will thus include 4 through lanes 12 feet wide and one two-way left-turn lane, 16 feet wide. A modern roundabout will be constructed at the existing intersection of Northwestern Avenue (US 231) and Yeager Road.

IN20061278



Mr. David Buck
September 23, 2009
Page 2

The existing signalized intersection is skewed causing delay at the intersection and impaired sight distance. The existing Yeager Road intersection with Northwestern Avenue (US 231) will require reconstruction to accommodate the added travel lanes and improve the substandard skew (22.6 degrees) and curvature (245-foot radius) to meet design standards.

The methods of investigation used for the RFI are a database search of state and federal records provided by IndianaMap, Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC), and Environmental FirstSearch. The resources investigated are included for reference.

Below are the topics investigated and their corresponding results.

1) **Infrastructure**

- Cattail Trail is located 1,200 feet south of the project area along Lindberg Road.

2) **Environment**

- Three Industrial Waste Sites are located within the investigated area.
 - 1) Endocyte Inc. is located at 1205 Kent Avenue. This site was registered as a small quantity generator in 2006. A small quantity generator (SQG) generates more than 100 kilograms but less than 1,000 kilograms of hazardous waste per month. This site is currently not in business.
 - 2) Bioanalytical Systems Inc. is located at 2701 Kent Avenue. This site was registered as a small quantity generator in 2003.
 - 3) CTS Corp RF Integrated Modules is located at 1201 Cumberland Avenue. This site is a registered Conditionally Exempt Small Quantity Generator (CESQG). A CESQG generates 100 kilograms or less of hazardous waste per month.
- Three Underground Storage Tank (UST) sites are located within the investigated area.
 - 1) INDOT Division of Research is located at 1205 Montgomery. According to IDEM Virtual File Cabinet, two 1,000-gallon tanks were removed in 1995 by INDOT. INDOT requested closure approval from IDEM in February 1995. IDEM stated the request for closure approval was incomplete. No further information was available in the documents reviewed.
 - 2) CTS Corporation Microelectronics is located at 1201 Cumberland Avenue. According to the IDEM Virtual File Cabinet data sheet, three 20,000-gallon storage tanks are permanently out of service. IDEM VFC had no information for this site.
 - 3) Grace Baptist Church, located at 1395 Neil Armstrong Drive, was identified as UST Site on the IndianaMap GIS database. A UST Notification Form was submitted to IDEM in 1997 that indicated the tank was no longer in use. Review of the available databases did not present any additional information for this site.

Mr. David Buck
September 23, 2009
Page 3

- Six Leaking Underground Storage Tank (LUST) sites are located within the investigated area.
 - 1) Amoco SS 10109 is located at 1209 Sagamore Parkway. According to the IDEM Virtual File Cabinet, there are seven LUSTs at this site. Leaks were first reported in 1989, and a site assessment was performed in 1990. There is one 550-gallon tank, four 12,000-gallon tanks, one 10,000-gallon tank, and one 6,000-gallon tank. Tanks ID numbers 1 through 4 were removed in 1991. Tanks ID numbers 5 through 7 are currently in use. This site is on the medium priority list, and the remediation efforts are active. According to the IDEM Virtual File Cabinet, the latest monitoring report submitted to IDEM was February 2009.
 - 2) U-Haul 76462 is located at 1090 Sagamore Parkway. According to the IDEM Virtual File Cabinet, there are two 550-gallon tanks, one 10,000-gallon tank, and one 250-gallon tank. All tanks were removed in 1999. In June 2000, IDEM issued a No Further Remedial Action.
 - 3) State Farm Mutual Auto Insurance Co. is located at 2550 Northwestern Avenue. According to the IDEM Virtual File Cabinet, there are two 12,000-gallon tanks on this site. Both tanks are permanently out of service. In March 1994, IDEM was requested to close the investigation on this site. No more information is available.
 - 4) Walgreens, formerly Jiffy-Lube, is located at 1000 Sagamore Parkway. According to the IDEM Virtual File Cabinet seven underground storage tanks were removed from the site in 1995. There were two 550-gallon tanks, one 1,000-gallon tank, one 3,000-gallon tank, one 4,000-gallon tank, one 6,000-gallon tank, and one 10,000-gallon tank. Gasoline and waste oil contamination in the soil was noted during tank removal. Remediation actions were initiated in April of 1995. The site is still under remediation monitoring.
 - 5) City of West Lafayette City Building, formerly the City of West Lafayette Police Station, is located at 609 West Navajo Drive. Three tanks were removed from the site in 1991, at which time petroleum hydrocarbons were at elevated levels in the soil. One 300-gallon tank, one 500-gallon tank, and one 1,000-gallon tank were removed. Remediation and monitoring began in 1991, and IDEM issued a No Further Remedial Action for this site in 2004.
 - 6) George Davis Manor is located at 1051 Cumberland Avenue. According to the IDEM Virtual File Cabinet, there is one unregulated 250 gallon diesel tank that was first reported leaking in September 2003. The tank is permanently out of use. No further information was available in the information reviewed.

3) Hydrology

- National Wetland Inventory wetlands are mapped within the investigated area. The mapped wetlands are identified as the following.
 - Palustrine, Emergent, Temporarily Flooded (PEMA)

Mr. David Buck
September 23, 2009
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- Palustrine, Emergent, Seasonally Flooded (PEMC)
- Palustrine, Emergent, Semipermanently Flooded (PEMF)
- Regulated Floodway Maps of the area indicate a regulated floodway Zone A for the Celery Bog, located 1400 feet west of the project corridor
- Mapped hydric soils include Houghton Muck and Mahalasville-Treaty Complex

4) **Geology**

- The project corridor is located within the Iroquois Till Plain Physiographic Region.

In conclusion, no further investigations are needed based on the geology, hydrology, or infrastructure surveys within the project area. However, A Phase 1a Initial Site Assessment is recommended for specific sites within the project area due to the potential for contaminated soils or other hazardous materials. Based on these findings, we believe additional investigation for hazardous materials is warranted for the following sites.

- 1) CTS Corp
- 2) Grace Baptist Church
- 3) State Farm
- 4) Walgreens/Jiffy Lube
- 5) George Davis Manor
- 6) Amoco SS 10109 (BP Gas Station)

You are asked to review this information and provide any comments you may have relative to anticipated impacts of the project. Your timely cooperation in the development of this project will be appreciated. If you have any questions or concerns with the information provided or require additional information, please do not hesitate to contact me at bchapman@structurepoint.com, or by phone at (317) 547-5580.

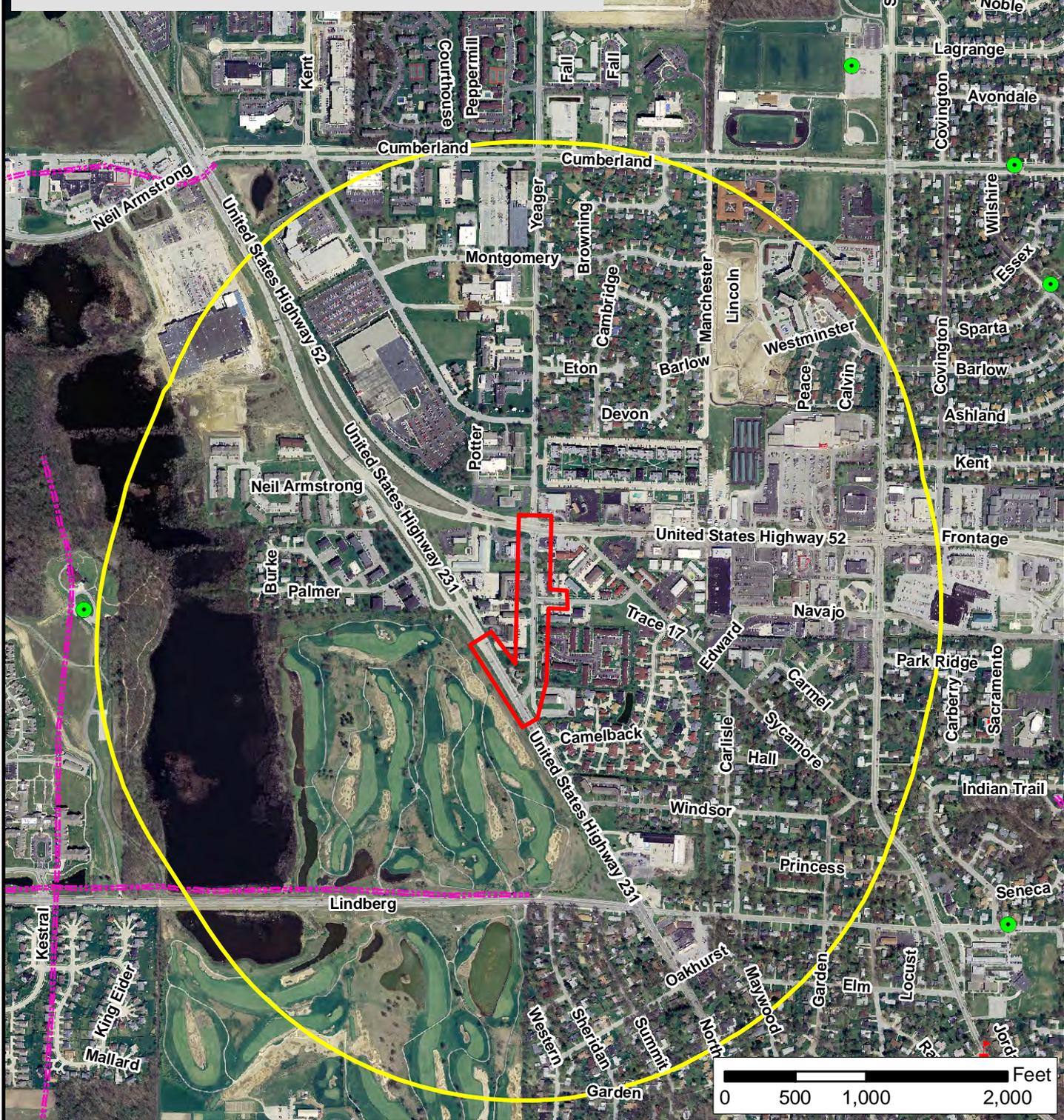
Very truly yours,
American Structurepoint, Inc.

Brianna W. Chapman
Environmental Scientist

BWC:ems

Enclosures

- Project Area
- Investigated Area - 1/2 mile buffer
- ✈ Airports
- + Dams
- + Hospitals
- ★ Industrial Parks
- ⚡ Gas and Electric Utilities
- Recreational Facilities
- Religious Centers
- ▴ Schools
- + Rail System Active & Abandoned
- + Rail System
- ⋯ Trails



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Infrastructure

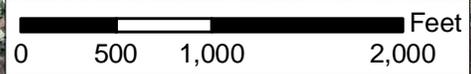
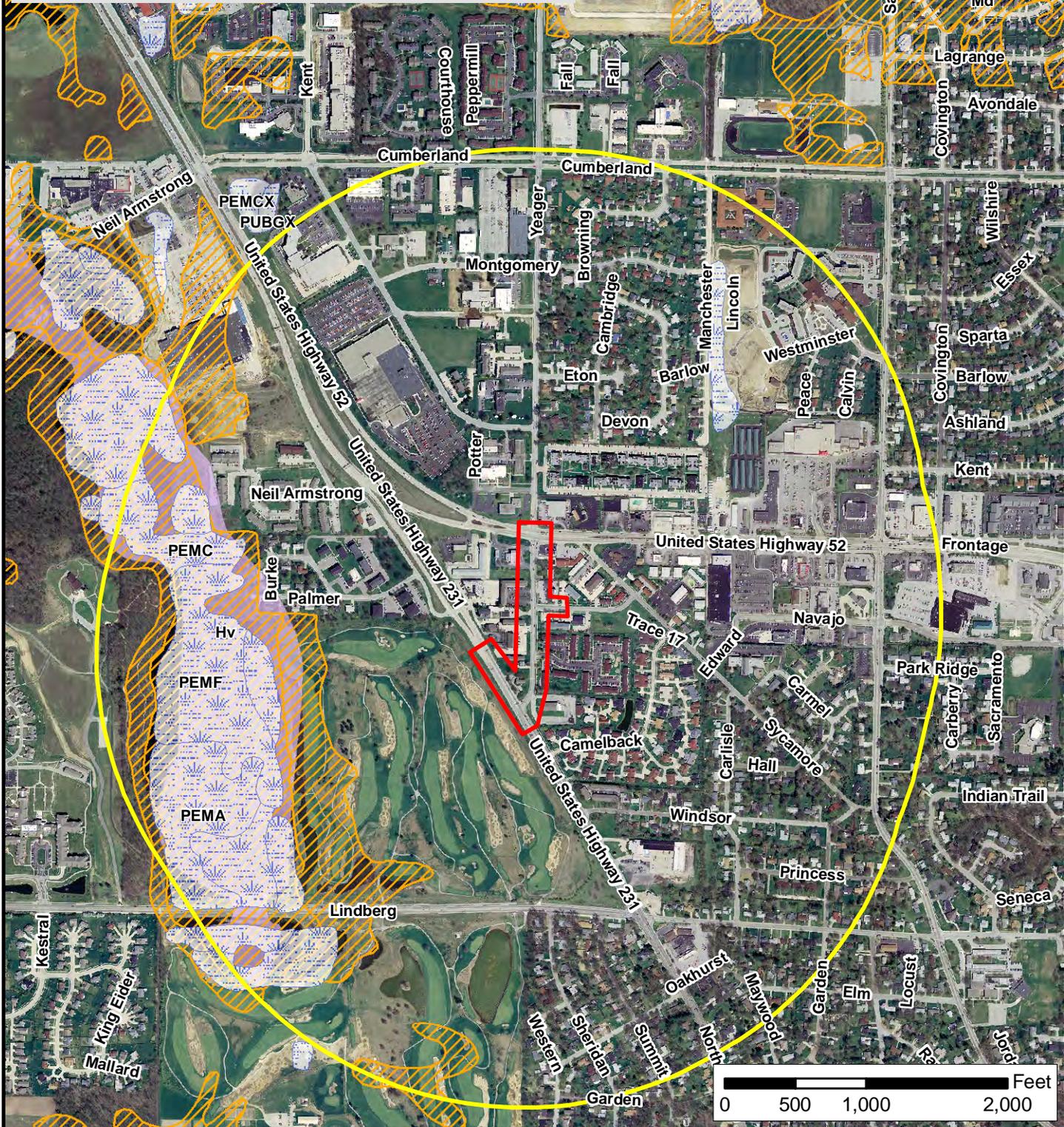
Applicant: City of West Lafayette
 City Hall
 609 West Navajo Street
 West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
 Township: Wabash
 County: Tippecanoe
 State: Indiana

Date: 08/05/2009 Appendix E
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- Project Area
- Investigated Area - 1/2 mile buffer
- Historic Canal Structures
- Historic Canal Routes
- Streams
- Impaired Streams
- Rivers
- Impaired Lakes
- NWI
- Tippecanoe Hydric Soils
- Regulated Floodway



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Hydrology

Applicant: City of West Lafayette
 City Hall
 609 West Navajo Street
 West Lafayette, Indiana 47906

Yeager Road Improvements

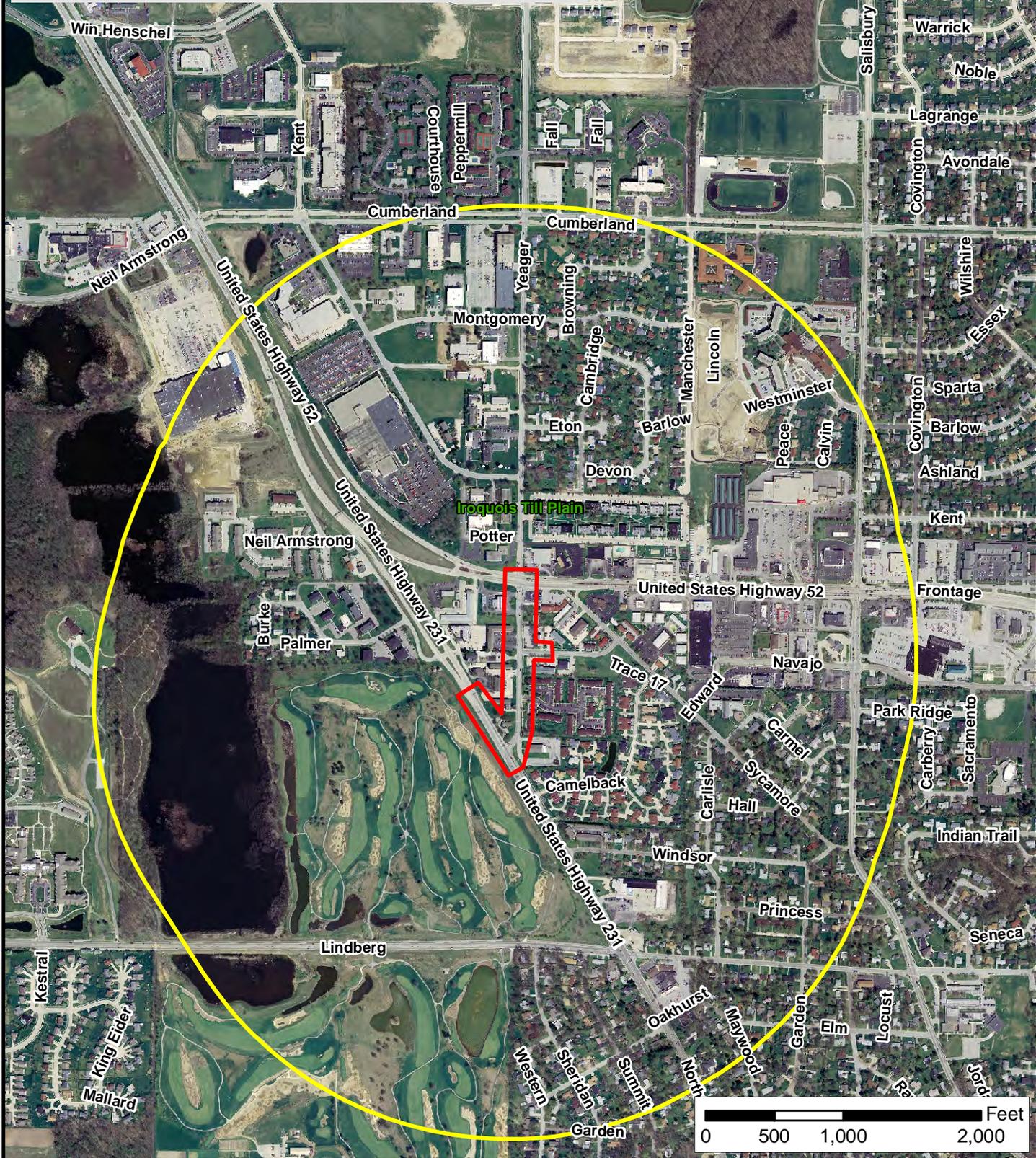
Location: West Lafayette
 Township: Wabash
 County: Tippecanoe
 State: Indiana

Date: 08/05/2009

Appendix E
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- Project Area
- Investigated Area - 1/2 mile buffer
- ▶ Active Industrial Mineral Sites
- Abandoned Quarries
- ▼ Abandoned Sand & Gravel Pits
- Surface Coal Mines
- Underground Coal Mines
- Physiographic Region



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Geology

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Yeager Road Improvements

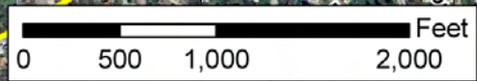
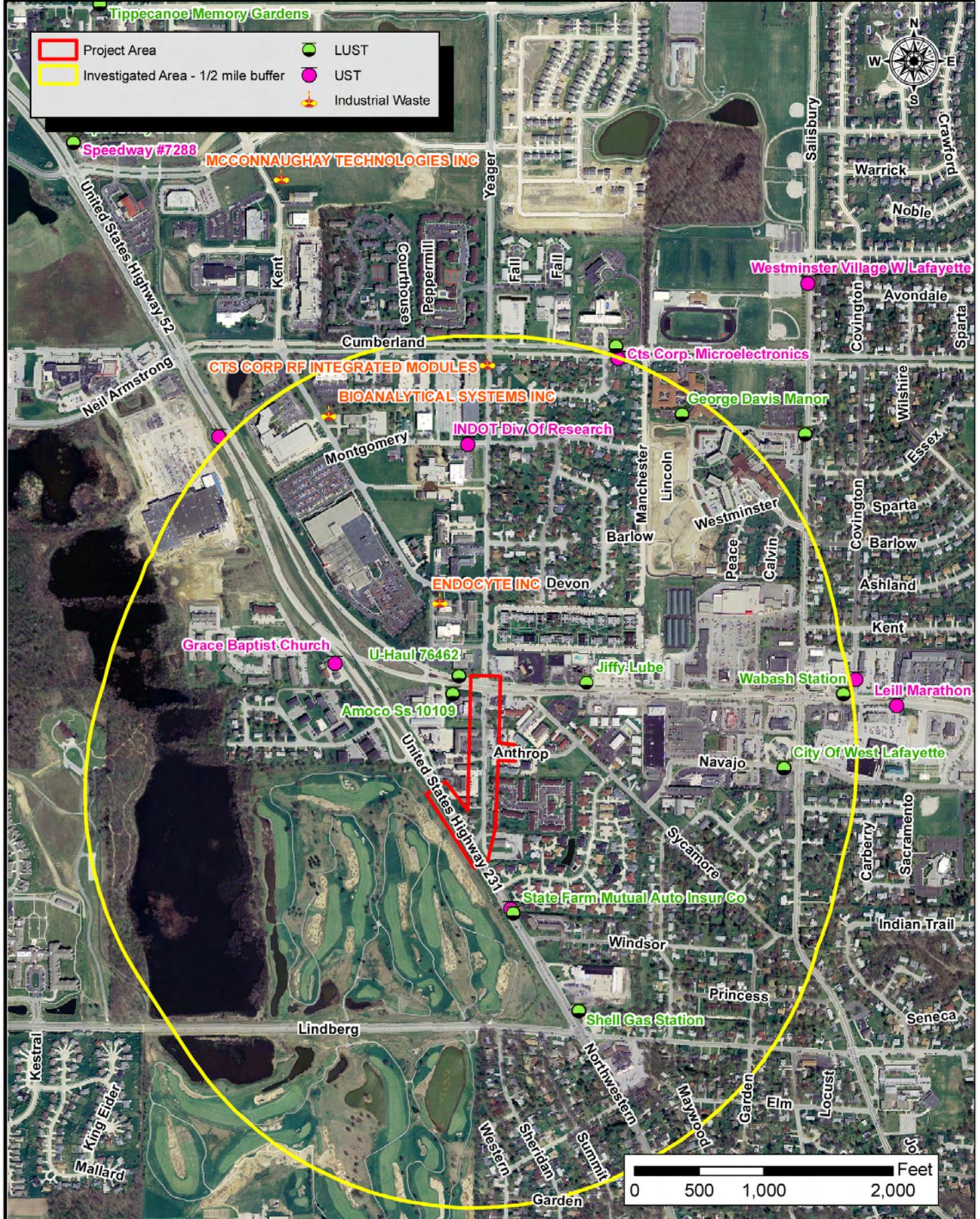
Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 08/05/2009

Appendix E
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Tippecanoe Memory Gardens

	Project Area		LUST
	Investigated Area - 1/2 mile buffer		UST
			Industrial Waste



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Environment

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Appendix E
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Date: 08/05/2009



AMERICAN
STRUCTUREPOINT
INC.

Phase I Initial Site Assessment

Yeager Road Improvement
West Lafayette, Tippecanoe County, Indiana
Des. No. 0600696

Prepared for:

City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Prepared by:

American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
(317) 547-5580

March 12, 2010

IN20061278



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Section 1- SUMMARY

A Phase 1 Initial Site Assessment (ISA) has been prepared by American Structurepoint, Inc., evaluating an area in northwest West Lafayette, Tippecanoe County, Indiana. The project corridor follows Yeager Road from US 52 (Sagamore Parkway) to US 231 (Northwestern Avenue). The project area is composed of a mixture of commercial and residential (including multi-family residential) properties. Maps depicting the project area are located in the appendix of this report.

The City of West Lafayette is developing a federal-aid project to improve Yeager Road from US 52 to US 231 and the intersection of Yeager Road and US 231.

A Phase I ISA was performed for the project area to determine potential sources of hazardous materials that may affect the proposed project within the proposed right-of-way (ROW). Evaluation of the project study area included, but was not limited to, the following sources available: aerial photography, regulatory records, and field reconnaissance.

The results of this investigation revealed one site as a recognized environmental condition (REC), namely, the BP Amoco station located at the southwest corner of Yeager Road and Sagamore Parkway. Further investigation of the soils and groundwater adjacent to this site is recommended prior to the proposed construction of the Yeager Road improvements.

Section 2 – INTRODUCTION

Purpose of Investigation

The purpose of this ISA is to identify RECs associated with the study area, including those related to current and past activities and on adjoining parcels. The need for the ISA is the result of the proposed Yeager Road Improvement Project in West Lafayette, Tippecanoe County, Indiana. The City of West Lafayette is developing a federally funded project to improve Yeager Road from US 52 (Sagamore Parkway) to US 231 (Northwestern Avenue) as well as upgrading the intersection of Yeager Road and US 231. The purpose of the project is to improve traffic flow and motorist safety along Yeager Road and at the intersection.

A Red Flag Investigation (RFI), which is the initial screening of sites for potential recognized environmental conditions, was conducted prior to this study and resulted in the finding of 12 records associated with 12 sites. Results of the RFI are summarized as follows.

- Three industrial waste sites were identified within the 0.5-mile search radius. One of these sites is currently active with a history on on-site contamination;
- Three underground storage tank (UST) sites were identified within the 0.5-mile search radius; however, only two of these sites are located immediately adjacent to the project area, and
- Six leaking underground storage tank (LUST) sites were identified within the 0.5-mile search radius. Two of these sites have been closed and were issued “No Further Remedial Action” designations from the Indiana Department of Environmental Management (IDEM). One

additional site listed in the RFI is located outside of the 0.5-mile search radius of the project area.

Scope of Services

This ISA has been conducted in general conformity with the ASTM Standard E 1527-05, *Standard Practice for Environmental Site Assessments: Phase I Environmental Site Assessment Process* and includes review of state and federal records, databases, site reconnaissance and a review of current and historical aerial photography.

The methods of investigation used for the RFI was limited to a database search of state and federal records sites provided by the Indiana Geological Survey (IGS) through the GIS Atlas for Indiana. Of the 12 sites identified in the RFI, five were recommended for further investigation. Those sites listed in the RFI but not in this ISA were excluded for various reasons including distance from the project area, level of severity, and/or potential for a recognized environmental condition that may impact the proposed project area.

Section 3 – SITE DESCRIPTION

Location and General Characteristics

The project is located in West Lafayette, Tippecanoe County, Indiana. The project will include the reconstruction of Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana.

Yeager Road is a north-south arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy Purdue event traffic. Presently, Yeager Road is a 3-lane arterial with multiple private and commercial driveways.

The alignment for Yeager Road will not change except the intersection with Northwestern Avenue. The pavement is in fair condition; however, the plan is to fully reconstruct the roadway with full-depth pavement. With reconstruction, the roadway cross-section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. For non-motorized travel, sidewalks with trail connections will be constructed. The typical section will fit within an 80-foot minimum ROW width.

Land use within the immediate vicinity of the project is commercial and residential. Multi-family residential buildings are present within the project limits. Strip ROW acquisition is anticipated; however, no business or residential relocations will be necessary.

The study area is located in an extensively developed urban area. The area is developed for residential and commercial uses. The topography of the area is relatively flat. There are no waterways or wetlands within the project area.

Soils identified in the Natural Resources Conservation Service Soil Survey for Tippecanoe County within the project area consist of Urban Starks-Fincastle (UsA). UsA is made up of 45 percent Urban Land, 25 percent Starks, and 15 percent Fincastle. Starks soils are somewhat poorly drained soils located on till plains. The capacity of the most restrictive layer to transmit water is high. Starks soils do not flood or pond. The available water capacity of this soil is high. Fincastle soils are somewhat poorly drained and located on till plains. The capacity of the most restrictive layer to transmit water is low. Fincastle soils do not flood or pond. The available water capacity of this soil is moderate. UsA does not meet the hydric soils criteria.

The Wabash River is the primary drainage channel for Middle Wabash-Little Vermillion watershed and provides the greatest topographical relief for the county. The Wabash River cuts diagonally from the northeast to the southwest across the county; the Wabash River and its tributaries drain the entire county (Tippecanoe County Soil Survey, 1987).

Physiographically, the study area is located within the Tipton Till Plain division of the Central Till Plan Region, as defined by Gray (2000). The Tipton Till Plain is characterized by relatively uniform topography with very little relief. The till plain was created by relatively narrow tongues of ice that surged across a flat, wet slippery substrate, then stagnated and melted in place (Gray, 2000)

Maps depicting the project area are located in Section 10 of this report.

Current Land Use in the Project Area

The project area is made up of commercial and residential land use. Immediately north of the project area is composed of commercial properties with some multi-family residential properties. Immediately east and south east of the project area are single- and multi-family residential properties, and immediately west of the project area is the Purdue University Kampen Golf Course.

Based on the results of the RFI, five sites were recommended for further investigation to determine whether RECs exist that may impact the project area. Existing land use at the five identified sites is provided in the table below and site-specific information is further described in the following sections.

Site No.	Site Name	Current Use
1.	CTS Corp RF Integrated Modules	Commercial/industrial
2.	Grace Baptist Church	Buildings no longer present. Multi-residential housing
3.	State Farm Mutual Auto Insurance Company	State Farm Insurance Company/commercial land use
4.	Jiffy Lube	Walgreens/commercial land use
5.	Amoco SS 10109	BP gas station/commercial land use

Historical Land Use in the Project Area

According to the 1939 aerial photography provided by Indiana Geological Survey (IGS) Indiana Historical Aerial Photo Index (IHAPI), the project area and adjacent properties were entirely in agricultural production with few residential properties. In the 1951 aerial photography, development along the west side of Yeager Road is visible; however, adjacent properties remain in agricultural production. By 1957, additional development along Yeager Road from US 52 to US 231 is visible. Development to the immediate north, east, and south along US 52 and US 231 are in development as

well. Few adjacent properties remained in agricultural production. The Purdue University Kampen Golf course is visible on the 1957 aerial photography. The 1963 aerial photography differs very little from the 1957 aerial photography; however by 1970, the entire project corridor and adjacent properties are developed.

Assumptions

Relevant data sources researched as part of this assessment are assumed to be accurate and current.

Limitations and Exceptions

Services performed by American Structurepoint are limited and ministerial in nature. Summaries and findings of this report describe only those conditions that appeared to be present at the time of our study.

It should be noted environmental evaluations are inherently limited in the sense conclusions are drawn and opinions are rendered from information obtained from limited research and site investigation. For these types of evaluations, it is often necessary to use information prepared by others, and American Structurepoint cannot be responsible for the accuracy of such information. Additionally, the passage of time may result in a change in the environmental characteristics at this site and on surrounding properties. This report does not warrant against conditions present of a type or at a location not investigated. This report is not a regulatory compliance audit and is not intended to satisfy the requirements of any state, federal, or local real estate transfer laws.

Subsurface conditions were not field investigated, as that was outside the scope of this study, and they may differ from the conditions implied by observations of the surface. This study is not intended to quantify soil impacts, waste emplacement, or groundwater impacts. That data is accessible only by subsurface material and groundwater sampling through the completion of soil borings and the installation of monitoring wells. The scope of work did not include these activities. No investigation into mining maps or records have been made.

Other conditions may exist in inaccessible areas or be discussed in documents not reviewed. Therefore, this study does not eliminate the possibility unobserved and/or undocumented environmental conditions that may affect the economic viability of the site could be present. This study does not address the possible presence of environmental contaminants under the ground surface or in areas not accessible at the time of the site visit. Accordingly, American Structurepoint's liability for any matter in connection with this report is limited in accordance with the terms of our agreement for the preparation of this report.

Section 4 – SITE INFORMATION

Results of a regulatory database search (FirstSearch, 2010) indicated a total of ten site files associated with five (5) sites are located within the standard search radius, as outlined in ASTM Standard Practice E 1527-2005. Site files identified at the five sites are summarized in the table below and information for each site, as obtained through site reconnaissance and a review of the IDEM Virtual File Cabinet (VFC) is provided below.

Site No.	Site Name	UST	LUST	NFRAP	SPILLS	ERNS	RCRA	VCP	STATE
1	CTS Corps	X					X		X
2	Grace Baptist Church	X							
3	State Farm		X						
4	Jiffy Lube/Walgreens	X	X						
5	Amoco SS 10109	X	X				X		

- 1) **CTS Corps**
1201 Cumberland Avenue
Distance and Direction from Project: 0.37 mile north
Databases Listed On: RCRA GN, STATE, UST

Records Review

The property located at 1201 Cumberland Avenue is currently owned by Innovatech Development II, LLC. The IDEM VFC records indicate the facility operates as a Small Quantity Generator (SQG) of hazardous waste. The facility contained three permanently out of service 20,000-gallon USTs used to store virgin tetrachloroethene (PCE), recycled PCE, and waste PCE, respectively. The facility also stored chemical used in the manufacturing process in a container storage area (CSA) located near the USTs. In addition, four 1,000-gallon above-ground storage tanks (ASTs) were located in the basement of the facility which were used to store PCE still bottoms. CTS Corporation proposed to cease operation as a container storage area (CSA) in 1985 and only operate as an SQG. CTS submitted a Closure Plan in 1985, which was approved by IDEM in August 1987.

As part of the closure plan, the ASTs were properly decontaminated, removed and disposed of in 1987. Soil sampling, conducted as part of the CSA closure, indicated soils in the former CSA were impacted by various chlorinated compounds (principally PCE, trichloroethylene (TCE), 1,1,1-trichloroethane (1,1,1-TCA), trans-1,2-dichloroethylene (T-1,2-DCE) and methylene chloride) and toluene. Subsequent soil and groundwater investigations were conducted at the site from 1988 through 1993 which identified the site as containing soil and groundwater contamination; however, the extent of contamination was determined to be contained within the site grounds. Primary contaminants of concerns included Acetone, PCE, TCE, 1,1,1-TCA, T-1,2-DCE, 1,1-dichloroethylene (1,1-DCE), 1,1-dichloroethane (1,1-DCA), 1,2-dichloroethane (1,2-DCA), and methylene chloride. The site entered into the Voluntary Remediation Program (VRP) in 1996 with proposed clean closure through risk assessment and remediation; however, IDEM approval of this plan was not issued.

An amended closure plan was submitted to IDEM in 2004 which proposed additional investigation to determine the vertical and lateral extent of contamination. Moreover, this plan proposed closure of the on-site soils in-place and remediation of groundwater via chemical oxidation, air sparging/soil vapor extraction or a combination of these techniques. A copy of portions of this proposed closure plan is included in Section 10.7.

Because the groundwater data upon which the Closure Plan was based was over 12 years old, additional site-wide groundwater sampling was conducted in 2004 and 2005. Results of this sampling indicated the groundwater contamination on-site was still present, including elevated concentrations of vinyl chloride. Additional samples collected south of the site (south side of Montgomery Street) did not

indicate concentrations of contaminants of concern above the RISC Residential default closure levels (DCLs). Soil sampling was also conducted in the former UST and CSA areas. This sampling indicated PCE was present in soils at levels exceeding the RISC Industrial DCLs. The conclusions of this report stated that closure of the site could not be achieved without remediation.

Remedial work was initiated to address contamination from the UST and CSA areas. A Remediation Work Plan (RWP) was prepared and submitted to the IDEM in February 2007. The RWP was approved by the IDEM on April 11, 2007. The remedial strategy consisted of source area remediation by in-situ chemical oxidation using a proprietary oxidant (RegenOx™), and monitored natural attenuation for groundwater. Oxidant was injected at 268 locations on the CTS site in the summer of 2007. Confirmation sampling of the on-site soils indicated concentrations of PCE continued to be above RISC Industrial DCLs. Another attempt at oxidation was performed in May 2008 via a soil mixing technique. Confirmation sampling conducted following the 2008 remedial effort also indicated limited success; however, groundwater sampling indicated reductions of PCE and TCE concentrations in on-site monitoring wells. Documentation of the soil remediation and groundwater monitoring are reported in the “Nature & Extent Investigation Work Plan,” which is included in Section 10.7.

In 2009, the City of West Lafayette conducted a limited subsurface investigation in the ROW along the south side of Cumberland Avenue and both sides of Yeager Road adjacent to the property. The results of that investigation revealed concentrations of PCE above IDEM RISC industrial DCLs in soils sampled from both east and west of Yeager Road. Groundwater samples from both east and west of Yeager Road revealed concentrations of PCE and TCE above IDEM RISC industrial DCLs. A copy of this investigation report is included in Section 10.7.

In 2009, CTS submitted a Nature & Extent Investigation Work Plan that included plans for an off-site nature and extent investigation, on-site groundwater sampling, and evaluation of the performance of an enhanced anaerobic bioremediation pilot test. In addition to the Nature & Extent Investigation Work Plan, vapor intrusion sampling for four residences along Yeager Road across from the facility were requested by IDEM. The results of the Nature & Extent Investigation Work Plan and the vapor intrusion sampling are not yet available. The limits of the proposed investigations include the former CTS property, ROW south of Cumberland Avenue, the ROW east and west of Yeager Road, and the four residences east of the CTS property on Yeager Road.

Historical Use

CTS Corporation has been in operation at this location since 1965. From 1965 to 1985, CTS Corporation operated as a CSA regulated by the Resource Conservation and Recovery Act (RCRA). Prior to 1965, the site appeared to be in agricultural production, based on review of aerial photography from 1939, 1951, and 1963.

Site Reconnaissance

No surface staining or evidence of contamination was noted within the ROW. Two storage barrels marked “Purge Water” were located along the east side of the building. The proposed project will not require acquisition of ROW from this property. The southernmost edge of the former CTS property is approximately 0.37 mile (1,969 feet) north of the northernmost limit of the proposed project.

- 2) **Grace Baptist Church**
1395 Neil Armstrong Drive
Distance and Direction from Project: 0.30 mile west
Databases Listed on: UST

Records Review

The IDEM VFC records indicate that IDEM was notified on October 22, 1990, of one UST present at the site but permanently out of service. The notification indicated the tank contained gasoline, was 30 years old at the time and had a capacity of 500 gallons. According to the notification, the tank was filled with an inert material; however, the exact substance and date the material was placed was not reported.

As indicated in a letter dated, February 22, 2007, IDEM inspected the buildings for asbestos as part of a "Routine Demolition/Renovation Project." The Asbestos Inspection report stated that the renovation was completed prior to the inspection date of January 12, 2007. The inspector "inspected the vacant church and all outlying buildings. No suspect asbestos-containing debris was observed." No violations of 326- IAC 14-10 or 326 IAC 18-1 were observed at the time of the inspection.

No documentation was identified pertaining to UST closure and whether any groundwater or soil sampling was performed during tank closure. This site is not listed as a LUST on the IDEM site database. All information from the IDEM VFC for this site is located in Section 10.7 of this report.

Historical Use

The property operated as Grace Baptist Church since at least 1990. No information prior to 1990 was identified. The 1970 aerial photography indicates the site was developed; however, the exact land use is uncertain. Based on an interview with adjacent property owners, the site was redeveloped into a multi-family apartment complex some time after 2007. The property located at 1395 Neil Armstrong Drive is an apartment building owned by Fairway Apartment Complex. The 2005 Aerial Photography indicated approximately three buildings were located at 1395 Neil Armstrong Drive. Based on 2008 Aerial Photography and field reconnaissance, the buildings associated with Grace Baptist Church are no longer present. An apartment complex and wireless communication tower are located on the property.

Site Reconnaissance

No surface staining or other indications of hazardous materials including the presence of USTs were observed. A large wireless communication tower and small concrete block building are located at the site. A portion of the original site is currently owned by Fairway Apartments and multi-family apartment complex is present on site. The proposed project will not require acquisition of ROW from this property.

- 3) **State Farm Mutual Auto Insurance Co.**
2550 Northwestern Avenue
Distance and Direction from Project: 0.3 mile northwest
Databases Listed on: LUST

Records Review

The property located at 2550 Northwestern Avenue is currently the location of the State Farm Mutual Auto Insurance Company. The IDEM VFC indicates three 12,000-gallon UST were present at the site. One tank contained gasoline and the other two contained diesel fuel. In November 1992, State Farm submitted an Intent to Close notification to IDEM. In January 1993, one tank was removed and the other was abandoned on site. In March 1994, State Farm submitted another Intent to Close to IDEM for the third UST that was not previously abandoned or removed. Soil samples were retrieved as part of the tank closure. At the time of closure, soil samples collected from the tank pit indicated concentrations of Total Petroleum Hydrocarbons (TPH) ranging from 75 to 500 mg/kg, which is above the IDEM LUST closure thresholds. No further documentation was available on the IDEM VFC or the FirstSearch Report to detail the method of remediation or the outcome of monitoring; however, the IDEM Initial Incident Report (dated April 21, 1994) stated the likely method would include over-excavation and disposal of contaminated soils at a landfill. The IDEM LUST database indicates this site was designated low priority for soil contamination. Site closure is identified as “discontinued,” but this site is considered an active investigation. All information from the IDEM VFC for this site is located in Section 10.7 of this report.

Historical Use

State Farm indicated the tanks were six years old at the time they submitted the Intent to Close Notification to IDEM in November 1992. According to the Tippecanoe County GIS, the buildings associated with State Farm Mutual Insurance were built in 1961. Review of the 1957 aerial photography, this site was agricultural crop land prior to development as a commercial property.

Site Reconnaissance

State Farm Insurance still occupies the facility located at 2550 Northwestern Avenue. No staining or hazardous materials concerns were noted on site. The proposed project will not require ROW from this site.

- 4) **Walgreens (formerly Jiffy Lube)**
1000 Sagamore Parkway
Distance and Direction from Project: 0.5 mile east
Databases Listed on: UST, LUST

Records Review

The property located at 1000 Sagamore Parkway is currently a Walgreens drugstore. The IDEM VFC indicates seven storage tanks were removed from the site in April 1995. Four 4,000-gallon tanks, two 1,000-gallon tanks, and one 10,000-gallon tank containing gasoline and two 1,000-gallon tanks

containing waste oil were used at the site. All tanks were permanently out of use at the time of removal and had reportedly been out of service since 1983. Waste oil and petroleum-related contaminants TPH were identified in the soil at the time the tanks were removed; however, no free product or groundwater impacts were observed.

An Initial Site Characterization and Corrective Action Plan were submitted to IDEM on July 17, 1995. The plan stated that over-excavation of the waste oils tank sites would be performed and the soil would be taken to an approved disposal site. For the petroleum-contaminated soil, bioremediation would be performed. Remediation and quarterly monitoring was initiated in 1995.

In 2000, a fresh gasoline odor was observed in non-water-bearing discontinuous sand seam between five and six feet below ground level as part of the quarterly monitoring. Based on information from the closure of an adjacent gas station (located east of Walgreens across Salisbury Street), a hydraulic communication occurs through utility trenches from the east with a plume extending onto the Jiffy Lube property. It was recommended a slurry wall be placed along the southern perimeter of the property and a trench 6 feet deep to sort out the contaminants. Additional monitoring wells were added along the slurry wall and in the INDOT ROW to determine the source and extent of free product, groundwater gradient, and subsurface contamination.

Based on the most recent quarterly groundwater monitoring report (October 2009), injection of an oxygen release compound (ORC) was performed in June 2008 to remediate the residual benzene contamination in monitoring well RW-1. Groundwater sampling conducted in September 2009 indicated benzene concentrations appeared to decrease as a result of the ORC injection; however, concentrations remained above the RISC Residential DCL of 5 ug/L. This report indicated groundwater monitoring activities would be continued. All information from the IDEM VFC for this site is located in Section 10.7 of this report.

Historical Use

The site was developed as a service station/filling station in 1956 according to the UST System Closure Site Assessment (May, 1995). The site was actively farmed in 1951 according the aerial photography for that year. Two gasoline tanks, one waste oil tank, and one heating oil tank were installed in 1956 according to the UST System Closure Site Assessment. At the time of removal in 1995, it was determined the two gasoline tanks were replaced with four 4,000-gallon and one 10,000-gallon gasoline tank at some point between 1956 and 1995. All the tanks have reportedly been out of service since 1983 and were removed in 1995. Walgreens was built in 2001 according to the Tippecanoe County Assessor's information. No information about the land use of the site between 1983 and 2001 was identified.

Site Reconnaissance

Three groundwater monitoring wells were noted on the south side of the property. No surface staining or other hazardous materials concerns were noted on the property. The proposed project will not require ROW from this site.

**5) Amoco Service Station No. 10109 (Currently BP)
1209 Sagamore Parkway
Distance and Direction from Project: 0 mile
Databases Listed on: RCRA GN, UST, LUST**

Records Review

The property located at 1209 Sagamore Parkway is currently a BP gas station. The station, formerly an Amoco station, has been in operation since 1970. Currently, there are three 12,000-gallon USTs for gasoline. These USTs were installed at the site in May 1992 after four existing tanks and all lines, piping, and pump islands were removed. Prior to 1992, there were four tanks on site; one 6,000-gallon tank, one 10,000-gallon tank, one 12,000-gallon tank, and one 550-gallon tank. The three largest tanks were for unleaded gasoline, and the smallest tank for waste oil. All four tanks were removed in 1991 and 1992.

In June 1989, as part of the UST removal process, seven observation wells were installed to monitor groundwater. The reported findings indicated free product, Methyl tertiary butyl ether (MTBE) and benzene, toluene, ethylbenzene, and xylene (BTEX) were present. IDEM requested a Corrective Action Plan (CAP) be prepared for the impact site. The Corrective Action Plan was submitted to IDEM in July 1993. Corrective Action Progress Reports (CAPR) have been submitted to IDEM on a quarterly basis since April 1995. The latest CAPR was submitted to IDEM on December 17, 2009.

The on-site remediation system (installed in April 1995) consists of a Dual Phase Extraction system to recover groundwater and soil vapors. Recovered groundwater is treated above-ground by granular activated carbon and then discharged to an adjacent storm sewer. In addition, groundwater monitoring has been conducted on site since 1993. Presently, groundwater samples are collected from 11 observation wells and six recovery wells with analysis for MTBE and BTEX. Two observation wells are located within the ROW of Sagamore Parkway, north of the property line, and two observation wells are located adjacent to the ROW of Yeager Road, approximately 10 to 15 feet of the eastern property line.

Concentrations of Benzene and MTBE in Observation Well OW-4 (located just east of the pump island canopy, approximately 65-feet west of Yeager Road) consistently exceed the RISC Residential DCLs. However, two monitoring wells (OW-9 and OW-13) are located closer to Yeager Road, and these wells have not indicated elevated concentrations of BTEX or MTBE since at least May 2008. Observation Well OW-8, located in the northeast corner of the property adjacent to Yeager Road and Sagamore Parkway, has also shown elevated concentrations of MTBE since 1998. However, the two observation wells within the ROW of Sagamore Parkway (OW-11 and OW-12) have shown declining concentrations of MTBE since August 2008 and do not indicate elevated BTEX.

Groundwater flows from the south of the property north toward Sagamore Parkway. The December 17, 2009 report indicated BTEX and MTBE are still present in the groundwater samples taken from the interior of the site; however, the levels are less than the RISC Industrial DCLs. Active monitoring for soil and groundwater contamination is on-going at this site. All information from the IDEM VFC for this site is located in Section 10.7 of this report.

The FirstSearch report indicated the Amoco SS 10109 is also a registered Conditionally Exempt Small Quantity Generator of hazardous materials. The materials include benzene and corrosive waste.

Historical Use

Coordination with IDEM regarding this property dates back to July 1990. The site has been in operation as a service station/gas station since 1970. Review of the 1951, 1957, and 1963 Tippecanoe County aerial photography indicates this site has been developed for commercial or industrial use since 1951. No information pertaining to previous land use was identified in the material reviewed for this report. In the 1951 and 1957 aerial photography, the adjacent land use was agricultural. In 1963, adjacent parcels especially to the north and south were being developed. Review of the 1939 aerial photography indicates the site and adjacent properties were farmland.

Site Reconnaissance

De minimis surface staining on paved surfaces was observed on the site. The USTs are located on the north-side of the property along Sagamore Parkway. The proposed project will acquire ROW from this site along Yeager Road (east side of the site). At this time, no ROW will be acquired from the property along Sagamore Parkway. The USTs are approximately 80 feet west of the eastern property line (along Yeager Road) and approximately 20 feet from the northern property line along Sagamore Parkway.

Section 5 – INTERVIEWS

Interview with BP Gas Station owner (previously Amoco SS 10109)

The owner of the BP gas station declined to answer any questions over the phone. A letter was sent to the owner to request additional information. A copy of the questionnaire sent to the owner is included in Section 10.8 of this report.

Interview with Tippecanoe County Health Department

The Health Department records indicated no ongoing issues or complaints for the property located at 1209 Sagamore Parkway, BP Gas Station. A copy of the interview record is included in Section 10.8 of this report.

Section 6 – FINDINGS

The BP Gas Station (formerly Amoco SS 10109) is a LUST site undergoing active remediation efforts due to contaminated soil and groundwater. Three active USTs are located approximately 80 feet from the existing ROW of Yeager Road. Monitoring wells are located within 10 to 15 feet of the eastern property line along Yeager Road, and recent groundwater monitoring has not indicated elevated concentrations of BETX or MTBE in these wells. However, no groundwater or soil sampling has been conducted beyond the east property boundary or within the ROW of Yeager road to date. Therefore, it is undetermined whether the contaminant plume extends into the existing right-of-way of Yeager Road. Given the history of the property and known soil and groundwater impacts, this site is considered to be

a recognized environmental condition.

The CTS Corporation located at 1201 Cumberland Avenue is noted with a REC. The latest monitoring reports submitted to IDEM as part of the active remediation efforts on the site indicated contamination is still present in the soil and groundwater. From the results of a Limited Subsurface Investigation Report, PCE and TCE were found above RISC Industrial DCLs east and west of Yeager Road adjacent to the former CTS property. However, soil and groundwater sampling conducted south of the CTS property (south of Montgomery Street) indicates no migration of groundwater contamination south of the property. The proposed Yeager Road improvements are located approximately 2,000 feet (0.37 mile) directly south of the CTS property.

Grace Baptist Church, located at 1395 Neil Armstrong Drive, has one UST listed as permanently out of service. No records were available documenting the tank removal/ closure or any soil/groundwater sampling. None of the buildings associated with the church are currently present. An apartment complex and a wireless communication tower now occupy the site, and no indications of the former UST were observed. The site is approximately 0.3 mile west of the proposed project area.

State Farm Insurance Agency, located at 2550 Northwestern Avenue, is listed as a UST site with two USTs permanently out of service. One tank was removed, and the other was permanently closed in place. No further information about the site was available. The site is approximately 0.3 mile south east of the proposed project area.

Walgreens (formerly Jiffy Lube), located at 1000 Sagamore Parkway, is listed as a UST and LUST site. Seven USTs were removed from the site in April 1995. Contaminants such as MTBE and BTEX were noted in soil and groundwater at the time the tanks were removed. The contamination plume is approximately 0.5 mile from the proposed project site and appears to be migrating to the south, southeast away from the proposed project area.

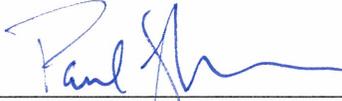
Section 7 – OPINION AND CONCLUSIONS

Based on this review of the environmental database search, IDEM site files, and field investigations, the BP Gas Station site, located at the south-west corner of Yeager Road and Sagamore Parkway, is considered an REC with respect to the Yeager Road project area.

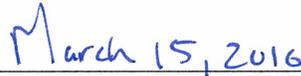
Based on groundwater monitoring conducted at the site since 1993, concentrations of Benzene and MTBE have been persistent within the interior of the site, primarily at monitoring well OW-4 which is located approximately 65 feet west of Yeager Road. It is recommended further investigation of the proposed ROW area on the west side of Yeager Road, south of Sagamore Parkway be conducted prior to commencement of construction of the improvements. Soil and groundwater sampling should be performed to determine whether contaminants from the BP Gas Station have migrated into the proposed project area.

Section 9 – SIGNATURE OF THE ENVIRONMENTAL PROFESSIONAL

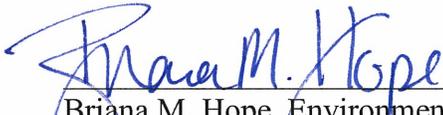
I declare that, to the best of my professional knowledge and belief, I meet the definition of Environmental Professional as defined in §312.10 of 40 CFR 312, and I have the specific qualifications based on education, training, and experience to assess a property of the nature, history, and setting of the subject property.



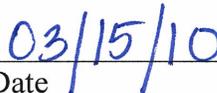
Paul A. Johnson, LPG
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
(317) 547-5580



Date



Briana M. Hope, Environmental Scientist
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256
(317) 547-5580



Date

QUALIFICATIONS OF THE ENVIRONMENTAL PROFESSIONAL

Mr. Johnson has Bachelor of Science and Master of Arts degrees in Geology from Indiana State University. He has performed environmental site analysis since 1991, including site evaluation and remedial design. He has prepared and supervised Phase I ESAs since 1998. He is a Licensed Professional Geologist in the State of Indiana.

Ms. Hope has a Bachelor of Science degree in Natural Resources and Environmental Science from Purdue University School of Agriculture. She has performed environmental site analysis and conducted Phase I ESAs since 2002.

Section 8 – REFERENCES

Electronic Field Office Technical Guide provided by the United States Department of Agriculture - Natural Resources Conservation Service. <http://www.nrcs.usda.gov/Technical/efotg/>

First Search Technology Corporation, Environmental FirstSearch Report. July 16, 2008 (updated on March 1, 2010).

Gray, H.H, 2000, Physiographic Divisions of Indiana, Indiana Geological Survey Special Report 61, Indiana University. Bloomington, Indiana, 15p.

IDEM Virtual File Cabinet, Indiana Department of Environmental Management, www.state.in.us/idem/resources/records/index.html

Indiana Geological Survey, GIS Atlas for Indiana. <http://igs.indiana.edu/>

SSURGO Digital Soils Data provided by the United States Department of Agriculture - Natural Resources Conservation Service. <http://soildatamart.nrcs.usda.gov/>

United States Geological Survey 7.5 Minute Topographic Quadrangle Map. Valparaiso Quadrangle. 1992.

Indiana Historical Map Index, [http:// http://129.79.145.7/arcims/IHAPI/index.html](http://129.79.145.7/arcims/IHAPI/index.html)

Appendix F: Water Resources

- a. Biological Assessment Form
- b. National Wetland Inventory Mapping
- c. FEMA Floodplain Mapping

ECOLOGICAL SITE EVALUATION FORM

Road: Yeager Road Des. No: 0600696 Project No: _____ County: Tippecanoe
 Project Description: Travel lanes, drainage improvements, curbs and gutters, sidewalk and multi-use path will be added to accommodate event and residential traffic.
 Project Location: Approximately 1,300 feet of Yeager Road from the Northwestern intersection to the intersection with US 52, West Lafayette, Tippecanoe County, Indiana
 Natural Region and Section: Loamy High Lime Till Plains of the Eastern Cornbelt Plains
 8-Digit Watershed: 05120108 USGS Quadrangle: Lafayette West Soil Survey Map Sheet Sheets 4 and 5.

RIGHT-OF-WAY BY LAND USE TYPE

Permanent Right-of-way		
Land Use Type	R/W (ha)	R/W (ac)
Commercial		1.21
Industrial		
Residential		0.15
Agricultural		
Wooded		
Other		0.18
Total Perm R/W		1.54

Temporary Right-of-way		
Land Use Type	R/W (ha)	R/W (ac)
Commercial		0.53
Industrial		
Residential		0.19
Agricultural		
Wooded		
Other		0.01
Total Temp R/W		0.72

Is the project located in an urban or a rural setting? Urban
 Is land use in the project changing? **Yes** No If yes, explain: The land use is changing from residential, commercial to transportation land use.

QUADRANT DESCRIPTION

Northeast Residential/ Commercial land use
 Northwest Residential/ Commercial land use
 Southeast Residential/ Commercial land use
 Southwest Commercial/ Recreational land use

STREAM INFORMATION- No streams are present within the project area.

Channel Width: _____ Channel Depth: _____ Maximum Water Depth in Channel: _____
 Substrate Material: (circle one) silt sand gravel loose rock bedrock
 Flow Velocity: (circle one) stagnant slow moderate swift rapid
 Does the stream contain riffle/pool complexes? Yes No
 Does the stream contain meanders within the proposed right-of-way? Yes No
 Is channel work proposed as part of this project? Yes No If yes, describe: _____

Is aquatic flora present? Yes No If yes, please list: _____

Is aquatic fauna present? Yes No If yes, please list: _____

Comments: _____

TERRAIN

Immediate Area: Depressed **Flat** Gently Rolling Rolling Hilly
 Extended Area: Depressed **Flat** Gently Rolling Rolling Hilly

TERRESTRIAL WILDLIFE

ENDANGERED AND THREATENED SPECIES

Is this project located within the range of any Federally Endangered or Threatened Species? Yes No

If yes, please list below.

Common Name	Scientific Name	Status	Suitable Habitat Present
			Yes No

Will any of the above listed species be impacted by the planned improvements? Yes No

NATURAL AREAS

Are there any natural areas located within 5 miles of the project area? Yes No

If yes, please list below.

Property Name	Ownership	Proximity to Project
Celery Bog	West Lafayette Parks & Recreation Department	0.30 miles west of the project area.

Will any of the above listed properties be impacted by the planned improvements? Yes No

WETLAND INFORMATION

Are wetlands mapped within or adjacent to project limits? Yes No

If yes, please list below.

Wetland Type	Abbreviation	Location within Project	Confirmed in Field?
			Yes No Undetermined

Were any of the following wetland indicators observed in or adjacent to project limits?

	Yes	No	Location within Project
Standing Water	___	<u>X</u>	_____
Saturated Soil	___	<u>X</u>	_____
Depressional Areas	___	<u>X</u>	_____
Water Marks on Trees	___	<u>X</u>	_____
Drift Lines	___	<u>X</u>	_____
Fluted Tree Trunks/Roots	___	<u>X</u>	_____
Sediment Deposits	___	<u>X</u>	_____
Water Stained Leaves	___	<u>X</u>	_____
Other _____	___	<u>X</u>	_____

Is there a potential for impacts to jurisdictional wetlands as a result of the planned improvements? Yes No

Comments: _____

GENERAL PROJECT COMMENTS

The project area runs along Yeager Road from the intersection of Yeager Road and Northwestern to the intersection with US 52. This project includes improvements to 1,300 feet of a three lane arterial roadway which includes improved drainage features, new curbs and gutters, a sidewalk on the east roadside, and a multi-use path on the west roadside. A total of 1.54 acres of permanent right-of-way will be acquired from 15 parcels. This right-of-way will come from residential, commercial, and recreational land use. No wetlands or habitats for endangered/threatened species were found within the project area. Some small trees and bushes are within the project area; however, they are part of landscaping for the apartment complexes along Yeager Road. One natural area is located within 5 miles of the project area, The Celery Bog. The Bog, operated by the West Lafayette Parks Department is a “significant ecological site” according to the Indiana Natural Heritage Program. The Celery Bog covers approximately 84 acres. The intersection of Yeager Road and Northwestern Ave. (western termini of the proposed project) is 0.3 miles from the Celery Bog. The proposed project will not cause any adverse impacts to the natural area.

Performed by: Brianna W. Chapman

Date: April 29, 2008



National Wetland Inventory



Project Area



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AMERICAN
STRUCTUREPOINT
INC

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National Wetland Inventory Mapping
US Fish and Wildlife Service Data

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Appendix F
Page 5 of 6

Date: 05/14/2009

 Project Area
 100 Year Floodplain



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FEMA 100-Year Floodway Mapping

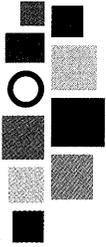
Applicant: City of West Lafayette
 City Hall
 609 West Navajo Street
 West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
 Township: Wabash
 County: Tippecanoe
 State: Indiana

Date: 05/14/2009

Appendix G: Public Involvement
a. Hearing Certification Packet



AMERICAN
STRUCTUREPOINT
INC.

March 1, 2010

Ms. Rebecca L. Pyland
Program Director
INDOT Hearing Section
Indiana Government Center North, Room N955
100 North Senate Avenue
Indianapolis, Indiana 46204-2228

Re: Request for Public Hearing Certification
Des. No. 0600696
Yeager Road Improvement Project
West Lafayette, Tippecanoe County, Indiana
Project No. IN20061278

Dear Ms. Pyland:

We are transmitting the following material in support of our request for certification of public hearing requirements.

1. Legal Notice of Public Hearing
2. Complete mailing list, including property owners
3. Publisher's affidavit from the Journal and Courier
4. Public Hearing speakers presentation
5. Verbal statement transcript
6. Information packet distributed at the Public Hearing
7. Summary of verbal written comments received and responses

IN20061278



○ AMERICAN STRUCTUREPOINT, INC.

Ms. Rebecca Pyland

March 1, 2010

Page 2

The Legal Notice of Public Hearing was published in the Journal and Courier on January 26, 2010, and February 3, 2010. The advertised date for the hearing was Wednesday February 10, 2010, with an open house at 6 p.m. and formal presentation at 7 p.m. in the City Council Chambers of the West Lafayette City Hall at 609 West Navajo Street, West Lafayette, Indiana.

One property owner verbally expressed their comments during the public statement session at the Public Hearing. The opportunity for additional written comments was given with an established deadline of February 24, 2010. A summary of all verbal and written comments and responses is included with this letter.

On behalf of the City of West Lafayette, American Structurepoint Inc., is requesting your assistance in an expedited review of the Hearing Certification Packet and issuance of the Hearing Certification. This request for an expedited review was precipitated based on public comments at the hearing by the family of a resident that will be relocated as part of the project. As documented in the hearing transcript, the resident is in need of assisted care and the family is unable to obtain this care due to financial reasons until the property is acquired. INDOT has stated FMIS approval for right-of-way acquisition will not be granted until the environmental document is approved. Therefore, we are working to assist the City of West Lafayette to expedite the remaining steps in the environmental document approval. Any assistance on your part to expedite the approval on the environmental document will be appreciated.

Please contact me at (317) 547-5580 with any questions or comments.

Very truly yours,
American Structurepoint, Inc.


Brianna W. Chapman
Environmental Scientist

BWC:ems

cc: David M. Buck, City of West Lafayette, Engineering Department

Enclosures

Legal Notice of Public Hearing

Legal Notice
of
Public Hearing

The City of West Layette will hold a public hearing on Wednesday, February 10, 2010, starting with an open house from 6:00 p.m. to 7:00 p.m. and the formal public hearing beginning at 7:00 p.m. in the West Lafayette City Hall, City Council Chambers at 609 West Navajo Street, West Lafayette, Indiana.

The purpose of the public hearing is to offer all interested persons an opportunity to comment on the Level 3 Categorical Exclusion Document (CE) and supporting documents for the proposed Yeager Road Reconstruction Project. The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52) in West Lafayette, Tippecanoe County, Indiana. The project is located in Sections 7 and 12, Township 23 North, Range 4 West, Wabash Township, Indiana. Yeager Road is a north-south arterial roadway that handles significant daily traffic demand and is a keystone in the network of roadways used to alleviate heavy Purdue event traffic. Presently, Yeager Road is a 3-lane arterial with multiple private and commercial driveways.

The scope of the project includes widening and reconstructing 1,300 feet of Yeager Road to a 5-lane arterial road. The proposed improvement will include the addition of two through lanes, 12 feet wide, and one 16-foot left-turn-only lane. New gutters and curb will be installed along both sides of the road, and a new multi-use path will be along the west side of the road. Drainage improvements will also be included. A modern roundabout will be constructed at the existing intersection of Northwestern Avenue and Yeager Road. The existing Yeager Road intersection with Northwestern Avenue will require reconstruction to accommodate the added travel lanes and improve the substandard skew and curvature to meet design standards.

The proposed project plans to acquire 1.54 acres of permanent right-of-way and 0.72 acre of temporary right-of-way for grading, driveway construction, and path/sidewalk construction. One residential and one commercial relocation are anticipated as part of the proposed project.

Maintenance of traffic will consist of phasing the construction into four phases to provide through lanes in both directions during construction. Access to businesses and residences will be available during all phases of construction. All lanes shall remain open and unobstructed during Purdue University home football games as this is a time of increased vehicular and pedestrian traffic.

The proposed project will require approximately 0.41 acre of strip right-of-way from the Purdue University Kampen Golf Course. As the golf course is a public recreational facility, it is protected under Section 4(f) of the U.S. Department of Transportation Act.

Section 6009 of SAFETEA-LU allows determinations that certain uses of 4(f) land will have no adverse effect – *de minimis* – on protected resources. A Section 4(f) ‘*de minimis*’ Impact Determination was approved for the Yeager Road Reconstruction Project by INDOT and FHWA on November 23, 2009.

The Federal Highway Administration and the Indiana Department of Transportation have agreed that this project falls within the guidelines of a Categorical Exclusion Level 3, with no significant adverse effect to the environment or community.

A public statement session will be offered after the presentation of the project. Individuals interested in participating in the public statement session may sign the speakers schedule prior to the presentation.

All comments collected before, during, and for a period of two (2) weeks after the hearing, will be evaluated and addressed in the Final Environmental Document. Before and after the formal hearing, the Environmental Document will be available for review. Anyone interested in talking to the engineers about the project may do so before or after the formal hearing. Conversations prior to or after the formal hearing will not be part of the official record.

Information on the project is available for viewing at the following locations:

- City of West Lafayette City Hall, 609 West Navajo Street, West Lafayette, IN 47906
- City of West Lafayette Public Library, 208 West Columbia Street, West Lafayette, IN 47906

This notice is published in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled “Early Coordination, Public Involvement and Project Development”, and the Indiana Public Involvement/Public Hearing Procedures for Federal-Aid Project Development approved by the Federal Highway Administration, US Department of Transportation on July 8, 1997.

Brianna Chapman
American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, Indiana 46256

WINTER WEATHER NOTICE

This meeting will be rescheduled at a later date if hazardous winter weather conditions arise. Please call (317) 547-5580 to find out whether the meeting will be held or postponed.

Complete Mailing List

**Complete Mailing List
Yeager Road Improvement**

Mike Wink	Crawfordsville District PM	41 West 300 North	Crawfordsville, IN 47933
Mike Eubank PE	Crawfordsville District ESM	42 West 300 North	Crawfordsville, IN 47933
Mary Jackman	INDOT - Hearings Section	100 North Senate Avenue	Indianapolis, IN 46204
Rickie Clark	INDOT - Hearings Section	101 North Senate Avenue	Indianapolis, IN 46204
Steve Buyer	4th District Legislature	100 South Main Street	Monticello, IN 47960
Sallie Dell Fahay	TCAPC	20 North 3rd Street	Lafayette, IN 47901
Ben Lawrence	INDOT - Office of Environmental Services	101 North Senate Avenue	Indianapolis, IN 46204
Larry Heil	Federal Highway Administration	575 North Pennsylvania Street	Indianapolis, IN 46204
Mathew W. Kopf	Property Owner	2306 Northwestern Avenue	West Lafayette, IN 47906
Richfield Apartments of West Lafayette	Property Owner		
		2300 Yeager Road	West Lafayette, IN 47906
Lou Ann Steckel	Property Owner	2331 Yeager Road	West Lafayette, IN 47906
Community Rentals, LLP	Property Owner	2339 Yeager Road	West Lafayette, IN 47906
Anthrop Limited Liability Company	Property Owner	1193 Anthrop Drive	West Lafayette, IN 47906
Eleanor M. Kelly and Jeanne E. McGrath	Property Owner		
Sunshine Residency, LLC	Property Owner	2336 Northwestern Avenue	West Lafayette, IN 47906
Michael J. and Emily J. Lin	Property Owner	2367 Yeager Road	West Lafayette, IN 47906
Horizon Properties I, LLC	Property Owner	2400 Yeager Road	West Lafayette, IN 47906
Michael J. and Emily J. Lin	Property Owner	2417 Yeager Road	West Lafayette, IN 47906
Michael J. and Emily J. Lin	Property Owner	2406 Yeager Road	West Lafayette, IN 47906
Rredy N-Terprises, LLC	Property Owner	2412 Yeager Road	West Lafayette, IN 47906
David Hoodie	Purdue Research Foundation	1199 Sagamore Parkway West	West Lafayette, IN 47906
West Lafayette Lodge 724 F. & A. M.	Property Owner	1281 Win Hentschel Blvd., Suite 2212 Yeager Road	West Lafayette, IN 47906

Publisher's Affidavit

Journal and Courier
217 N. 6th Street
Lafayette, IN 47901

American Structure Point

(Governmental Unit)

Ad order #

Tippecanoe County, Indiana

1174338

PUBLISHER'S CLAIM

LINE COUNT

Display Matter (must not exceed two actual lines, neither of which shall total more than four solid lines of the type in which the body of the advertisement is set)—number of equivalent lines.

Head--number of lines
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Tail--number of lines

Total number of lines in notice

COMPUTATION OF CHARGES

274.00 lines 1.00 columns wide equals 274.00 equivalent

lines at 0.4880 cents per line.

\$ 133.71

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(50 percent of above amount)

\$ 0.00

Charge for extra proofs of publication

(\$1.00 for each proof in excess of two)

\$

TOTAL AMOUNT OF CLAIM

\$ 133.71

DATA FOR COMPUTING COST

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Pursuant to the provisions and penalties of Ch. 155, Acts 1953, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and the no part of the same has been paid.
Brandy Stevens, Clerk

PUBLISHER'S AFFIDAVIT

Personally appeared before me, a notary public in and for said county and state, the undersigned Brandy Stevens who, being duly sworn, says that she is Clerk of the Journal and Courier a daily newspaper of general circulation printed and published in the English language in the city of Lafayette in state and county aforesaid, and that the printed matter attached hereto is a true copy, which was duly published in said paper 1 time(s), the date(s) of publication being as follows :

1/26/2010

2/3/2010

Subscribed and sworn to before me this : Tuesday, February 9, 2010

Brandy Stevens

UPPER STORE

USED CAR

Des. No.: 0600696
Legal Notice
of Public Hearing
The City of West Lafayette will hold a public hearing on Wednesday, February 10, 2010, starting with an open house from 6:00 p.m. to 7:00 p.m. and the formal public hearing beginning at 7:00 p.m. in the West Lafayette City Hall, City Council Chambers at 609 West Navajo Street, West Lafayette, Indiana. The purpose of the public hearing is to offer all interested persons an opportunity to comment on the Level 3 Categorical Exclusion Document (CE) and supporting documents for the proposed Yeager Road Reconstruction Project. The City of West Lafayette is developing plans to reconstruct Yeager Road from Northwestern businesses and residences will be available

1/26/2010

2/3/2010

Subscribed and sworn to before me this : Tuesday, February 9, 2010

ORD • BROWSE THE CLASS

able during all phases of construction. All lanes shall remain open and unobstructed during Purdue University home football games as this is a time of increased vehicular and pedestrian traffic. The proposed project will require approximately 0.41 acre of

ed in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled "Early Coordination, Public Involvement and Project Development"; and the Indiana Public Involvement Public Hearing Procedures for Federal-Aid Project Development

47906
This notice is published

Public Hearing Presentation



Public Hearing

Yeager Road Improvements Project
Between US 231 and US 52
West Lafayette, Tippecanoe County, Indiana

February 10, 2010

AMERICAN
STRUCTUREPOINT
DEFINING THE BUILT ENVIRONMENT

Project Team

- City of West Lafayette
 - John Dennis, Mayor
 - David Buck, City Engineer
- American Structurepoint, Inc.
 - Clint Sparks, PE, Project Director
 - Jeromy Grenard, PE, Project Manager
 - Christopher Schultz, PE, Project Engineer
 - Briana Hope, Environmental Scientist
 - Brianna Chapman, Environmental Scientist

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DEFINING THE BUILT ENVIRONMENT

Public Hearing Agenda

- Hearing and comment process
- Presentation of proposed project
 - Project description
 - Right-of-Way (R/W) impacts
 - Anticipated project costs and schedule
 - Land acquisition
 - Environmental impacts
- Publicly recorded statements
- Individual question and answer period

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Purpose of Public Hearing

- Provide overview of proposed improvements
- Present environmental and design documents prepared for proposed project
- Gather comments, concerns, and suggestions on environmental issues and the proposed design

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Public Hearing Process

- Will be recorded and transcribed for public record
- Presentation of proposed project
- Comment period
- Closing
- Individual question and answer session

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Public Comment Process

- Verbally to audio recorder
- Comment sheet in information packet
- Mail comments to American Structurepoint, Inc.
- Formal public statement
- Comment deadline – February 24, 2010
- Comments will be addressed in the Environmental Document

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Legal Notice of Public Hearing

- Legal Notice of Public notice of hearing was advertised in the *Lafayette Journal and Courier* on January 26 and February 3, 2010
- A copy of the Legal Notice was mailed to as many affected property owners as possible
- Mailing list sign-up sheet

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Documents Available for Review

- Categorical Exclusion (CE) Report
- Viewing locations:
 - City of West Lafayette City Hall
Engineering Department
609 West Navajo Street
West Lafayette, Indiana 47906
 - City of West Lafayette Public Library
208 West Columbia Street
West Lafayette, Indiana 47906

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Need for Improvement

- Improve capacity
 - It is anticipated that Yeager Road traffic demand will increase as West Lafayette develops to the north
- Safer travel way
 - Provide a safer travel way for pedestrians and vehicles
 - Increase pedestrian connectivity
- Improve deteriorating pavement
- Improve drainage

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Location Map



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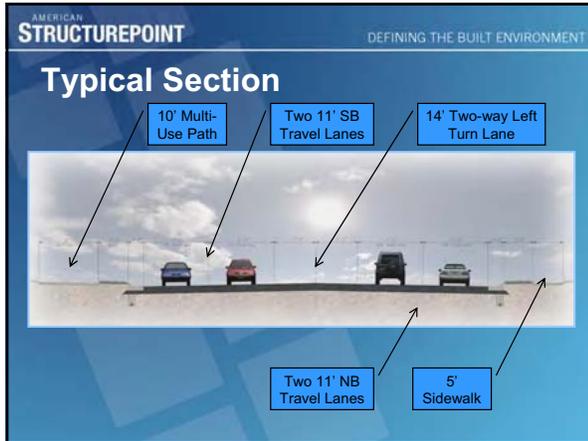
Project Description

- Project limits
 - From US 231 (Northwestern Avenue) to US 52 (Sagamore West)
 - Approximately 0.22 mile
- Planned improvements
 - 20 year projected traffic data increasing by 4,000 vehicles/day to 21,100 vehicles/day
 - Therefore, design will accommodate an additional northbound and southbound travel lane
 - Curve correction at south end of project (intersection with Northwestern Avenue)

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Project Description

- Planned Improvements (cont'd.)
 - Drainage improvements
 - Roundabout intersection improvement at Northwestern Avenue
 - Additional turn lane storage length and traffic signal upgrades at US 52
 - Addition of curbs, multi-use path, and lighting



- AMERICAN STRUCTUREPOINT DEFINING THE BUILT ENVIRONMENT
- ## Maintenance of Traffic
- Improvements to be constructed under traffic
 - Yeager Road
 - One lane open in each direction throughout construction
 - All properties accessible throughout construction
 - No lane reductions during Purdue football home game weekends
 - Northwestern Avenue
 - One lane open in each direction throughout construction
 - No lane reductions during Purdue football home game weekends
 - US 52
 - No through-lane closures

- AMERICAN STRUCTUREPOINT DEFINING THE BUILT ENVIRONMENT
- ## Four Alternatives Studied
- Do Nothing
 - Current configuration would not accommodate future traffic demands
 - Pedestrian facilities and roadway safety are not addressed as traffic increases
 - Pavement in fair to poor condition
 - Three-Lane Reconstruction
 - Would improve pedestrian and motorist safety
 - Would not accommodate future traffic volumes
 - Four-Lane Reconstruction
 - Number of drives/left turns makes this option very similar to three-lane option
 - Five-Lane Reconstruction (Selected Alternative)
 - Would improve pedestrian and motorist safety
 - Would accommodate future traffic volumes

- AMERICAN STRUCTUREPOINT DEFINING THE BUILT ENVIRONMENT
- ## Right-of-Way Requirements
- Land Use (Yeager Road)
 - Mix of residential and commercial
 - R/W Width
 - Currently varies from ~36 feet to 60 feet
 - Proposed – 100 feet
 - Relocations
 - Two relocations anticipated near the Northwestern Avenue intersection

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R/W Requirements

Land Use	Permanent R/W
Residential	1.56
Commercial	0.87
Other	0.42
Total (acres)	2.85

- AMERICAN STRUCTUREPOINT DEFINING THE BUILT ENVIRONMENT
- ## Land Acquisition Process
- Acquisition
 - Explains the process of buying property needed for highway improvements
 - Relocation
 - Explains the process of relocation and the Relocation Assistance Program
-

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Land Acquisition Process

- “Uniform Act” of 1970
 - All federal, state, and local governments must comply
 - Requires just compensation – cannot be less than fair market value
- Acquisition process
 - Two independent appraisals
 - Offer
 - Negotiations
 - If no agreement, can go to mediation or condemnation

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Corridor Project Costs

- Roadway construction = \$1,670,000
- Engineering/CI = \$ 550,000
- R/W acquisition = \$1,072,000
- **Project Total = \$3,292,000**

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Project Schedule

- Heavily influenced by
 - Large number of utilities in corridor
 - R/W acquisition
 - Purdue football schedule
 - Other significant construction projects in the vicinity
- Anticipated schedule
 - 2010 and 2011: R/W purchasing and utility relocations
 - 2012 – Most of Yeager Road, with temporary intersection at Northwestern Avenue
 - 2013 – Remainder of work on Yeager, Northwestern Avenue roundabout

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Environmental Impacts

- Categorical Exclusion (CE) examines potential social, economic, and environmental factors
- CE reviewed by Indiana Department of Transportation (INDOT)
- Released for public involvement on January 11, 2010
- No areas of significant impact identified

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Environmental Impacts

- Scope of environmental study
 - Evaluate impacts to natural and human environment
 - Waterways, wetlands, and endangered species
 - Historical properties
 - Social and economic factors

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Communication and Information

- Thirty-five federal, state, and local agencies asked to provide input
- Coordinated with several state and local historic preservation organizations

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Communication and Information (cont.)

- Archaeological reconnaissance and Historical Properties investigation performed
 - One property, Yeager Road Masonic Temple, was recommended as eligible for the National Register of Historic Places
 - Determination of “No Adverse Effect” (State Historic Preservation Office (SHPO) concurred on September 2, 2009)
 - Public Notice advertised on August 19, 2009, to provide the public an opportunity to comment on the “No Adverse Effect” finding

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Section 4(f) Resources

- Purdue University Kampen Golf Course
 - Public recreational facility protected under Section 4(f) of the Department of Transportation Act of 1966.
 - ‘*de minimis*’ impact
 - Proposed project will acquire a strip of right-of-way from the golf course.
 - Proposed project will not impact the activities, features, and attributes that qualify the Kampen Golf Course under protection of Section 4(f).
 - Purdue Research Foundation stated no objections to the location of the intersection and how it will affect the Kampen Golf Course.
 - Section 4(f) Document and ‘*de minimis*’ finding was approved by INDOT on November 18, 2009 and by the Federal Highway Administration on November 23, 2009.

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Environmental Impacts

“Waters of the US”

- **Site Reconnaissance and Ecological Site Evaluation Form**
 - No jurisdictional wetlands or streams identified

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Environmental Requirements

Permits Expected to be Required:

- Rule 5 Erosion Control – Tippecanoe County Soil and Water Conservation District

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Public Comments

- American Structurepoint, 7260 Shadeland Station, Indianapolis, Indiana 46256, Attn: Brianna Chapman
- Email: bchapman@structurepoint.com
- Respectfully request comments postmarked by February 24, 2010
- All comments will be reviewed, evaluated, and given full consideration during the decision making process

Verbal Statement Transcript

**Yeager Road/Northwestern Avenue Public Hearing – Public Comments
February 10, 2010**

My name is Luanne Steckel. My mother lives at 2331 Yeager Road, which is the second house north of Northwestern on the west side. We've really been aware of this project for about a year. Let's see, my dad died in March, and Dave and John came out about a month after he died and told my mom they were going to tear down her house. Well, they really didn't say it that way, but you know, they said that she was going to lose her house. And of course, she was tearful; she's lived there 58 years. That's the only house I've ever known. I have a lot of issues about the things that we say. I was just jotting down some stuff, and I don't really have my glasses, but you know, it's like drainage issues. You know, I've lived there 55 years, and you all never wanted to put in curbs and gutters there in the 55 years I've been alive. I remember when they used to tar the road. I don't know how that affects your environmental stuff, but I'm pretty sure it wasn't a good idea. And then we now say, it doesn't really affect anybody socially or economically, you're wrong. You're just flat out wrong. It affects that little woman right there that's 89 years old. Part of the presentation talked about all properties will be accessible throughout construction, including...does that include the ones torn down? I didn't think we wanted that part on public record somehow. And then the other thing I guess I need you to know, and this is going to be hard for me to say, but when they approached us a year ago, we went out to our attorney and we got an appraiser. And we had an appraisal done, and we were told their appraiser would be there in August, and we haven't heard anything. You know, and I told mom at the time, it would be three years before the government moved and so not to really worry about it. She just turned 89 in January, and she's starting to fall. The house is two stories. I need to move my mother to a different location, but I can't sell my house, because who's going to buy it? Anyone here want to buy 2331 Yeager Road. And then get an offer from the city or whatever government we're talking about? No, I didn't think so. And my mom's assets are in that house. So I can't move her without it. So that's where your social and economic stuff comes in. It comes in big time when you add the personal factor. And I know Purdue is important to this town, but it's my job to be the advocate for my mom, and I appreciate you listening.

Information Packet



AMERICAN
STRUCTUREPOINT
INC.



PUBLIC HEARING INFORMATION PACKET

Yeager Road Reconstruction
From US 52 (Sagamore Parkway) to US 231 (Northwestern Avenue)
West Lafayette, Tippecanoe County, Indiana
Designation No. 0600696

February 10, 2010

Open House 6 p.m.
Public Hearing 7 p.m.

West Lafayette City Hall, City Council Chambers
609 West Navajo Street, West Lafayette, Indiana

Prepared by:

American Structurepoint, Inc.
7260 Shadeland Station
Indianapolis, IN 46256



February 10, 2010

Dear Resident:

Welcome to the City of West Lafayette public hearing regarding the proposed reconstruction of Yeager Road from US 52 to US 231. The total combined project length is 0.22 mile.

The purpose of the public hearing is to explain the project to interested people and to receive their comments. There are several ways your comments can be presented.

1. You may verbally express your concerns tonight by using the tape recording equipment that will be available after the presentation. You may do this by signing the Speaker's Schedule, located at the table with the handout materials. All statements will be recorded and transcribed so it may be included in the official public hearing transcript.
2. You may complete one of the comment sheets (attached) and turn it in utilizing one of the following methods.
 - a. Return it to one of the hearing representatives from American Structurepoint, Inc., in attendance this evening.
 - b. Mail it to Brianna Chapman at American Structurepoint, Inc., 7260 Shadeland Station, Indianapolis, IN 46256.
 - c. Fax it to (317) 543-0270, attention Brianna Chapman.
 - d. Email it to bchapman@structurepoint.com.

All written comments received by February 24, 2010, will become a part of the transcript and will be addressed in subsequent project documentation, along with the verbal comments presented here tonight.

All comments received will be evaluated and responded to in writing within subsequent project documentation. The document will address all concerns raised during the public hearing process and describe the final decisions reached following careful consideration of the views and concerns of the public. Every comment received as a part of this process will be addressed.

The draft environmental document is available for public review and inspection at the following locations.

- City of West Lafayette City Hall, Engineering Department, 609 West Navajo Street, West Lafayette, Indiana 47906
- City of West Lafayette Public Library, 208 West Columbia Street, West Lafayette, Indiana 47906

This information will remain available for public review and inspection. In addition, when Final Approval is granted for this proposed project, copies of the Final Environmental Document will also be made available at the locations mentioned above

This notice is provided in compliance with Title 23, Code of Federal Regulations, Section 771.111(h) entitled "Early Coordination, Public Involvement and Project Development," and the Indiana Public Involvement/Public Hearing Procedures for Federal-Aid Project Development approved by the Federal Highway Administration, US Department of Transportation on July 8, 1997.

Hearing Agenda

Engineers will be available in the display area to explain proposed project details and environmental impacts before and after the official hearing.

1. Meeting Called to Order
Clint Sparks
American Structurepoint
2. Hearing and Comment Process
Briana Hope
American Structurepoint
3. Project Need, Location, and Description
Jeromy Grenard
American Structurepoint
4. Maintenance of Traffic
Jeromy Grenard
American Structurepoint
5. Alternatives Studied
Jeromy Grenard
American Structurepoint
6. Right-of-Way Requirements
Amount and Description
Jeromy Grenard
American Structurepoint
7. Project Schedule and Costs
Jeromy Grenard
American Structurepoint
8. Environmental Document Approval
Brianna Chapman
American Structurepoint
9. Public Statement Session
10. Questions and Answers – Engineers will be available in the display area to answer questions.

Adjournment

Location and Project Description

This project involves the widening of Yeager Road between US 52 (Sagamore Parkway) and US 231 (Northwestern Avenue) and reconstruction of the intersection of Yeager Road and US 231 in West Lafayette, Tippecanoe County, Indiana.

The design of the project will meet all federal, state, and American Association of State Highway and Transportation Officials (AASHTO) design criteria. The scope of the project is to widen and reconstruct 0.22 mile (1,171 feet) of Yeager Road to a 5-lane urban arterial. The project also includes approximately 630 feet of improvements along US 231. The proposed road cross-section will include four thru lanes that will be 11 feet wide and one, two-way left-turn lane (TWLTL) that will be 14 feet wide. A modern roundabout will be constructed at the existing intersection of US 231 (Northwestern Avenue) and Yeager Road. Concrete curb and gutters will border the roadway. In addition, a new sidewalk will be constructed on the east side of Yeager Road and a multi-use path/trail will be constructed on the west side of the roadway, new street lighting will be installed, and new storm sewers and roadside drainage will be constructed.

Need for Improvement

The primary purpose of the proposed project is to provide system linkage, sufficient capacity, and safe travel for motorists, bicycles, and pedestrians along Yeager Road. A secondary purpose of the project is to address the deteriorating pavement and upgrade the storm drainage system.

It is necessary to improve the capacity, deteriorating pavement, and upgrade the storm drainage system because Yeager Road is a main north-south corridor in West Lafayette. Yeager Road/Northwestern Avenue is one of only three major north-south corridors in West Lafayette, with the other two being Salisbury Street and Soldiers Home Road/Happy Hollow Road/River Road. Yeager Road connects a federal highway, US 231 (Northwestern Avenue), with another federal highway, US 52 (Sagamore Parkway) and provides two of the US 52 and US 231 interchange movements (northbound to eastbound and westbound to southbound). Further improvements to Yeager Road north of Sagamore Parkway are shown in the Tippecanoe County Area Plan Commission's (TCAPC) 2025 Plan.

Yeager Road is a main thoroughfare for Purdue University football and basketball traffic, and thus experiences a significant amount of traffic during home football and basketball games.

It is also necessary to address safety concerns in the project area. Crash data collected from 2006, 2007, and half of 2008, identified 33 crashes within 500 feet of the intersection of Yeager Road and Northwestern Avenue. Upon studying the crash data and considering the factors involved in 82 percent of the crashes, the need to reduce vehicle speeds and improve intersection geometry were identified.

In addition, multi-residential properties and student housing are located within the project area. Because of the large student population residing in the area, there is a need for safe pedestrian and bicycle multi-use paths to provide additional access to and from campus and retail centers, as well as the recreational benefit that a multi-use path would provide. The multi-use path would also provide bicyclists and pedestrians with vital links to other planned Trails/Greenway projects identified in the 2009 TIP for the TCAPC.

Maintenance of Traffic

Traffic will be maintained in the following manner throughout construction of the Yeager Road Improvements:

Yeager Road

- At least one northbound and one southbound lane will be open to traffic at all times.
 - Construction will begin on the east half of Yeager Road, and two lanes of traffic will be maintained on the west half, utilizing the southbound lane and the two-way left-turn lane.
 - Construction will then shift to the west half of Yeager Road, and two lanes of traffic will be maintained on the reconstructed half, utilizing the two northbound lanes.
- During Purdue football home game weekends, no lane restrictions will be in place.
- Driveways will be accessible at all times

US 231 (Northwestern Avenue)

- At least one lane in each direction will be open to traffic at all times.
- A majority of the work on Northwestern Avenue will happen during Purdue's summer session, so as to minimize disruption to the traveling public.
- During Purdue football home game weekends, no lane restrictions will be in place.

US 52 (Sagamore West)

- Only minimal closures of the eastbound right turn lane will occur. No through lanes will be closed.

Design Data

Yeager Road (Line “A”)

Project Standards	4R (Non-Freeway)
Functional Classification	Urban Primary Arterial
Terrain	Level
Design Speed	35 mph
Posted Speed	30 mph
Access Control	None
Number of Lanes and Widths	4 at 11’; 1 at 14’ TWLTL
Shoulders/Curb	Curb and Gutter
Curb Offset	2’-0”
Right-of-Way Width	50’ Lt., 50’ Rt. (100’ total)
Clear Zone	10’-0”
Side Slopes	3:1 or flatter

US 231 Northwestern Avenue (Line “B”)

Project Standards:	4R (Non-Freeway)
Functional Classification:	Urban Divided Primary Arterial
Terrain:	Level
Design Speed:	45 mph
Posted Speed:	45 mph
Access Control:	Partial Limited Access
Number of Lanes and Widths:	5 at 12’
Shoulders/Curb:	Curb and Gutter
Curb Offset:	2’-0”
Maximum Right-of-Way Width:	65’ Lt., 70’ Rt. (135’ total)
Clear Zone:	10’-0”
Side Slopes:	3:1 or flatter

Traffic Data

	<u>Yeager Road</u>		<u>US 231</u>	
AADT (2008)	17,290	VPD	23,830	VPD
AADT (2028)	21,100	VPD	29,080	VPD
DHV (2028)	1,899	VPH ¹	2,617	VPH ¹
Directional Distribution	50	% ²	50	% ²
Trucks	2% ³	% DHV	2% ³	% DHV

The AADT for Yeager Road is from the Tippecanoe County Area Plan Commission Average Daily Traffic Estimate for 2005 projected to 2008 and 2028 using a growth rate of 1 percent annually. The AADT for US 231 is from INDOT Annual Daily Traffic Estimate for Tippecanoe County for 2002 projected to 2008 and 2028 using the same growth rate.

¹ - DHV was figured assuming a K-Value of 0.09

² - Assumed distribution

³ - Assumed percentage

AADT = Annual Average Daily Traffic

DHV = Design Hour Vehicles

VPD = Vehicles Per Day

VPH = Vehicles Per Hour

Description of Right-of-Way

The project will require the purchase of right-of-way. Refer to the pamphlets distributed at this meeting. ACQUISITION – Acquiring Real Property for Federal and Federal-Aid Programs and Projects; and RELOCATION – Your Rights and Benefits as a Displaced Person Under the Federal Relocation Assistance Program.

Approximately 2.85 acres of additional permanent right-of-way is needed from 15 parcels. The right-of-way will be predominantly residential with some commercial. Approximately 0.72 acre of temporary right-of-way is required for drive construction, lawn grading, sidewalk construction, drive removal, and building removal. The majority of the acquisitions will be in the form of narrow strips paralleling the existing public right-of-way along the frontage of the parcels. One residential relocation and one commercial relocation are anticipated for the proposed project

Project Schedule

The Yeager Road Reconstruction schedule is heavily influenced by the number of utilities that will need to be relocated, the amount of right-of-way that needs to be purchased, the schedule of other significant scale construction projects in the vicinity of the project site, and the Purdue football schedule. For these reasons, the following dates could slide forward or backward by one to two years.

Right-of-Way Appraising Begins	Spring 2010
Right-of-Way Purchasing and Utility Relocations	2010 and 2011
Anticipated Construction Start	Spring 2012

Estimated Project Cost Summary (2010)

Engineering and Construction Inspection ⁵	\$ 550,000
Right-of-Way	\$ 1,072,000
Construction	\$ 1,670,000
Total Cost	<u>\$ 3,292,000</u>

⁵ Includes survey and right-of-way engineering costs

Environmental Document Approval

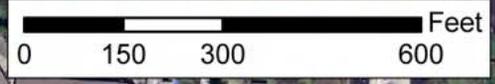
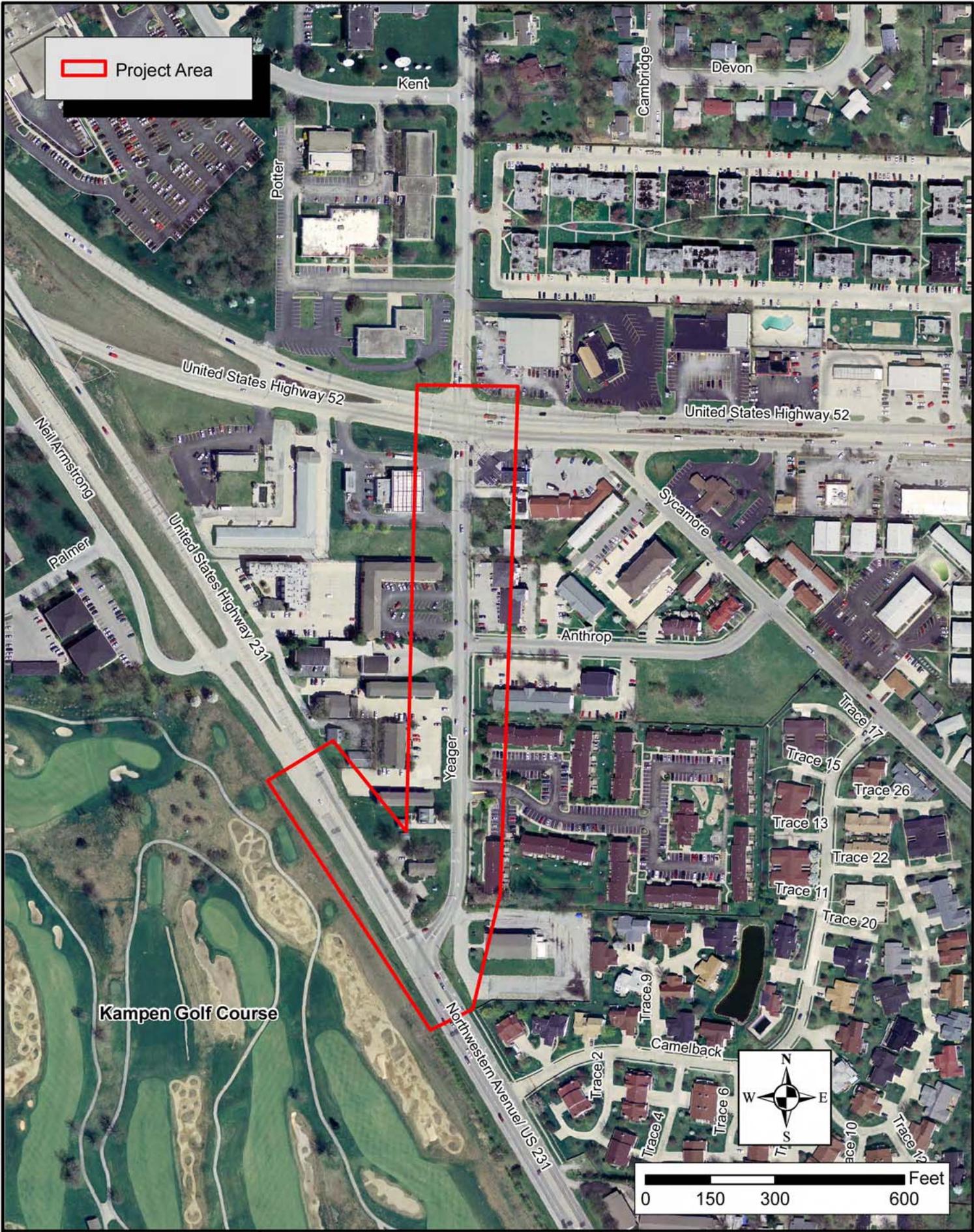
The Indiana Department of Transportation (INDOT) has reviewed the Categorical Exclusion (CE) Level III prepared by American Structurepoint, Inc., for this project and released the report for public involvement on January 11, 2010. The CE evaluates the impact of the overall project on the natural and human environment. No areas of potentially significant impacts have been identified.

The project has been coordinated with several state and local historic preservation organizations, including the State Historic Preservation Officer (SHPO), in compliance with Federal Highway Administration (FHWA) procedures for Indiana projects. With the Area of Potential Effect, one structure eligible for inclusion on the National Register of Historic Places was noted, the Masonic Temple located at 2212 Yeager Road. The proposed project will not adversely impact the eligible property. INDOT, on behalf of FHWA, has issued a Determination of "No Adverse Effect" on August 7, 2009. SHPO concurred with the finding on September 2, 2009.

In addition to the historic resources related to this project, the CE also considered a broad range of potential social, economic, and environmental factors. Twenty-four federal, state, and local agencies were contacted and asked to provide input regarding the project's impacts on areas in which they have either jurisdiction or special expertise.

It is not expected the project will require a Section 404 Permit issued by the US Army Corps of Engineers (USACE) or a Section 401 Water Quality Certification from the Indiana Department of Environmental Management (IDEM). It is expected however, that a Rule 5 Submittal for Erosion/Sediment Control Plan reviewed by the Tippecanoe County Soil and Water Conservation District and the IDEM will be required for the proposed project.

 Project Area



\\lindysamp\Projects\IN2006\1278\Drawings\Work\Environmental\Arcview\Exhibits\IN2006\1278.EV\2010-02-09.PH.Aerial.mxd



**AMERICAN
STRUCTUREPOINT
INC**

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Project Location Map

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 10/05/2009

Summary of Verbal and Written Comments Received and Responses

Summary of Public Comments

1. Luanne Steckel (Verbal)	Concerning curbs and gutters, they were never needed before.	At the time that the existing Yeager Road was built, this area was still somewhat rural in nature. It has built up substantially since that time, and thus warrants an urban cross-section. Curb and gutter is a requirement on all urban arterials per the Tippecanoe County Thoroughfare Plan.
	How can it be said this project does not affect people socially or economically?	This project will affect the community socially and economically; however, according to National Environmental Policy Act and associated policies, the project will not have a substantial adverse impact. The City of West Lafayette is very sensitive to the impacts that property acquisition will have on the family and business that must relocate and are trying to assist to the greatest extent possible.
2. Allen Busenitz (Email)	The north entrance to the BP station should be closed and the south entrance/exit to the BP station should be widened by a lane. Cars turning left off of US 52 onto Yeager must stop in the intersection of Yeager and US 52 when cars turn left and immediately turn right into the BP Station	This situation will be discussed with the property owner, and the plans will be updated, if necessary. Currently, the BP station receives its fuel deliveries on the north side of the lot, and the project team is uncertain if closing this drive will have an impact on these deliveries. The proposed plan has an additional southbound lane, which will allow traffic to be able to go around vehicles queued at the entrance to BP.
3. Joe Krause (Email)	In support of the project: 1. “A paved trail would be helpful for pedestrians and bus passengers whenever CityBus bypassed Anthrop-Sycamore-Carlisle because of sidewalk and street construction” 2. Often make left turns off of Yeager. 3. Supports roundabouts	The project on Sycamore Lane will be complete before construction begins on the Yeager Road project. The paved trail will be beneficial for the many area residents, as well as to those using the overall West Lafayette trail system, as this will eventually be a vital link in that system.
	Diminished parking at Asia Market is a concern.	Coordination with the owners of the Asia Market have been initiated. The City of West Lafayette will work with the business to find suitable compensation for any loss of parking.
	The City should move quickly to purchase the property at 2331 Yeager Road as requested by the homeowner.	The project team is doing everything possible to expedite the purchase of this parcel. The Categorical Exclusion document must be approved before appraisals can begin, and Public Hearing certification is the last step before submitting the Categorical Exclusion document for approval.

<p>3. Joe Krause (Email) Cont'd.</p>	<p>Access to the Prestige Inn is difficult at this time. The proposed roundabout would slow traffic down along Northwestern therefore motorists could more safely exit from the Prestige Inn parking lot on to northwestern.</p>	<p>This comment may be referring to the properties that access Northwestern Avenue directly across from Neil Armstrong Drive. It is true that the roundabout will now slow traffic heading north from the Yeager Road and Northwestern Avenue intersection. This is likely to translate into a safer exit from this driveway.</p>
<p>4. Aaron Washburn (Email)</p>	<p>Do not want roundabouts in West Lafayette, no one knows how to use them, and therefore may lead to accidents.</p>	<p>A roundabout intersection is much different than a signalized intersection; however, the City of West Lafayette has been making progress in public education of how to use roundabouts, and will continue to pursue public education at every opportunity. As proven by their use at other locations throughout Indiana and the United States, roundabouts are safer than traditional intersections. There are major tourist destinations (with many first-time drivers), such as the ski village of Avon, Colorado, that make extensive use of multi-lane roundabouts with less crashes than before the roundabouts were constructed.</p>
	<p>Do not need the proposed trail. It is a bad location for a trail.</p>	<p>Because of the large student population residing in the area, there is a need for safe pedestrian and bicycle multi-use paths to provide additional access to and from campus and retail centers, as well as the recreational benefit that a multi-use path would provide. The multi-use path would also provide bicyclists and pedestrians with vital links to other planned Trails/Greenway projects identified in the 2009 Transportation Improvement Plan (TIP) of the Tippecanoe County Area Plan Commission (TCAPC). The TIP can be found on the TCAPC website.</p>
	<p>The money required for the project could be spent elsewhere, such as road resurfacing. There is not enough traffic on Yeager Road to warrant any changes.</p>	<p>Currently, 17,290 vehicles per day pass through this corridor and that is expected to increase to 21,200 VPD by 2028. In addition, traffic for Purdue home football and basketball games use this corridor for access/egress to/from the stadiums. Signage encourages northbound I-65 traffic to use the SR 25/US 52/Yeager Road/Northwestern Avenue route for event traffic.</p>

5. Laszlo Kovacs (Email)	An existing roundabout in West Lafayette is underutilized and is a luxury.	The one existing roundabout in West Lafayette is in the middle of the Purdue Research Park, where traffic volumes are low. The roundabout was selected and built by the Purdue Research Park to become an aesthetic feature of the park. It is also larger than a typical roundabout, with a diameter of 215 feet. A typical single-lane roundabout is 100 to 150 feet in diameter. Increased safety at an intersection is also not a luxury.
	A roundabout at this intersection will be utilized by many drivers who may never had experience with such traffic pattern and will be confused.	There are major tourist destinations (with many first-time drivers), such as the ski village of Avon, Colorado, that make extensive use of multi-lane roundabouts with less crashes than before the roundabouts were constructed.
	The intersection at Northwestern Avenue (US 231) and Yeager Road needs improvement but a roundabout is not the solution. The cost of the project would be less without a roundabout.	A roundabout was chosen for this intersection because it handles the heavy skew angle between Yeager Road and Northwestern Avenue better than a traffic signal. When compared to the traffic signal, the cost is approximately the same, as the amount of pavement is slightly less than what is required for a signalized intersection. The cost of the roundabout is <i>less</i> when considering maintenance and power needs of the traffic signal equipment over the life of a roundabout (approx. 25-30 years). Also, because roundabouts process traffic efficiently, user costs in delay and fuel will be significantly lower than at a traffic signal.
6. Sharon Steel (Letter)	The intersection of Northwestern Avenue (US 231) and Yeager Road has traffic congestion during Purdue events and at the beginning/end of the semester. Many drivers will be first time visitors and unfamiliar with roundabout driving, especially with trailers, and construction vehicles related to Purdue.	<p>The City of West Lafayette has been making progress in public education of how to use roundabouts, and will continue to pursue public education at every opportunity. As proven by their use at other locations throughout Indiana and the United States, roundabouts are safer than traditional intersections. There are major tourist destinations (with many first-time drivers), such as the ski village of Avon, Colorado, that make extensive use of multi-lane roundabouts with less crashes than before the roundabouts were constructed.</p> <p>The roundabout will be designed to accommodate the turning radius of a large semi with a 65 foot long trailer, as seen on the interstate, and thus trailers, buses, and construction vehicles will have no difficulty maneuvering through the intersection.</p>

6. Sharon Steel (Letter) Cont'd.	Cost [of roundabout]	When compared to the traffic signal, the cost is approximately the same, as the amount of pavement is slightly less than what is required for a signalized intersection. The cost of the roundabout is <i>less</i> when considering maintenance and power needs of the traffic signal equipment over the life of a roundabout (approx. 25-30 years). Also, because roundabouts process traffic efficiently, user costs in delay and fuel will be significantly lower than at a traffic signal.
	Add a traditional corner with turn lanes and lights.	This option was considered. It was not chosen for several reasons: 1) roundabouts have been proven to be safer than traditional intersections, 2) the roundabout will process traffic more efficiently than a signalized intersection, and 3) the roundabout will serve as a transition for southbound traffic coming from US 52 (higher speed) to the more dense area of Northwestern Avenue (lower speed).
7. Matt Kopf (Email)	West Lafayette's traffic issue is because of Purdue University's traffic. Why is Purdue not giving up its land to accommodate their traffic?	The project will require the acquisition of 0.42 acres of property from the Purdue University Kampen Golf Course.
	Public hearings are meant to be an open forum that addresses public concerns; not set up for the city to control the meeting environment.	The Public Hearing held on February 10, 2010 was formatted and held in accordance with the INDOT Public Involvement Manual (April 2009) as approved by the Federal Highway Administration. The City provided an <i>open house</i> to answer any questions the public may have before and after the formal public hearing. A representative of INDOT was present to oversee the hearing and make sure the City and its partners adhere to the Public Involvement policies.
	Every year West Lafayette attracts new students and staff from out of state and foreign countries in addition the city has a high senior citizen population. Roundabout driving education will not reach these drivers. The roundabout will cause more accidents than prevent.	The City of West Lafayette has been making progress in public education of how to use roundabouts, and will continue to pursue public education at every opportunity. As proven by their use at other locations throughout Indiana and the United States, roundabouts are safer than traditional intersections. There are major tourist destinations (with many first-time drivers), such as the ski village of Avon, Colorado, that make extensive use of multi-lane roundabouts with less crashes than before the roundabouts were constructed.

<p>7. Matt Kopf (Email) Cont'd.</p>		<p>Regarding elderly drivers, the City of Clearwater, Florida has a large retirement population, and many roundabout installations. Many of the roundabouts in Clearwater were placed because of petitioning from local residents.</p>
	<p>Better traffic flow is needed in this corridor but there is a better solution than a roundabout.</p>	<p>Both a roundabout and a traffic signal were considered for the Yeager Road and Northwestern Avenue intersection. The traffic signal option was not chosen for several reasons: 1) roundabouts have been proven to be safer than traditional intersections, 2) the roundabout will process traffic more efficiently than a signalized intersection, 3) the roundabout will serve as a transition for southbound traffic coming from US 52 (higher speed) to the more dense area of Northwestern Avenue (lower speed), and 4) the traffic signal alternative would require a similar project footprint in order to accommodate the heavy skew angle between Yeager Road and Northwestern Avenue within current design standards.</p>
<p>8. Michael and Emily Lin (Fax)</p>	<p>The proposed project will remove 15 of the 36 parking spaces. Future development plans on their property may require additional parking spaces; will the City allow the project to proceed without the required parking spaces? Can the 15 parking spaces removed be counted in the total required number of parking spaces in future development?</p>	<p>Any parking spaces being removed will be compensated as damages according to Federal Highway Administration procedures for R/W acquisition. The City will continue to coordinate with Mr. and Mrs. Lin to minimize any impacts to parking.</p>
	<p>Would like to propose building a 24 feet wide driveway from Anthrop to alleviate the access difficulties that delivery trucks, customers, and tenants will experience.</p>	<p>The City will continue to coordinate with Mr. and Mrs. Lin regarding this and other concerns.</p>
	<p>Shift the entire Yeager Road widening project to the west.</p> <p>The City will benefit in the future by widening Yeager Road on the north side of US 52. However if the project is not shifted west many parking spaces will be taken from ACE Hardware as well.</p>	<p>The two project relocations are on the west side of the road. If the project is shifted west, right-of-way acquisition on the west side of the road will be increased, possibly requiring additional relocations. In addition, parking spaces are already being removed on the west side of the road. The City would not like to displace or further inconvenience businesses or residents on the west side of the road any more than currently shown in the project plans.</p> <p>Yeager Road north of US 52 will require some careful consideration, as there are parking lots on both the east and west side of Yeager Road within roughly the same distance from the edge of pavement.</p>

8. Michael and Emily Lin (Fax) Cont'd.	Eliminate the green space at the east end of the ASIA Market building and replace it with parking spaces.	This would require a variance of green space requirements from the City of West Lafayette. The City will continue to coordinate with the property owner regarding this and other concerns.
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Chapman, Brianna

From: Allen Busenitz [abusenitz@juno.com]
Sent: Thursday, February 11, 2010 12:02 PM
To: Chapman, Brianna
Subject: Yeager Road Reconstruction Project from US 52 to US 231

My wife and I frequently drive on Yeager Road, usually the portion from US 52 to US 231 regarding which the reconstruction project is being proposed.

Quite often when coming from the east on US 52 and turning left onto Yeager Rd, I find myself following a car that turns into the BP station at the first or north entrance. If the car is particularly cautious or encounters some obstacle in the BP station, those cars following are forced to stop on US 52 in the path of east-bound traffic creating a fearful situation. ***This has happened to me quite a few times.***

Therefore, I would suggest that the north entrance be closed and the south entrance/exit to the BP station widened by a lane. This should not inconvenience customers and should make the area much safer.

Some mention was made that it might make it too difficult for fuel trucks to negotiate if entering from the south entrance. Upon looking at the site, I am quite certain that this should not be a problem at least as things presently stand. I have seen trucks successfully maneuver in much tighter situations.

Sincerely,

Allen Busenitz
1201 Lindberg Rd
West Lafayette

[Diet Help](#)
[Cheap Diet Help Tips Click here.](#)

Fax Coversheet

ASIA ORIENTAL MARKET

2400 YEAGER ROAD, W. LAFAYETTE, IN 47906

Date and Time: 2-24-10

Attention: Brianna Chapman

Company: American Structurepoint

Fax Number: 317-543-0270

From: Asia Oriental Market

Fax Number: 765-497-4137

No. of Pages: 3 , including cover

If there are any problems or questions regarding this transmission, please call (765)463-4114.

Comments: _____

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Michael and Emily Lin
Asia Oriental Market
2400 Yeager Road
West Lafayette, IN 47906

RE: Parking and Access Due to Yeager Road Construction

February 24, 2010

Brianna Chapman
American Structurepoint
7260 Shadeland Station
Indianapolis, IN 46256

Dear Ms. Chapman:

We are writing regarding the proposed construction plans for widening Yeager Road between Northwestern Avenue and U.S. 52. Our family-owned business, Asia Oriental Market, and rental apartments are located at 2400 Yeager Road, an area directly affected by the proposed project. We will incur two major problems: (1) the loss of 15 of our 36 parking spaces and (2) obstructed access to our property for delivery trucks, customers, and tenants.

We thus make three proposals, which are detailed in the rest of this letter:

- (1) We propose that the entire Yeager Road widening project from Northwestern Avenue to U.S. 52 be shifted to the west.
- (2) We propose eliminating the green space at the east end of our building and replacing it with parking spaces. If parking becomes an issue in our future development of 2406 and 2412 Yeager Road, we would also like to be permitted to include the number of lost parking spaces as part of our total required number of spaces.
- (3) We propose building a driveway about 24 feet wide to enable access to 2400 Yeager Road from Anthrop Street.

The current construction plans deal only with that portion of Yeager Road to the south of U.S. 52 to Northwestern Avenue. The City would benefit in the future by widening Yeager Road on the north side of U.S. 52 as well. To do so, however, and to simultaneously line up the side of Yeager Road north of U.S. 52 with the side of Yeager Road south of U.S. 52, would require many parking spaces to be taken from ACE Hardware. These lost spaces would be in addition to the lost spaces we will incur if the current construction plans move forward. We thus propose that the entire Yeager Road widening project from Northwestern Avenue to U.S. 52 be shifted to the west. We would lose less parking spaces, and in the event that Yeager Road is widened on the north side of U.S. 52, ACE Hardware would lose less spaces as well. Such a modification to the proposed construction plans would facilitate the City's development goals while mitigating the impacts on local businesses.

Even assuming that the construction plans are shifted to the west, we will still lose parking spaces. We need a permanent solution that will create parking spaces to substitute for the spaces lost due to the widening of Yeager Road. We need these spaces to meet the parking needs of our customers and tenants and to comply with the local ordinance concerning required parking spaces. To that

end, we propose eliminating the green space at the east end of our building and replacing it with parking spaces.

Because of the loss of parking spaces, we are also concerned about our future development of 2406 and 2412 Yeager Road. If parking becomes an issue when we develop these areas, we would like to be permitted to include the number of lost parking spaces as part of our total required number of spaces.

Finally, regarding obstructed access to our property, we propose building a driveway about 24 feet wide to enable access from Anthrop Street. An Anthrop Street driveway would alleviate the access difficulties that delivery trucks, customers, and tenants will experience during the Yeager Road construction.

Thank you for your time and consideration. We look forward to hearing from you.

Sincerely,

Michael Lin
Emily Lin
Owners, Asia Oriental Market

cc: City of West Lafayette

2/19/10

BRIANNA CHAMBERLAIN.

AM CONCERNED WITH THE PROPOSED
PLAN TO BUILD A ROUNDABOUT AT
NORTHWESTERN AVE'S YEAGER ROAD.
IN PARTICULAR:

- 1) SAFEM - THE CORNER HAS
SIGNIFICANT TRAFFIC DURING
PURDUE EVENTS AND AT THE
BEGINNING/END OF THE SEMESTER.
MANY DRIVERS WILL BE FIRST
TIME VISITORS AND UNFAMILIAR
WITH ROUNDABOUT DRIVING'S
HAULING TRAILERS. CONSTRUCTION VEHICLES
(OST) TO/FROM PURDUE CAMPUS.
- 2) ADD TRADITIONAL CORNER W/ UP US52.
TURN LANES, LIGHTS
- 3)

THANK YOU.

SHARON STEEL

1141 GLENNWAY

W. LAFAYETTE, IN

47906

Chapman, Brianna

From: Kopf, Matt [MattKopf@allstate.com]
Sent: Tuesday, February 23, 2010 11:23 AM
To: Chapman, Brianna
Subject: Submission for Public Record---Yeager Road Reconstruction Project

Brianna Chapman,

The Yeager Road Reconstruction Project will be displacing my business along with one other property owner located behind my business. It is hard to believe that a city short on money for its teachers will remove yet 2 more tax paying properties. In my opinion, West Lafayette's traffic issue is because of Purdue University's traffic. Why is Purdue not giving up its land to accommodate their traffic?

I have spoke to many residents in regard to this project and few believe that it will help traffic flow this close to the university. With as many demographics in drivers that West Lafayette has along with game day traffic, it will cause more accidents than prevent. The Journal & Courier reported that the city will spend over \$24,000 to educate the public how to drive in a roundabout. One of my thoughts are that the drivers that will be contributing to the accidents will not have any of this education due to the fact that every year West Lafayette attracts new students and staff from out of state and foreign countries. Add to the mix of drivers West Lafayette's high senior citizen population that also may have never seen a roundabout, this solution does not serve its purpose. I agree that there needs to be better traffic flow and I think there are better solutions for everyone.

The public hearing held on this project was a joke; we are suppose to stand up and state our opinion or write our comments down only to have to wait on a written response back that is guarantee to be some general political response not addressing anything. Public hearings are meant to be an open forum that addresses public concerns; not set up for the city to control the meeting environment.

Matt W. Kopf

Exclusive Agent

[Allstate Insurance Co.](#)

[2306 Northwestern Ave](#)

[West Lafayette, IN 47906](#)

P: 765-497-9696

F: 765-497-9523

Toll Free 800-775-7704

Chapman, Brianna

From: Peppers [peppers2051@comcast.net]
Sent: Monday, February 22, 2010 1:53 PM
To: Chapman, Brianna
Subject: Yeager Road Project - West Lafayette

Attachments: Yeager Road.doc



Yeager Road.doc
(30 KB)

Ms. Chapman: My comments are enclosed for your files. L. Kovacs, Prof.

Emeritus

To: Brianna Chapman
American Structurepoint
bchapman@structurepoint.com

From: Laszlo Kovacs

RE: Yeager Road Project

Date: February 22, 2010

I have lived in West Lafayette since 1974, and am very familiar with the roadways in the area and have invested time and tax monies into the welfare of the community.

Costly engineering mistakes in the West Lafayette City Hall have been repeated many times, the most striking example is the 'Lindberg Road' project, which by the time is rebuilt will cost taxpayers \$ 15,000,000. There is an obvious **fixation on 'roundabouts'** in the West Lafayette engineer's office based on the assumption that it is a working alternative to solve the local traffic problems.

1. The existing 'roundabout' at the junction of Win Hentschell Dr. & McClude Av. is underutilized and is a luxury. I stopped there several times during the past months to survey the area and **NOT A SINGLE** automobile used that circle. Such traffic arteries are more properly designed for newly developed subdivisions, such as in west Zionsville and south-east Bloomington.

2. There was a public meeting a year ago at the West Lafayette City Hall about planning a 'second roundabout' in West Lafayette. That evening I voiced strong opposition about designing and funding such a plan. Starting another 'roundabout' project in the **Cumberland** area is ill-conceived, unjustifiable and a waste of the taxpayers money.

3. The currently proposed **Yeager Road Project and 'roundabout'** is another extravagant project; the design is ill-conceived and does not deserve public support. Yes, there is congestion at 231 (Northwestern Avenue) and Yeager Road; and also at the intersection of Yeager Road and 52 (Sagamore Parkway North).

Bu this third 'roundabout' **on state roads** would be used by drivers from many parts of Indiana and beyond, those who never had experience with such traffic pattern and will be confused!!! Adjusting driving habits is not that simple, and will not solve the pressure "for handling traffic for the rest of our lifetime." That is a mistaken assumption!

The financial dilemma of the country and the state will not be solved by Washington's infusion of obscene amount of *stimulus* funds into projects such as this. Yes, jobs should be created but by local and state initiatives. The financial pressure on

senior citizens just to survive on meager retirement funds has been mounting each year, with no relief in sight. The average adjustment in Social Security benefits for 2010 will be sufficient to purchase two pizzas a month. At the same time the state government is increasing sales tax; the local government adding hidden taxes; individual health care expenditures are spiraling; a referendum is imposed on us for increasing school support, and the list goes on.

During 2010-2015 period the emphasis should focus on improving the existing infrastructure of roads, bridges, and levies along the Wabash River. I agree that the intersection at Northwestern-Yeager Road needs improvement but **without** a 'roundabout,' that way the project would cost less than the projected three million dollars. Use the taxpayers' money wisely at the time when the country is heading toward fiscal bankruptcy.

Those who are making decisions about the Yeager Road project are doing the correct thing: should have the support of the taxpaying local public. Listen to us – please ?? !!

1709 Sandpiper Drive South
West Lafayette, IN 47906

e-mail: peppers2051@comcast.net

**PLEASE ACKNOWLEDGE THE RECEIPT
OF THIS COMMUNICATION.**

Thank you.

Chapman, Brianna

From: Aaron Wasburn [awasburn83@hotmail.com]
Sent: Thursday, February 11, 2010 5:25 PM
To: Chapman, Brianna
Subject: Yeager Road Project in West Lafayette

Thank you for taking public comments on the Yeager Road project. I have a few concerns.

- 1) First and foremost, I do not want roundabouts in West Lafayette. They are a very bad idea. No one knows how to use them, and I much prefer intersections. There will be tons of accidents if a roundabout is put at Yeager and Northwestern.
- 2) We do not need the trails. It is a bad location for a trail, and with the traffic, all there is to do on a trail like that is inhale exhaust.
- 3) The money it would cost could (I think I read \$1.3 million) could definitely be better spent elsewhere, such as when the roads need resurfacing. I don't think there is enough traffic on Yeager road to warrant any changes.

I would like to request that the project be cancelled.

Thank you very much for your concern and thanks again for taking public comments.

Chapman, Brianna

From: JKr5182853@aol.com
Sent: Thursday, February 11, 2010 1:50 PM
To: Chapman, Brianna
Subject: Yeager Road project in West Lafayette

Attention: Brianna Chapman
 Yeager Road Roundabout Project in West Lafayette, Indiana

My name is Joe Krause, I live at 2202 Trace 22, (Camelback) West Lafayette, IN.

I am writing in support of the Yeager Road project as far as I understand at this point. I am a former West Lafayette Councilor, having served two terms from 1988 to 1996. Currently I am an appointed member of the CityBus (Greater Lafayette Transportation Corporation) serving in my third four-year term.

I regret that I was not at the meeting on Wednesday, February 10, but I could not find out from the West Lafayette City web site as to whether the meeting was going to be held. You may recall there were many cancellations because of the inclement weather. Apparently your meeting was held anyhow. But neither the *Lafayette Journal and Courier* announcement nor the City of West Lafayette web site, nor for that matter, today's (2/11/10) *J&C* article mentioned which specific department was hosting the meeting; Development? Engineering?

OK, that out of the way.

1. I have often waited for an in-bound (towards Purdue campus) CityBus -Northwestern Route at a bus shelter on Anthrop near the intersection with Yeager. When Sycamore Lane was being worked on, the Northwestern bus by-passed Sycamore lane construction, going westward toward Northwestern. So, yes, I have waited alongside busy Yeager Road (in a sloped, sometimes muddy area--where there is no sidewalk) A paved trail would be helpful for pedestrians and bus passengers whenever CityBus bypassed Anthrop-Sycamore-Carlisle because of sidewalk and street construction.

2. When I drive, I do go over to the BP station at the corner of U.S. 52 (Sagamore Parkway West) and Yeager Road. So I often make left turns off of Yeager into a gas station.

3. I usually walk to the Asian Market and do know that the store has a very small parking lot.

4. I like roundabouts. I often use the roundabout currently located in Purdue Research Park. I recommend that more citizens practice using that roundabout if they feel confused or intimidated by such a configuration.

5. Roundabouts are safer; they slow down vehicles approaching intersections; they are efficient at moving crossing traffic. Properly designed they can protect pedestrians and also can accommodate large vehicles like trailer trucks and city buses. I favor all the roundabout plans the city of West Lafayette announced at previous times:

6. 'Specifically as to the roundabout at Yeager and Northwestern, **I think that would be excellent.** It would slow down cars coming southward toward campus from U.S. 52. They need to slow down anyhow as they are coming into a more highly congested area where there also could be considerable pedestrian crossing over to the Purdue Golf Course and to the Celery Bog to the west. It would also allow relatively easy right turns off of Northwestern (eastward) onto Yeager Road--this is already somewhat the case with a fairly constant "green arrow" to the right.

7. Specifically as to widening of Yeager Road, I gather that at least one house will have to be taken and the owner (Alice Steckel) understands that but would like to have the city move as quickly as possible. I agree and urge speedy disposal of this problem. I am worried a bit about diminished parking at Asia Market although

some persons do walk to it from nearby apartments. I am also worried about safe and easy access to Asia Market for those who must drive. So, widen if you must, but do it with care.

8. Finally, there is a small motel, Prestige Inn which has been sort of cut off from access from Northwestern Avenue. The only way to get to Prestige Inn from Yeager Road is to turn northward into the BP lot and then proceed carefully northward to the Prestige Inn. Coming from Sagamore Parkway eastward, a driver has to look for a quick right turn (no turn lane) from relatively high speed traffic before one comes to the Yeager-Sagamore Parkway West traffic light. One cannot access the Prestige Inn at all from northbound Northwestern and it would be very unsafe to make a left turn or even a U turn on the existing highway back to the Prestige Inn. But with a traffic roundabout at Yeager and Northwestern, it might be possible for a driver to come to the roundabout, go 270 degrees around it and then exit to Prestige Inn shortly thereafter. Also persons leaving the Prestige Inn could more safely exit onto Northwestern if traffic was being slowed by the roundabout at the former intersection of Yeager and Northwestern. (It would be much easier to show this with a map.but you get the general idea.

I am supplying contact information below.

Sincerely, Joe Krause

Joe Krause
home address: 2202 Trace 22
West Lafayette, IN 47906-1886
phone: (765) 743-8853
e-mail JKr5182853@aol.com

Appendix H: Air Quality

- a. Tippecanoe Area Plan Commissions Transportation Improvement Plan (excerpt)

Planning Support for TIP Projects

The following two tables document the planning support for both local and State Projects. Each list provides a project description or code number and the document and page number where the planning support can be found.

LOCATION	PROJECT TYPE	PROJCT or DES NO.	SUPPORTING DOCUMENTATION
Beck Lane (Poland Hill to Old 231)	Road Reconstruction & Widening	---	TP, TFP-15
Concord Road (Brady Lane to CR 350S)	Road Reconstruction & Widening	0500092	TP, TFP-14/15, FY '09 TIP
Concord Rd. & Maple Point (US 52 to Brady Lane)	Road Reconstruction & Widening & New CN	0800256	TP, FY '09 TIP
North 26th (Union to Cason)	Sidewalks & Ramps	0800010	SRTS Application
Old Romney Road (Twyckenham to SR 25)	Road Reconstruction & Widening	---	TP
South 9th (Twyckenham to CR 350S)	Road Reconstruction & Widening	---	TP, TFP-15, FY '09 TIP
Rome Drive (Shenandoah to Creasy)	Road Reconstruction	---	City Assessment
South 9th (CR 350S to CR 430S)	Road Reconstruction & Widening	---	TP, TFP-14, FY '08 TIP
South 18th Street (CR 350S to CR 430S)	Road Reconstruction & Widening	---	TP, TFP-14, FY '08 TIP
36th Street (Union to SR 26)	Road Reconstruction & Widening	---	City Assessment
Crosswalk, Flashes & Ramps	SRTS Grant	0800011	SRTS Application
Grant, Chauncey, Vine (Phase 1B)	Reconfigure One-Way Pair	---	TP, TFP-15, FY '09 TIP
Happy Hollow (US 52 to North River R.)	Reconstruction	---	TP, TFP-15, FY '09 TIP
School-Centered Program	SRTS Grant	0800009	SRTS Application
Soldiers Home Road (Kalberer Rd to US 52)	Road Reconstruction & Urbanization	---	TP, TFP-15, FT '09 TIP
Sycamore Lane (US 52 to Salisbury St.)	Traffic Calming	0600792	HES Study, FY '09 TIP
Wabash Heritage Trail Trolley Line to existing trail	New Trail Construction	0710997	West Laf. Strategic Plan
Yeager (US 52 to Northwestern)	Added Travel Lanes	0600696	TP, TFP-15, FY '09 TIP
Wabash Heritage Trail (Happy Hollow to Rose St)	New Trail Construction	0810347	West Laf. Strategic Plan

Appendix I: Additional Studies

- a. Environmental Justice Data Analysis and Mapping
- b. Section 4(f) Documentation



AMERICAN
STRUCTUREPOINT
INC.

Section 4(f) Documentation
for
Impacts to Recreational Facility

Yeager Road Improvements
Purdue University Kampen Golf Course
West Lafayette, Tippecanoe County, Indiana
Des. No. 0600696

City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

Indiana Department of Transportation
Office of Environmental Services
642 Government Center North
100 North Senate Avenue
Indianapolis, Indiana 46204

Federal Highway Administration
254 Federal Office Building
575 North Pennsylvania Street
Indianapolis, Indiana 46204

November 9, 2009

INDOT Approval

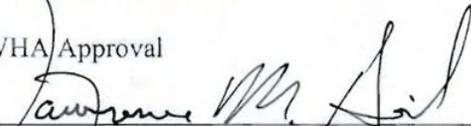


Ben Lawrence, Administrator

11-18-09

Date

FWHA Approval



Robert F. Tally, Jr., PE, Division Administrator

11-23-2009

Date

IN20061278



Section 4(f) ‘de minimis’ Impact Documentation
FOR
Yeager Road Improvements
West Lafayette, Tippecanoe County, Indiana
Des. No. 0600696

A. Proposed Action

The City of West Lafayette is planning a project that involves the widening and reconstruction of Yeager Road in West Lafayette, Tippecanoe County, Indiana. The project limits are from Northwestern Avenue (US 231) to Sagamore Parkway West (US 52), a distance of approximately 1,300 feet. The project will include Transportation System Management (TSM) improvements, which include providing and improving drainage, maintaining the existing center two-way left-turn lane (TWLTL) for adjacent property access, providing one additional through lane in each direction, along with a multi-use path/trail and sidewalk for pedestrians.

This road reconstruction (non-freeway) project will include specific improvements as follows. The existing three lane configuration will be reconstructed adding two additional through lanes, one in each direction. The center TWLTL will be maintained, as there are numerous private driveways along the project corridor. The road cross-section will thus include 4 through lanes 11 feet wide and one TWLTL, 14 feet wide.

A modern roundabout will be constructed at the existing intersection of Northwestern Avenue (US 231) and Yeager Road. The existing signalized intersection is skewed causing impaired sight distance. The existing Yeager Road intersection with Northwestern Avenue (US 231) will require reconstruction to accommodate the added travel lanes and improve the substandard skew (22.6°) and curvature (245-foot radius) to meet design standards.

The Yeager Road Corridor serves the west side of West Lafayette as a north-south urban connector. The proposed project has a total length of approximately 1,300 feet. The project begins at Northwestern Avenue (US 52) and extends due north to Sagamore Parkway West (US 231).

Major intersections along the corridor include Northwestern Avenue, Anthrop Drive, and Sagamore Parkway West. The two intersections at each end of the project are signalized and Anthrop Drive, which is a T-intersection, is stop-controlled for the minor approach. The roadway includes a center TWLTL, one northbound lane, and one southbound lane. There is some curbing of varying types and a sidewalk on the east side only. Most of the adjoining property owners are commercial, with some single-family residential use.

The improvement of Yeager Road will provide additional capacity, serving local and sub-regional needs in the north-south corridor. Yeager Road/Northwestern Avenue is one of only three major north-south corridors in West Lafayette, with the other two being Salisbury Street and Soldiers Home Road/Happy Hollow Road/River Road. Yeager Road connects a federal highway, Northwestern Avenue (US 231), with another federal highway, Sagamore Parkway West (US 52) and provides two of the US 52 and US 231 interchange movements (NB to EB and WB to SB). Further improvements to Yeager Road north of Sagamore Parkway West are shown in the Tippecanoe County Area Plan Commission’s (TCAPC) 2025 Plan.

Improvements to Yeager Road will provide system linkage for pedestrians and bicyclists on the planned multi-use path/trail. The multi-use path will provide bicyclists and pedestrians with vital links to other planned trails/greenway projects identified in the 2009 Transportation Improvement Program (TIP) for the TCAPC.

B. Purpose and Need

The primary purpose of the proposed project is to provide system linkage, sufficient capacity, and safe travel for motorists, bicycles, and pedestrians along Yeager Road. A secondary purpose of the project is to address the deteriorating pavement and upgrade the storm drainage system.

It is necessary to improve the capacity and deteriorating pavement and upgrade the storm drainage system, because Yeager Road is a main north-south corridor in West Lafayette. Yeager Road/Northwestern Avenue is one of only three major north-south corridors in West Lafayette, with the other two being Salisbury Street and Soldiers Home Road/Happy Hollow Road/River Road. Yeager Road connects a federal highway, US 231 (Northwestern Avenue), with another federal highway, US 52 (Sagamore Parkway West) and provides two of the US 52 and US 231 interchange movements (NB to EB and WB to SB). Further improvements to Yeager Road north of Sagamore Parkway West are shown in the TCAPC 2025 Plan.

Yeager Road is a main thoroughfare for Purdue University football and basketball traffic. Added capacity along Yeager Road is needed to facilitate traffic during home football and basketball games.

It is also necessary to address safety concerns in the project area. Crash data collected from 2006, 2007, and half of 2008 identified 33 crashes within 500 feet of the intersection of Yeager Road and Northwestern Avenue. Upon studying the crash data and considering the factors involved in 82% of the crashes, the need to reduce vehicle speeds and improve intersection geometry was identified.

In addition, multi-residential properties and student housing are located within the project area. Improvements to Yeager Road will provide system linkage for pedestrians and bicyclists on the planned multi-use path/trail. The multi-use path will provide bicyclists and pedestrians with vital links to other planned trails/greenway projects identified in the 2009 TIP for the TCAPC.

C. Section 4(f) Resources

The Purdue University Kampen Golf Course is located just west of Northwestern Avenue and the project area. The golf course is one of two golf courses owned by Purdue University, specifically the Purdue Research Foundation (PRF). The golf course is a publically-owned recreational resource within the project area. The Purdue University men's and women's golf teams use the course for practice; however, the course is also open to the public. The golf course has been home to more than ten golf championship tournaments and is one of the top collegiate golf courses in the nation. As the golf course is a public recreational resource, it is protected under Section 4(f).

D. Impacts on the Section 4(f) Property

The proposed roundabout at the intersection of Yeager Road and Northwestern Avenue will require strip right-of-way acquisition from the Kampen Golf Course (see attached exhibits). The golf course greens are buffered from the road by a large berm and roadside ditch. The acquisition of property will impact the berm and roadside ditch but will not impact the golf greens. The proposed project will acquire approximately 0.41 acre from the Kampen Golf Course.

E. Avoidance Alternatives

Several alternatives were studied for the proposed eastern loop road on campus. The following is a brief discussion of the alternatives studied.

Alternative A: The Do Nothing Alternative

The Do Nothing Alternative would leave the existing road as it currently is. This alternative would not correct the safety hazards present due to large volumes of traffic. No impacts to property protected under Section 4(f) would occur as a result of this alternative. This alternative is feasible but not prudent, as it does not meet the purpose and need for the proposed project. This roadway is heavily used during college events and is an important route for visitors.

Alternative B: Avoidance

This alternative includes reconstructing the roadway with full-depth pavement. With reconstruction, the roadway cross-section will be upgraded to a full urban cross-section with concrete curb and gutters and improved storm drainage facilities. No travel lanes will be added as a part of this alternative. For non-motorized travel, sidewalks with trail connections will be constructed. No reconfiguration of either intersection is proposed as a part of this alternative and therefore there would be no impact to Section 4(f) properties. This alternative is feasible but not prudent, as it does not meet the purpose and need for the proposed project. This alternative would not address the capacity deficiencies as noted in the purpose and need of the project.

Alternative C: Avoidance

This alternative involves the addition of two travel lanes in each direction to create a 5-lane cross-section. This alternative also includes the improvements to storm drainage facilities and the addition of concrete curb and gutters. Sidewalks with trail connections will also be constructed. This alternative involves no improvements to the intersections of Yeager Road and US 52 or Yeager Road and Northwestern Avenue. No Section 4(f) properties would be impacted by this alternative. This alternative is feasible but not prudent, as it does not meet the purpose and need for the proposed project. This alternative would not fully address the capacity deficiencies as noted in the purpose and need of the project.

F. Mitigation and Measures to Minimize Harm

The proposed project impact to Kampen Golf Course is considered a ‘*de minimis*’ impact. Approximately 0.41 acre will be acquired from the golf course and used for the construction of a roundabout at the intersection of Northwestern Avenue and Yeager Road. The right-of-way to be acquired consists of a roadside ditch and a berm. No portion of the greens will be impacted by the proposed project. This alternative would not adversely affect the activities, features, and attributes that qualify the Kampen Golf Course under protection under Section 4(f).

G. Coordination

Throughout the Section 4(f) Programmatic Evaluation, officials from the PRF have been consulted regarding the applicability of Section 4(f) and the anticipated ‘*de minimis*’ impacts to the Section 4(f) property.

David Hodde, Director of Real Estate and Physical Facilities at the Purdue Research Foundation, stated he has no objections to the location of the intersection and how it is affecting the Kampen Golf Course on the west side of US 231/Northwestern Avenue. For reference see the attached correspondence.

PURDUE

RESEARCH FOUNDATION

October 21, 2009

Jeromy L. Grenard, PE, PTOE
Traffic Services Manager
American Structurepoint, Inc.
1241 Cumberland Avenue, Suite D
West Lafayette, IN 47906

RE: Yeager Road Project
Des. No. 0600696
Project No. IN20061278

Dear Mr. Grenard:

Purdue Research Foundation is in receipt of the plans sent to us under cover letter of October 12, 2009 from Brianna Chapman of your staff. These plans are stamped "Stage 2 Submittal Submitted By: American Structurepoint, Inc. Date: June 23, 2009".

We have reviewed these plans and have no objection to the encroachment that is identified as being onto the outer limits of Kampen Golf Course along Northwestern Avenue. I am making no reference to the effect this may have on any utilities and I make no reference as to approval on that.

Should you have any questions on this, do not hesitate to contact me.

Sincerely,

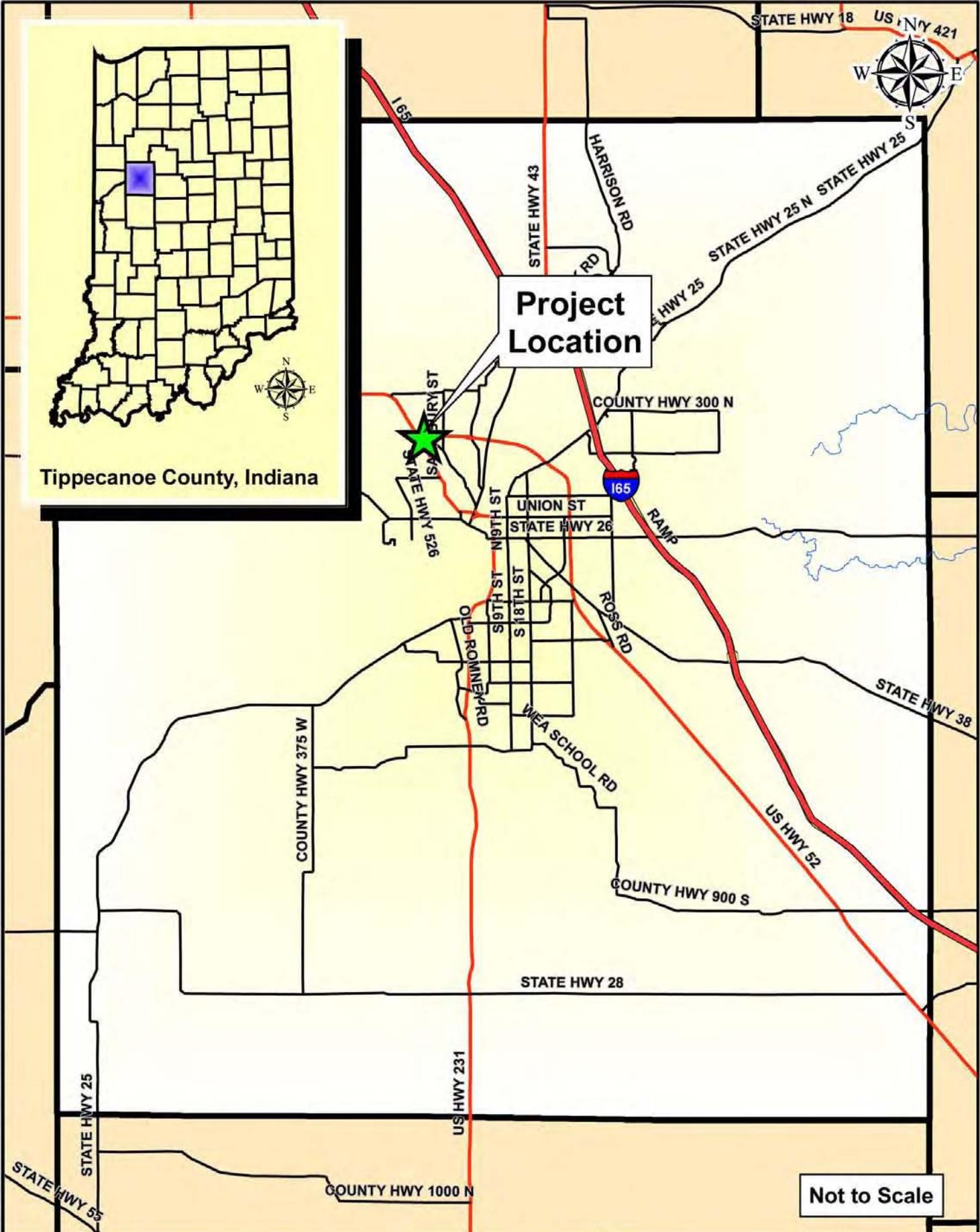


David L. Hodde
Assistant Vice President,
Director, Real Estate & Physical Facilities



Tippecanoe County, Indiana

Project Location



Not to Scale

Project Location Map

Yeager Road Improvements

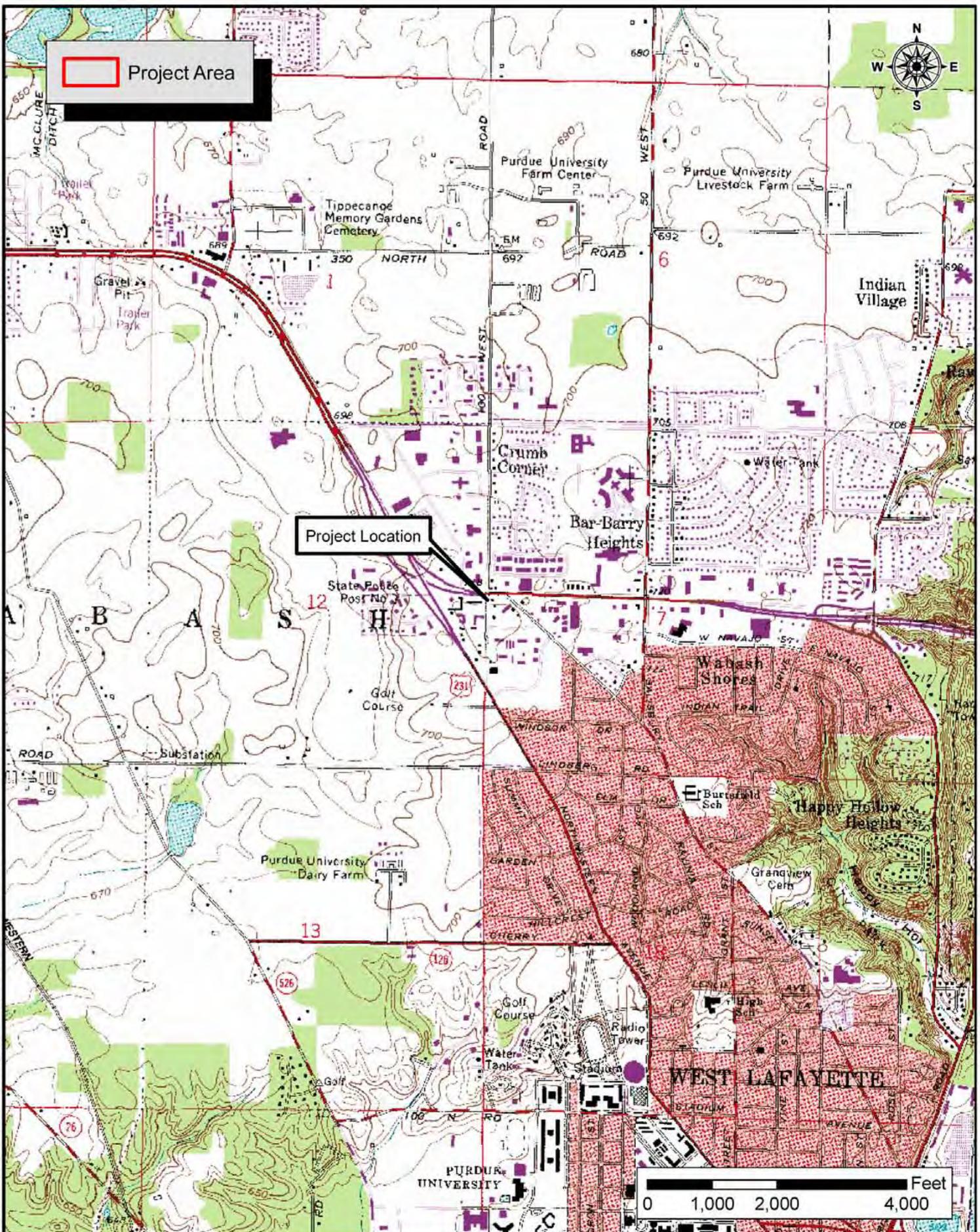


Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

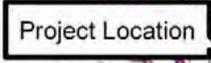
Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

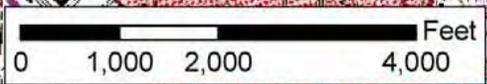
Appendix I
Section 4f
Page 7 of 13

Date: 04/18/2008



 Project Area

 Project Location



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USGS Topographic Mapping
West Lafayette Quadrangle

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

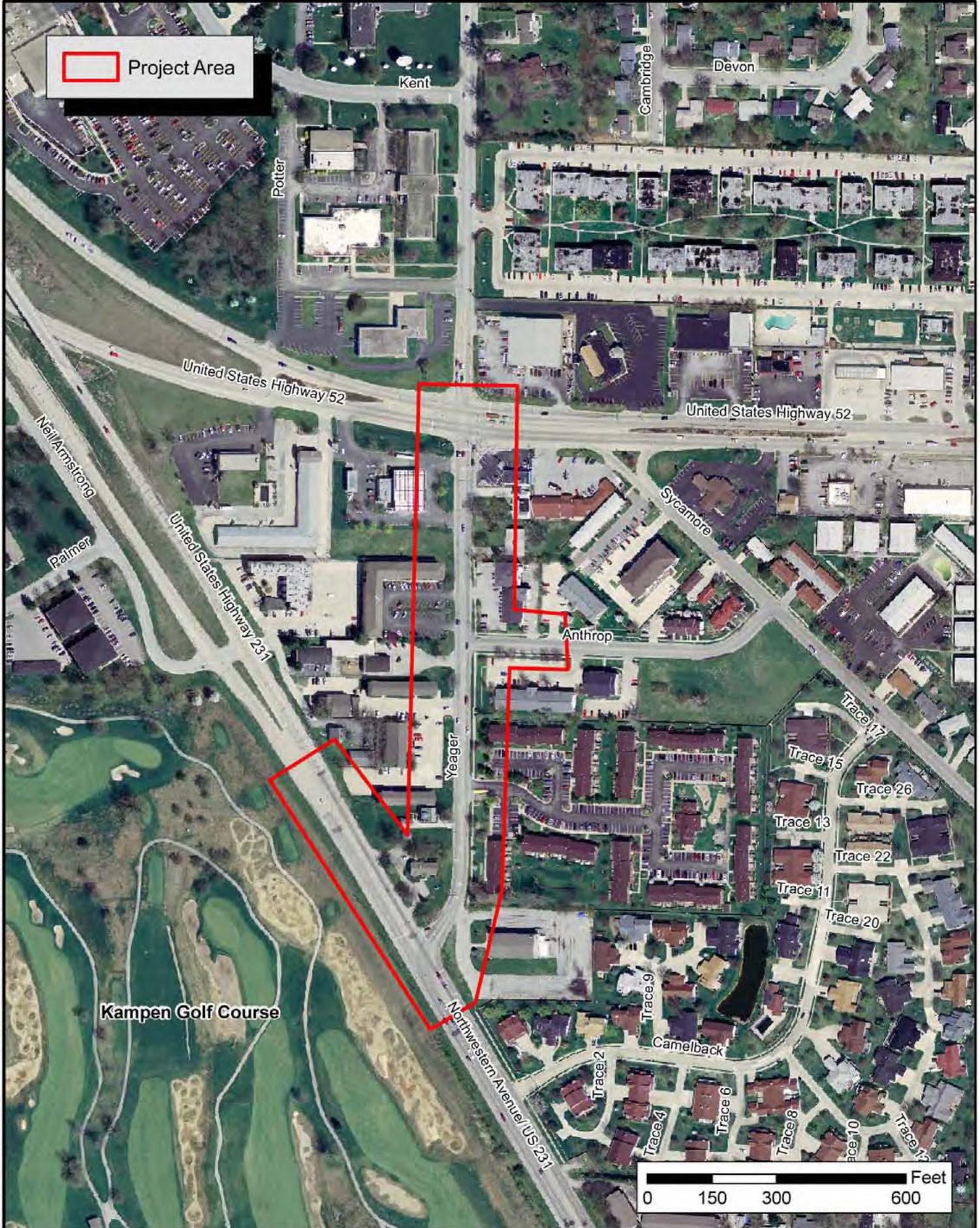
Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 04/18/2008

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Section 4f
Page 8 of 13

Project Area



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Project Location Map

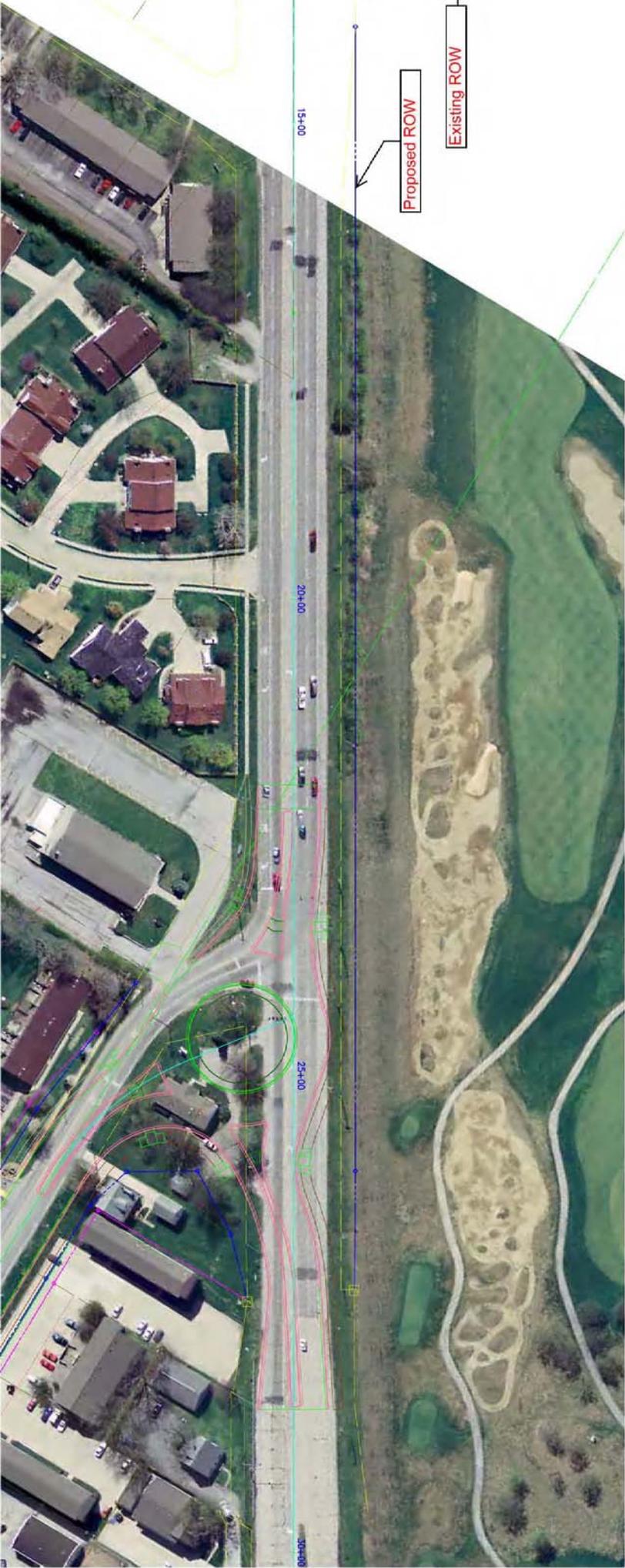
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City Hall
609 West Navajo Street
West Lafayette, Indiana 47906

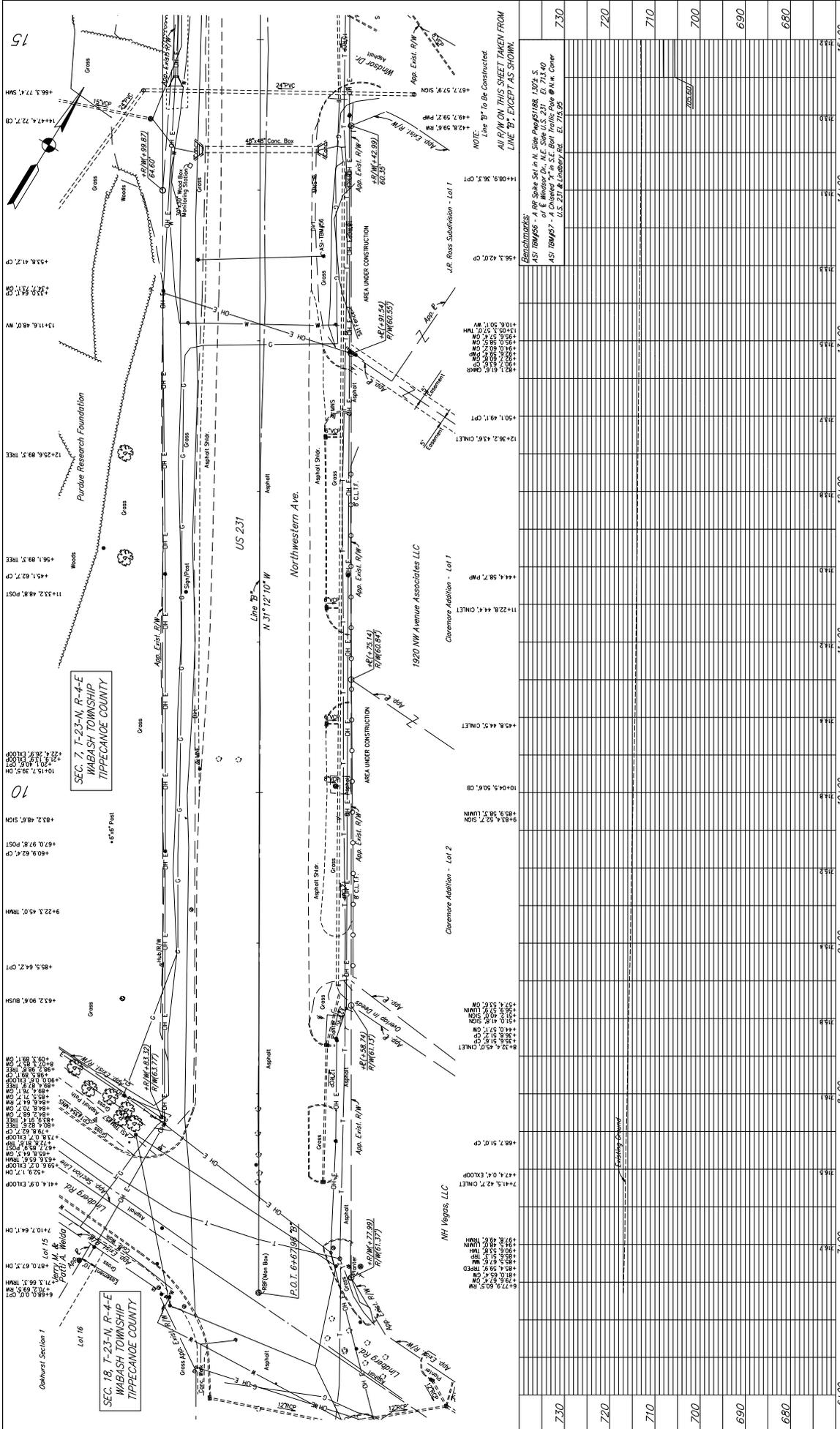
Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Date: 10/05/2009

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SEC. 7, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

SEC. 18, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

STATION	ELEVATION	DESCRIPTION
6+50.00	680.00	GRAVEL
6+55.00	680.00	GRAVEL
6+60.00	680.00	GRAVEL
6+65.00	680.00	GRAVEL
6+70.00	680.00	GRAVEL
6+75.00	680.00	GRAVEL
6+80.00	680.00	GRAVEL
6+85.00	680.00	GRAVEL
6+90.00	680.00	GRAVEL
6+95.00	680.00	GRAVEL
7+00.00	680.00	GRAVEL
7+05.00	680.00	GRAVEL
7+10.00	680.00	GRAVEL
7+15.00	680.00	GRAVEL
7+20.00	680.00	GRAVEL
7+25.00	680.00	GRAVEL
7+30.00	680.00	GRAVEL
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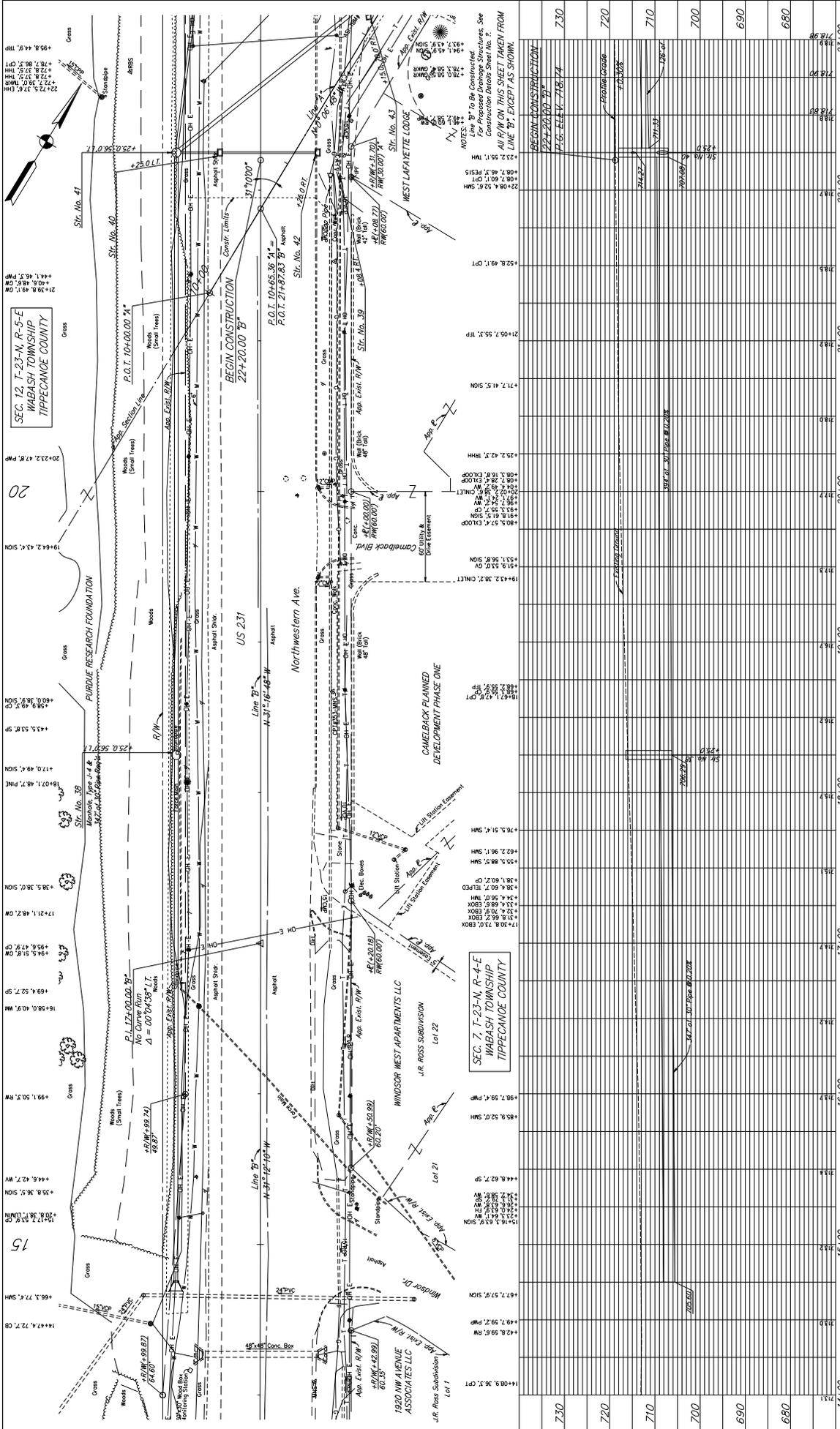
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
 LINE "B"

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: JMT	DRAWN: RJC	
CHECKED: MPB	CHECKED: MPB	

HORIZONTAL SCALE	BRIDGE FILE
VERTICAL SCALE	DESIGNATION NO.
1" = 10'	060606
SURVEY BOOK	SHEETS
CONTRACT	26 of 60
PROJECT NO.	

Appendix
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SEC. 12, T-23-N, R-5-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

20

15

SEC. 7, T-23-N, R-4-E
 WABASH TOWNSHIP
 TIPPECANOE COUNTY

1920 NW AVENUE
 ASSOCIATES LLC
 J.R. Ross Subdivision
 Lot 1

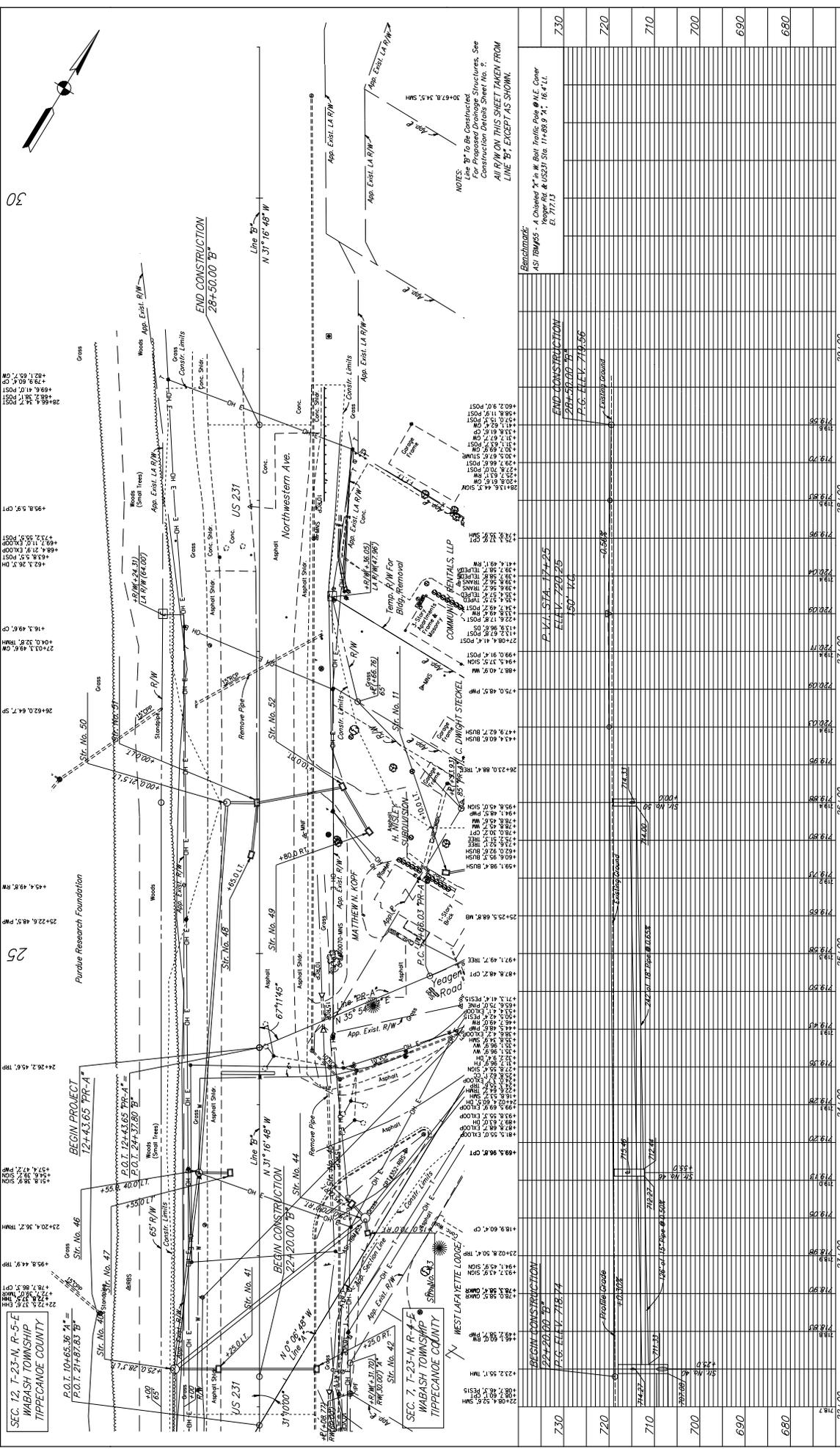
WINDSOR WEST APARTMENTS LLC
 J.R. ROSS SUBDIVISION
 Lot 22

CAMELBACK PLANNED
 DEVELOPMENT PHASE ONE

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: JMT	CHECKED: MPB	DRAWN: RJC	CHECKED: MPB		

INDIANA DEPARTMENT OF TRANSPORTATION		PLAN AND PROFILE		LINE "B"	
HORIZONTAL SCALE	BRIDGE FILE	DESIGNATION NO.	060606	SHEETS	27 of 60
VERTICAL SCALE	1" = 10'	SURVEY BOOK		CONTRACT	
		PROJECT NO.			

LEGEND
 [Symbol] Beg/LA R/W
 [Symbol] End LA R/W



NOTES:
 1. See P. 10 for Construction Structures. See Construction Details Sheet No. 8.
 2. All R/W ON THIS SHEET TAKEN FROM LINE "B" EXCEPT AS SHOWN.

Station	Remarks	Vertical Elevation
224+00	BEGIN CONSTRUCTION	730
224+20	15'-0" R/W	720
224+40	15'-0" R/W	710
224+60	15'-0" R/W	700
224+80	15'-0" R/W	690
225+00	15'-0" R/W	680
225+20	15'-0" R/W	670
225+40	15'-0" R/W	660
225+60	15'-0" R/W	650
225+80	15'-0" R/W	640
226+00	15'-0" R/W	630
226+20	15'-0" R/W	620
226+40	15'-0" R/W	610
226+60	15'-0" R/W	600
226+80	15'-0" R/W	590
227+00	15'-0" R/W	580
227+20	15'-0" R/W	570
227+40	15'-0" R/W	560
227+60	15'-0" R/W	550
227+80	15'-0" R/W	540
228+00	15'-0" R/W	530
228+20	15'-0" R/W	520
228+40	15'-0" R/W	510
228+60	15'-0" R/W	500
228+80	15'-0" R/W	490
229+00	15'-0" R/W	480
229+20	15'-0" R/W	470
229+40	15'-0" R/W	460
229+60	15'-0" R/W	450
229+80	15'-0" R/W	440
230+00	15'-0" R/W	430
230+20	15'-0" R/W	420
230+40	15'-0" R/W	410
230+60	15'-0" R/W	400
230+80	15'-0" R/W	390
231+00	15'-0" R/W	380
231+20	15'-0" R/W	370
231+40	15'-0" R/W	360
231+60	15'-0" R/W	350
231+80	15'-0" R/W	340
232+00	15'-0" R/W	330
232+20	15'-0" R/W	320
232+40	15'-0" R/W	310
232+60	15'-0" R/W	300
232+80	15'-0" R/W	290
233+00	15'-0" R/W	280
233+20	15'-0" R/W	270
233+40	15'-0" R/W	260
233+60	15'-0" R/W	250
233+80	15'-0" R/W	240
234+00	15'-0" R/W	230
234+20	15'-0" R/W	220
234+40	15'-0" R/W	210
234+60	15'-0" R/W	200
234+80	15'-0" R/W	190
235+00	15'-0" R/W	180
235+20	15'-0" R/W	170
235+40	15'-0" R/W	160
235+60	15'-0" R/W	150
235+80	15'-0" R/W	140
236+00	15'-0" R/W	130
236+20	15'-0" R/W	120
236+40	15'-0" R/W	110
236+60	15'-0" R/W	100
236+80	15'-0" R/W	90
237+00	15'-0" R/W	80
237+20	15'-0" R/W	70
237+40	15'-0" R/W	60
237+60	15'-0" R/W	50
237+80	15'-0" R/W	40
238+00	15'-0" R/W	30
238+20	15'-0" R/W	20
238+40	15'-0" R/W	10
238+60	15'-0" R/W	0
238+80	15'-0" R/W	-10
239+00	15'-0" R/W	-20
239+20	15'-0" R/W	-30
239+40	15'-0" R/W	-40
239+60	15'-0" R/W	-50
239+80	15'-0" R/W	-60
240+00	15'-0" R/W	-70
240+20	15'-0" R/W	-80
240+40	15'-0" R/W	-90
240+60	15'-0" R/W	-100
240+80	15'-0" R/W	-110
241+00	15'-0" R/W	-120
241+20	15'-0" R/W	-130
241+40	15'-0" R/W	-140
241+60	15'-0" R/W	-150
241+80	15'-0" R/W	-160
242+00	15'-0" R/W	-170
242+20	15'-0" R/W	-180
242+40	15'-0" R/W	-190
242+60	15'-0" R/W	-200
242+80	15'-0" R/W	-210
243+00	15'-0" R/W	-220
243+20	15'-0" R/W	-230
243+40	15'-0" R/W	-240
243+60	15'-0" R/W	-250
243+80	15'-0" R/W	-260
244+00	15'-0" R/W	-270
244+20	15'-0" R/W	-280
244+40	15'-0" R/W	-290
244+60	15'-0" R/W	-300
244+80	15'-0" R/W	-310
245+00	15'-0" R/W	-320
245+20	15'-0" R/W	-330
245+40	15'-0" R/W	-340
245+60	15'-0" R/W	-350
245+80	15'-0" R/W	-360
246+00	15'-0" R/W	-370
246+20	15'-0" R/W	-380
246+40	15'-0" R/W	-390
246+60	15'-0" R/W	-400
246+80	15'-0" R/W	-410
247+00	15'-0" R/W	-420
247+20	15'-0" R/W	-430
247+40	15'-0" R/W	-440
247+60	15'-0" R/W	-450
247+80	15'-0" R/W	-460
248+00	15'-0" R/W	-470
248+20	15'-0" R/W	-480
248+40	15'-0" R/W	-490
248+60	15'-0" R/W	-500
248+80	15'-0" R/W	-510
249+00	15'-0" R/W	-520
249+20	15'-0" R/W	-530
249+40	15'-0" R/W	-540
249+60	15'-0" R/W	-550
249+80	15'-0" R/W	-560
250+00	15'-0" R/W	-570
250+20	15'-0" R/W	-580
250+40	15'-0" R/W	-590
250+60	15'-0" R/W	-600
250+80	15'-0" R/W	-610
251+00	15'-0" R/W	-620
251+20	15'-0" R/W	-630
251+40	15'-0" R/W	-640
251+60	15'-0" R/W	-650
251+80	15'-0" R/W	-660
252+00	15'-0" R/W	-670
252+20	15'-0" R/W	-680
252+40	15'-0" R/W	-690
252+60	15'-0" R/W	-700
252+80	15'-0" R/W	-710
253+00	15'-0" R/W	-720
253+20	15'-0" R/W	-730
253+40	15'-0" R/W	-740
253+60	15'-0" R/W	-750
253+80	15'-0" R/W	-760
254+00	15'-0" R/W	-770
254+20	15'-0" R/W	-780
254+40	15'-0" R/W	-790
254+60	15'-0" R/W	-800
254+80	15'-0" R/W	-810
255+00	15'-0" R/W	-820
255+20	15'-0" R/W	-830
255+40	15'-0" R/W	-840
255+60	15'-0" R/W	-850
255+80	15'-0" R/W	-860
256+00	15'-0" R/W	-870
256+20	15'-0" R/W	-880
256+40	15'-0" R/W	-890
256+60	15'-0" R/W	-900
256+80	15'-0" R/W	-910
257+00	15'-0" R/W	-920
257+20	15'-0" R/W	-930
257+40	15'-0" R/W	-940
257+60	15'-0" R/W	-950
257+80	15'-0" R/W	-960
258+00	15'-0" R/W	-970
258+20	15'-0" R/W	-980
258+40	15'-0" R/W	-990
258+60	15'-0" R/W	-1000
258+80	15'-0" R/W	-1010
259+00	15'-0" R/W	-1020
259+20	15'-0" R/W	-1030
259+40	15'-0" R/W	-1040
259+60	15'-0" R/W	-1050
259+80	15'-0" R/W	-1060
260+00	15'-0" R/W	-1070
260+20	15'-0" R/W	-1080
260+40	15'-0" R/W	-1090
260+60	15'-0" R/W	-1100
260+80	15'-0" R/W	-1110
261+00	15'-0" R/W	-1120
261+20	15'-0" R/W	-1130
261+40	15'-0" R/W	-1140
261+60	15'-0" R/W	-1150
261+80	15'-0" R/W	-1160
262+00	15'-0" R/W	-1170
262+20	15'-0" R/W	-1180
262+40	15'-0" R/W	-1190
262+60	15'-0" R/W	-1200
262+80	15'-0" R/W	-1210
263+00	15'-0" R/W	-1220
263+20	15'-0" R/W	-1230
263+40	15'-0" R/W	-1240
263+60	15'-0" R/W	-1250
263+80	15'-0" R/W	-1260
264+00	15'-0" R/W	-1270
264+20	15'-0" R/W	-1280
264+40	15'-0" R/W	-1290
264+60	15'-0" R/W	-1300
264+80	15'-0" R/W	-1310
265+00	15'-0" R/W	-1320
265+20	15'-0" R/W	-1330
265+40	15'-0" R/W	-1340
265+60	15'-0" R/W	-1350
265+80	15'-0" R/W	-1360
266+00	15'-0" R/W	-1370
266+20	15'-0" R/W	-1380
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266+60	15'-0" R/W	-1400
266+80	15'-0" R/W	-1410
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267+60	15'-0" R/W	-1450
267+80	15'-0" R/W	-1460
268+00	15'-0" R/W	-1470
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268+80	15'-0" R/W	-1510
269+00	15'-0" R/W	-1520
269+20	15'-0" R/W	-1530
269+40	15'-0" R/W	-1540
269+60	15'-0" R/W	-1550
269+80	15'-0" R/W	-1560
270+00	15'-0" R/W	-1570
270+20	15'-0" R/W	-1580
270+40	15'-0" R/W	-1590
270+60	15'-0" R/W	-1600
270+80	15'-0" R/W	-1610
271+00	15'-0" R/W	-1620
271+20	15'-0" R/W	-1630
271+40	15'-0" R/W	-1640
271+60	15'-0" R/W	-1650
271+80	15'-0" R/W	-1660
272+00	15'-0" R/W	-1670
272+20	15'-0" R/W	-1680
272+40	15'-0" R/W	-1690
272+60	15'-0" R/W	-1700
272+80	15'-0" R/W	-1710
273+00	15'-0" R/W	-1720
273+20	15'-0" R/W	-1730
273+40	15'-0" R/W	-1740
273+60	15'-0" R/W	-1750
273+80	15'-0" R/W	-1760
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274+40	15'-0" R/W	-1790
274+60	15'-0" R/W	-1800
274+80	15'-0" R/W	-1810
275+00	15'-0" R/W	-1820
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275+60	15'-0" R/W	-1850
275+80	15'-0" R/W	-1860
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276+40	15'-0" R/W	-1890
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278+20	15'-0" R/W	-1980
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279+60	15'-0" R/W	-2050
279+80	15'-0" R/W	-2060
280+00	15'-0" R/W	-2070
280+20	15'-0" R/W	-2080
280+40	15'-0" R/W	-2090
280+60	15'-0" R/W	-2100
280+80	15'-0" R/W	-2110
281+00	15'-0" R/W	-2120
281+20	15'-0" R/W	-2130
281+40	15'-0" R/W	-2140
281+60	15'-0" R/W	-2150
281+80	15'-0" R/W	-2160
282+00	15'-0" R/W	-2170

Des. No. 0600696: Yeager Road Improvements
Environmental Justice Data Analysis
Comparison of the City of West Lafayette to Census Tract 52, Block Group 5

	COC	County	AC	Adjacent	Adjacent
	City of West Lafayette	Tippecanoe County	Block Group 5, Census Tract 52	Block Group 3 Census Tract 102.04	Block Group 2 Census Tract 51
LOW-INCOME POPULATION EJ ANALYSIS					
P087001	Population for whom poverty status is determined: Total	23760	1008	1445	2517
P087002	Population for whom poverty status is determined: Income in 1999 below poverty level	9099	354	214	737
	Percent Low-Income	38.3%	35.1%	14.8%	29.3%
	125 Percent of COC	47.9%	AC > 125% COC	AC > 125% COC	AC > 125% COC
	Potential Low-Income EJ Impact		NO	NO	NO
MINORITY POPULATION EJ ANALYSIS					
P007001	Total population: Total	28778	1064	1428	2552
P007002	Total population: Not Hispanic or Latino	27858	965	1369	2477
P007003	Total population: Not Hispanic or Latino; White alone	23454	723	1097	2014
P007004	Total population: Not Hispanic or Latino; Black or African American alone	675	86	53	95
P007005	Total population: Not Hispanic or Latino; American Indian and Alaska Native alone	42	7	7	6
P007006	Total population: Not Hispanic or Latino; Asian alone	3252	122	196	326
P007007	Total population: Not Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	8	0	1	0
P007008	Total population: Not Hispanic or Latino; Some other race alone	41	3	2	8
P007009	Total population: Not Hispanic or Latino; Two or more races	386	24	13	28
P007010	Total population: Hispanic or Latino	920	99	59	75
P007011	Total population: Hispanic or Latino; White alone	531	58	17	40
P007012	Total population: Hispanic or Latino; Black or African American alone	9	1	0	0
P007013	Total population: Hispanic or Latino; American Indian and Alaska Native alone	3	0	0	0
P007014	Total population: Hispanic or Latino; Asian alone	11	1	0	0
P007015	Total population: Hispanic or Latino; Native Hawaiian and Other Pacific Islander alone	1	1	0	0
P007016	Total population: Hispanic or Latino; Some other race alone	295	31	40	30
P007017	Total population: Hispanic or Latino; Two or more races	70	7	2	5
	Number Non-white/minority (P007001 - P007003)	5324	341	331	538
	Percent Non-white/minority	18.5%	32.0%	23.2%	21.1%
	125 Percent of COC	23.1%	AC > 125% COC	AC > 125% COC	AC > 125% COC
	Potential Minority EJ Impact		YES	YES	NO

Source: 2000 US Census Bureau

-  Project Area
-  Adjacent Block Groups
-  Affected Community (AC)



**Census Tract 005100
BlockGroup 2**

United States Highway 52

Neil Armstrong

Yeager

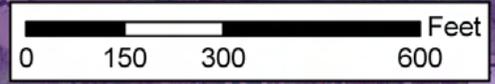
Sycamore

Anthrop

**Census Tract 005200
BlockGroup 5**

United States Highway 231

**Census Tract 010204
BlockGroup 3**



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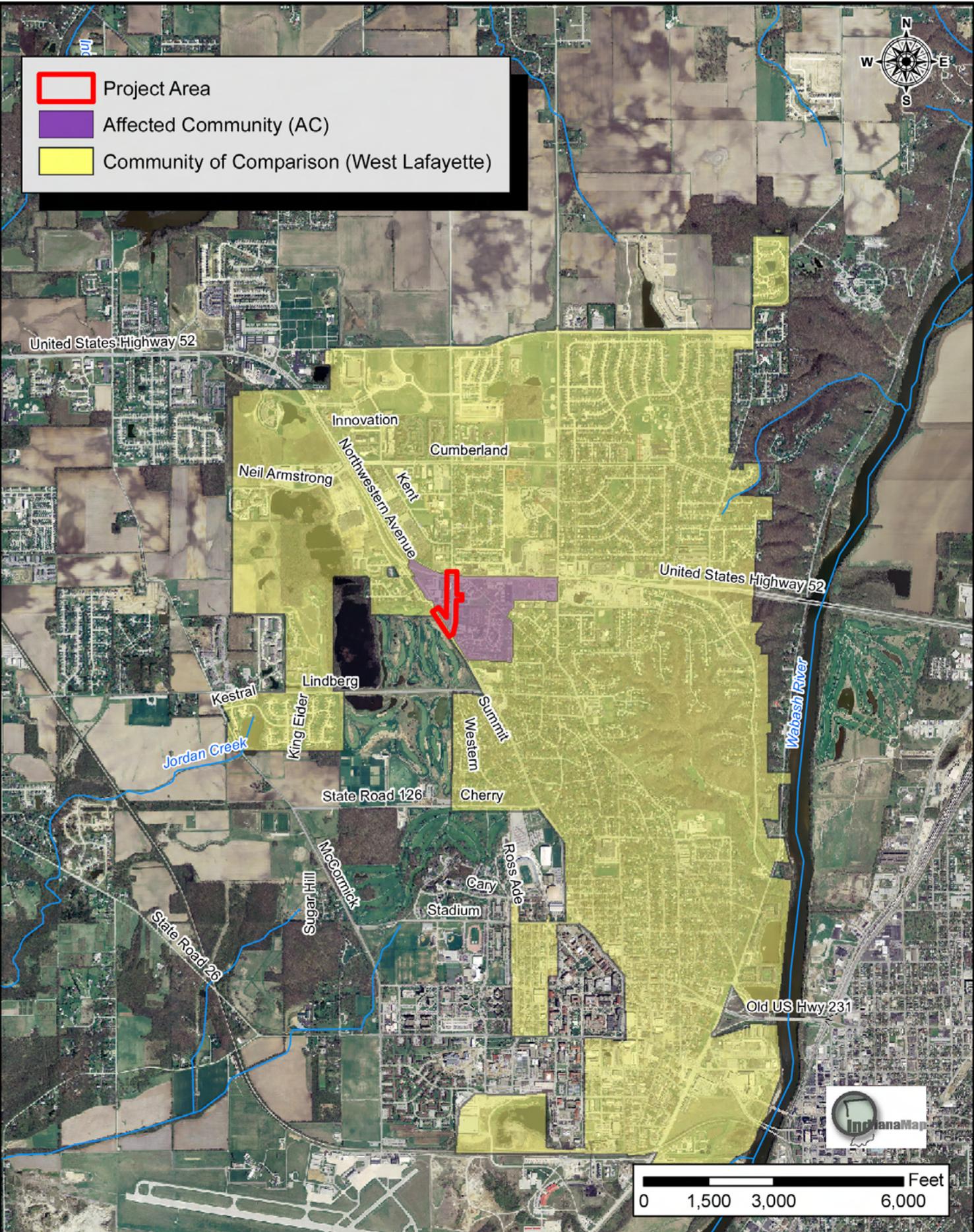


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US Census Bureau Tract & Block Group
 Applicant: City of West Lafayette
 City Hall
 609 West Navajo Street
 West Lafayette, Indiana 47906

Yeager Road Improvements
 Location: West Lafayette
 Township: Wabash
 County: Tippecanoe
 State: Indiana
 Appendix I
 EJ Evaluation
 Page 2 of 3
 Date: 05/14/2009

-  Project Area
-  Affected Community (AC)
-  Community of Comparison (West Lafayette)

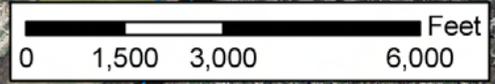


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Community of Comparison Mapping
City of West Lafayette, Indiana

Applicant: City of West Lafayette
City Hall
609 West Navajo Street
West Lafayette, Indiana 47906



Yeager Road Improvements

Location: West Lafayette
Township: Wabash
County: Tippecanoe
State: Indiana

Appendix I
EJ Evaluation
Page 3 of 3

Date: 08/03/2009